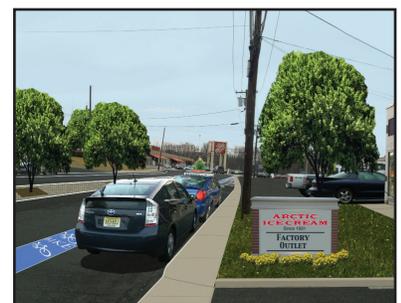


# OLDEN AVENUE

## *Redevelopment Plan*



February 11, 2011 (with Amendments August 12, 2014)





100 MARKETPLACE

A prime example of a shopping center can be built with the pedestrian in mind through scale and mixes of uses. Projects like this are similar to those that this Plan seeks to implement. While architectural elements can differ, the project's placement, orientation and pedestrian circulation system are the key components to implement.

*("100 Marketplace", Basking Ridge, NJ)*

## This Plan and its Application

*This Plan sets forth standards to correct those physical elements of the Redevelopment Area that negatively effect transportation and economic growth opportunities, particularly as it pertains to efficient access, utilization and an aesthetically pleasing atmosphere in which to do business in Ewing Township. The focus of the Plan has always been on creating a healthy, vibrant business district first and foremost. The Vision for Olden Avenue and its supportive network of streets, driveways, and easements sets forth a unified direction through which the Township may continue to grow the collaborative spirit with the Area's businesses and the County of Mercer that were solidified through this Plan's creative process.*

*More will always be required, particularly with regard to capital investment in the public's road infrastructure, but this Plan sets forth the framework. In order to truly implement this Plan, the Township and County must work to place Olden Avenue on the County's Transportation Improvement Program (TIP) and/or identify sources of capital. From this commitment, final roadway design documents can be developed, final cost estimates drawn, and funding for implementation sought. Notwithstanding, this Plan represents a vision of the future with direction on how to get there step-by-step, today.*

*The Plan also recognizes that strategic implementation will entail phasing, as funding will likely be difficult to obtain for 100 percent of the projects detailed herein. However, it is the vision of the Plan that will systematically bring Ewing toward remedying the issues that plague this area of the Township, both physically and socioeconomically. Additionally, the Township must continue to work through its Redevelopment Agency, Economic Development Commission, and members of its local Chamber of Commerce to realize the fantastic potential that exists along this historic commercial spine of Ewing Township.*



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# Preface

## Area Description

The Olden Avenue Redevelopment Area includes nearly 600 acres along North Olden Avenue from Princeton Avenue to north of Pennington Road, and extends north to the Shabakunk Creek and the Lawrence Township border and south to the City of Trenton border as shown on the Olden Avenue Redevelopment Area Map (Map 1). The area was designated by Township Council based on a Redevelopment Investigation Report dated June 1997. The study analyzed all parcels in the Area and determined that the majority of property met the criteria required by the Local Redevelopment & Housing Law (N.J.S.A. 40A:12A-1, et seq.). A redevelopment plan was then drafted and adopted by Ordinance of the Ewing Township Council on August 24, 1999.

## Efforts to Date

Since the adoption of the 1999 Olden Avenue Redevelopment Plan (OARP), the Ewing Township Redevelopment Agency (ETRA) has worked closely with both public and private sectors to implement the Plan. Notwithstanding, a new, highly coordinated approach is required to enable the marketplace improve and create new opportunities that spur economic growth within the Township.

It is clear that the OARP has helped to facilitate new economic development and aesthetic improvements that have increased the Township's ratable base. Several specific projects have come to fruition over the past six years that have had positive impacts on the surrounding neighborhoods whereby additional investment in the Township's existing housing stock has occurred. Some of the projects that have influenced these improvements include:

- The demolition of the flood prone 1800 Block of North Olden Avenue and the creation of Shabakunk Creek Park on the site of a FEMA funded project (2002) in Sub-area 1;



- The redevelopment of Parkside Plaza Shopping Center by S&S Investments that includes the new Shop-Rite (2002) in Sub-area 2;



- The redevelopment of the former Certified Steel site with new construction of a Home Depot (2002) in Subarea 6;
- The award in 2003, of a Livable Communities Grant from the New Jersey Department of Transportation that will help create utility easements on the 1800 block of North Olden Ave. Extension (Sub-area 1), relocate overhead utility lines (telephone, cable and electric), relocate and remove utility poles, and remove, relocate and replace sidewalks;
- The redevelopment of the Kuhn-Jacobs industrial facility into the Ewing Artist Loft Studios project was approved in Sub-area 6;
- The successful implementation of flood control measures along the Shabakunk Creek through a New Jersey Environmental Trust (NJEIT) grant and low interest loan. Although more needs to be accomplished, this project greatly improved the detrimental conditions imposed upon the Public's Health, Safety and Welfare;



- Mercer Development Associates, also known as Mercer Group International (MGI) entered into a “Redevelopment Agreement” to construct a 42,000 square foot, DEP approved, recycling facility;



- Neighborhoods long plagued by vacant and dilapidated structures continue to be improved through the efforts of the Agency.



- Reinvestment and expansion of businesses that once sat vacant and dilapidated, including the Senior Star Adult Day Care facility and residences have begun to take off.

### Why Amend the Plan?

It is appropriate that Redevelopment Plans be reviewed and amended just as Master Plans are required to under the Municipal Land Use Law. This OARP was revised in conjunction with the recent revisions to the Ewing Township Master Plan. As part of a NJDCA Smart Growth Planning Grant, ETRA, in cooperation with the Township Planning Board, accepted the responsibility of updating the Olden Avenue Redevelopment Plan. Through this update, it has been found that the design recommendations of the 1999 OARP are unfeasible based upon the issues that need to be addressed and the development standards necessary to implement change. This Plan advances a new approach that ties together economic development best practices and design techniques in order to resolidify the Avenue as the “economic spine” of the Township.

There are several reasons for updating the OARP:

- There has been development activity and interest in the Redevelopment Area that warrants a revisit of the Plan to better capture market potential.
- The Redevelopment Area exhibits characteristics that would allow continued short-term implementation while working toward a more ambitious longer-term vision.
- Redevelopment projects have shown the need for modification to the existing OARP and proposed transportation routes.
- The Urban Land Institute’s “Mercer Crossings” report recommended multi-jurisdictional cooperation between Ewing, Trenton and Lawrence Township’s. The New Jersey Local Redevelopment and Housing Law provides the legislative and planning tools to promote and implement stated objectives and to “develop uniform regulations” for the tri-municipal area. Ewing has the ability to drive this effort through a broader vision of regional stability incorporated in the redevelopment plan.
- The Ewing Township Master Plan was updated in 2006 and its associated land development ordinances are currently being updated by the Planning Board and Township Council.
- The redevelopment plan has the opportunity to take advantage of regional assets that can assist the Township to grow and prosper.

## The Challenges

Despite ETRA's proactive approach and some success, the Area has become more of a travel corridor rather than a shopping district. Greater traffic has led to the continuance of serious vehicular accidents at key intersections and turn lanes. This Plan's goals of traffic calming and reestablishing bus services along Olden Avenue remain high priorities and are envisioned to assist spur economic growth where it has been slow to be realized by creating better access and thus better, more friendlier place to do business.

The success of redevelopment will be influenced by how to redesign the roadway infrastructure and its impact on adjacent properties, neighborhoods, accessibility, traffic movement, and even aesthetic value. Lack of a complete road network increases congestion by reducing options for regional and local traffic - vehicular and pedestrian. The Delaware Valley Regional Planning Commission published a report that highlights roadway issues that require further analysis by the Township. <http://www.dvrpc.org/asp/pubs/reports/08053.pdf> Traffic calming, aesthetic improvements, and reestablishing bus services along Olden Avenue are in-fact high priorities but have been slow to occur because of the complexity of the road's design and the need for state and county public and private investment to assist pay for the necessary improvements. Through this plan ETRA seeks to:

- Improve travel safety for pedestrians, bicyclists, cars and trucks,
- Improve access to businesses, and
- Leverage public and private resources to create new economic development opportunities for the entire area.

## The Opportunities

While there are numerous opportunities evident, this new plan also builds on the Urban Land Institute's (ULI) "Mercer Crossings" project and subsequent Mercer County land use and transportation studies regarding Spruce Street, Princeton Avenue and the Farmers Market area. Mercer Crossings was a ULI Panel Services Forum that studied the area where Ewing & Lawrence Townships and the City of Trenton converge at Olden Avenue and Spruce Streets. The report and information regarding subsequent County studies in this area are available online at: [http://nj.gov/counties/mercer/departments/planning/mercer\\_crossings.html](http://nj.gov/counties/mercer/departments/planning/mercer_crossings.html).

This new OARP is intended to complement current local and regional planning efforts and lay the foundation for cooperative planning and development efforts that facilitate uniform development standards and allow redevelopment within and across municipal boundaries for the benefit of the region. By building a cooperative spirit and unified vision, economies of scale are created whereby every community benefits farther into the future than they would acting individually.

ETRA seeks to implement a vision that not only benefits the region but also importantly strengthens Ewing Township's ratable tax base. Under State Law, redevelopment plans and associated redevelopment agreements are among the few ways within a municipality by which strong design standards, that not only improve the aesthetic qualities of the Avenue but enhance the value of investment, can be established and enforced. The Plan contained herein allows the Township to implement its vision for Olden Avenue with sufficient flexibility built into the zoning standards that will enable private investment the ability to propose projects that the market can support without compromising the Area's future growth potential. Combined with a predictable development review process, this Plan will allow the Township the ability to effectively market itself while streamlining efforts by Township staff and Boards In affect, the Township will be in complete control of its destiny while showing property owners and investors alike that it is serious in creating a business-friendly atmosphere.



### **The Incentives**

Additionally, this Plan utilizes its development standards as an incentive for property owners to re-invest in their properties while also encouraging outside developer interest in improving the Area. Following the Plan will provide added density and bulk standards above what is currently allowed under Township Zoning. As can be found in more detail in the PILOT section under “Using the Plan” (page 59), the Township can also utilize PILOT’s or Payment in Lieu of Taxes as a tool that not only accelerates economic development within the Township thus adding to the tax base projects that would otherwise not have happened, but to direct public improvements to occur without additional burden on the taxpayers of Ewing. The Agency is also able to utilize its position to create programs for rehabilitation, direct potential investors toward competitive loan programs and reinvest PILOT money for infrastructure and streetscape improvements.

### **Establishing a Business Climate**

This plan is based on current land use development standards and practices. Through this plan, ETRA seeks to enhance Olden Avenue’s position in the region by further capitalizing on the area’s strengths to encourage new private investment to make Olden Avenue the premier place to do business. Through this Plan’s process, development applications will begin to have clearer direction and thus be streamlined and predictable. In effect, the Township’s implementation will be made efficient.

To date, the Township and ETRA have made significant progress together in implementing the plan, however the area remains at a disadvantage in competing with other destinations. A more ambitious vision is necessary to take the area’s residents and businesses toward a future where public gathering places, family- and community-oriented goods and services and new housing options for students, seniors and small households create a highly desirable place to invest in. To accomplish this, the Plan’s design standards and other incentives will facilitate private investment and inherently strengthen the ratable base into the foreseeable future. It will take hard time to accomplish, but “a journey of a thousand miles begins with a single step.





*Can this happen??? The opportunity lies in this Plan...*



# INTRODUCTION TO THE OLDEN AVENUE REDEVELOPMENT PLAN

The first Olden Avenue Redevelopment Plan was prepared more than a decade ago in cooperation with the Ewing Township Planning Board. It was authorized by the Ewing Township Mayor and Council pursuant to a Redevelopment Investigation Report approved by the Planning Board in June of 1997, which resulted in the designation by Council of the Olden Avenue Redevelopment Area and the creation of the Ewing Township Redevelopment Agency in 1998.

Redevelopment is a work in progress, but a solid Plan provides a framework for land use and land development within a redevelopment area, as well as an opportunity to apply financial incentives and other implementation tools to encourage the residents, business and property owners to improve and renovate certain parcels individually or through public-private partnership agreements among themselves, private sector redevelopers and other entities as defined by State law.

It is the intention of this Redevelopment Plan and the Ewing Township Redevelopment Agency to let market forces guide the implementation of the vision created through the Plan. This Plan provides incentives for design and development that enhance the market and promote negotiation with landowners without use of eminent domain. The designation of a redevelopment area and adoption of a redevelopment plan also provides opportunities to provide property tax incentives, in conjunction with special loan programs and coordinated public improvements to encourage and invigorate these forces.



## STATUTORY REQUIREMENTS

According to New Jersey's Local Redevelopment and Housing Law (N.J.S.A. 40A:12A-1, et seq.), the Redevelopment Plan shall include an outline for the planning, development, redevelopment or rehabilitation of the project area sufficient to indicate:

1. Its relationship to definitive local objectives as to appropriate land uses, density of population and improved traffic and public transportation, public utilities, recreational and community facilities and other public improvements;
2. Proposed land uses and building requirements in the project area;
3. Adequate provision for the temporary and permanent relocation as necessary of residents in the project area including an estimate of the extent to which decent, safe and sanitary dwelling units affordable to displaced residents will be available to them in the existing local housing market;
4. An identification of any property within the Redevelopment Area which is proposed to be acquired in accordance with the Redevelopment Plan;
5. Any significant relationship of the Redevelopment Plan to:
  - The Master Plans of contiguous municipalities;
  - The Master Plan of the County in which the municipality is located; and
  - The State Development and Redevelopment Plan adopted pursuant to the "State Planning Act" PL 1985, C398 (C52:18A-196 et al.).

## Redevelopment Area



### FIGURE 1 Description:

The Redevelopment Area boundaries as designated by Ewing Township Council in 1997: Beginning along Olden Avenue Extension, due west of Pennington Road, including certain blocks and lots fronting Olden Avenue Extension, including block 295 lots 8, 50, 55, 56, 7 and 25, fronting the westbound side of Olden Avenue Extension, and Block 234.01 Lot 41, and Block 296 Lots 48 and 24 fronting the eastbound side of Olden Avenue Extension as well as Lots 16-23, and lots 44-47 in Block 296. The boundary generally extends in an easterly direction including properties fronting along Olden Avenue Extension to its intersection with Pennington Road, where it extends in a northerly direction up Pennington Road to the intersection of Pennington Road and Pennwood Drive and includes those properties fronting on Pennington Road. The boundary then extends further east on Olden Avenue Extension to the intersection of Parkside Avenue and Olden Avenue Extension. The boundary then extends in a southerly direction on Parkside Avenue and then in an easterly direction towards Prospect Street, including certain properties north of Rosedale Avenue but not fronting Rosedale Avenue, specifically block 118.04 lots 3-5, 11, 12, 14, 16, 17, 19, 20, 21, 24, 25, 30, 31, 38, 39, 40, 43, 50, 53, 63, 73, 74 and 174. The boundary line then intersects with Prospect Street, continues south on Prospect Street to the City of Trenton border, including all properties to the east of Prospect Street. The boundary line follows the City of Trenton border along Ingham Avenue to its intersection with Calhoun Street, north on Calhoun Street and following the City of Trenton border to Princeton Avenue; extending north on Princeton Avenue to the intersection of Princeton Avenue and Spruce Street. The boundary then turns left on Spruce Street bordering Lawrence Township and continues in a westerly direction along Spruce Street to the intersection of Spruce Street and 4th Street where it the extends north along 4th Street including property on both sides of 4th Street to the intersection of 4th Street and Industry Court; the boundary then extends in a westerly direction, including property on both sides of Industry Court, and extending in an westerly direction towards 5th Street. The boundary then turns in a southerly direction down 5th Street including properties on the eastern side of 5th street, extending down 5th Street to Madison Avenue. At that point, the boundary turns west along Madison Avenue to its intersection with Prospect Street, including the following blocks and lots south of Madison Avenue but not fronting on Madison Avenue specifically block 40, lots 248-250 and block 40.01 lots 1-3, 5-11, 14-23, 33, and 35. The boundary then intersects with Prospect Street, and extends in a southerly direction where it intersects with Olden Avenue Extension incorporating specific lots in block 118.01 including lots 2, 3, 5-11, 14-19, 23 and 26 which front Olden Avenue Extension, the boundary extends north of block 118.01 along 13th Street to include lots 533-538 in block 85 and lots 542- 551 in block 84. The boundary then extends west along Olden Avenue Extension to the intersection of Olden Avenue Extension and Parkside Avenue.

## *A VISION OF THE FUTURE*

The Olden Avenue Redevelopment Plan envisions a transformation of the area into an active place where business can thrive from access by multiple modes of transportation including walking and biking. Creation of a climate where residents of Ewing Township and beyond come to enjoy amenities as they once did back in the days of Brieler's and the Parfait House is important toward creating economic diversity where once again Olden Avenue becomes the dynamic place it used to be but built in accordance with today's standards and consumer needs and desires.

The Olden Avenue Area will eventually reconnect the Township's stable, strong neighborhoods to that of the opportunities for diverse commercial goods and services with active public places designed into it. Taking full advantage of the Area's strategic location along the major gateways into three unique communities, new investment will help capture the opportunities for regional growth and long-term sustainability. Every building will add true value toward preserving the integrity of the surrounding communities with access by all reasonable modes of transportation.

New park spaces integrated within the fabric of each improved area will become gathering places. Environmental restoration and regional flood protection will become features that every resident can be proud of. Safe, clean waterways will become integral to the design of healthy neighborhoods and restoration of natural habitat.

New structures will provide exciting architectural elements that complement a diverse history. Buildings will, however, be built to the standards and requirements of the modern era and will accommodate some of the

best places to live, work, sell, or be entertained in the area. The new buildings will be contained in a web of new streets connected into the exiting network that are comfortable, well lit, green and shaded, colorful, lively, and friendly to both pedestrians and automobiles. Each corner, intersection, or gateway along these streets will be treated with a unique monumentality that captures the eye and attracts one and all.

Finally, the Olden Avenue Redevelopment Area will embrace the collegiate history of the Township by allowing and attempting to facilitate the integrating the Area's colleges (The College of New Jersey (TCNJ), Rider University, Mercer County Community College, and Thomas Edison College) into the fabric of everyday civic life. The Olden Avenue Area will be a place where job opportunities, community activities, and open spaces create the type of place where students, young families, and existing residents will desire to live, and where they can afford to do so. The Olden Avenue Area and its new neighborhoods will become another point of pride for Ewing Township and a source of stability in the region.



# *GENERAL GOALS OF THE REDEVELOPMENT PLAN*

To make this vision real, the following Goals apply throughout the Olden Avenue Redevelopment Area:

- A. Create a strong tax base that holistically combines proven planning and design techniques into a solid business -friendly atmosphere.
- B. Improve the functional and physical layout of the Area's street pattern which will calm regional through traffic, benefit businesses, and encourage walking, bicycling and public transportation.
- C. Establish a network of open spaces that unites all development through the integration of the greater Ewing Township and Mercer County open space network.
- D. Create public spaces throughout the area that provide focal points within each Sub-area. Focus on areas that may not be in Ewing but are community assets and should also be explored.
- E. Employ an integrated approach where today's projects do not inhibit the greater long-term vision for growth and development
- F. Improve pedestrian safety through a variety of techniques including traffic calming, intersection improvements and street-oriented land use patterns, where appropriate
- G. Control parking in the area by promoting shared parking as well as mixed private/public garage facilities and shared access drives.
- H. Create parking standards and regulation of parking placement within buildings to ensure that it is screened from view at sidewalk level by effective architectural treatment.
- I. Coordinate all development with new and existing land uses, circulation patterns, existing and proposed mass transit facilities, and streetscape improvements.
- J. Provide site improvement standards that seek to implement this comprehensive plan including height, setbacks and massing.
- K. Promote development that utilizes residential land uses as tool to spur economic development. Housing should include provisions for senior citizen, student and affordable housing.
- L. Create a destination where monumental gateways, unique and innovative architecture blend with surrounding neighborhoods.
- M. Create, where appropriate, mixed-use buildings that offer lively ground floor uses to enliven the streetscape and works to attract a variety of business types.
- N. Create new retail and office space for the area and the Township at large.
- O. Allow for short-term implementation of this plan by also considering long-term possibilities of alternatives to the existing traffic patterns in the Area.
- P. Promote cooperative intergovernmental efforts between the Township of Ewing, City of Trenton, Lawrence Township, Mercer County, NJDOT, NJDEP, and NJ Transit toward regional stormwater control, regional traffic patterns, new and existing public transit facilities, and open space funding.
- Q. Encourage investments in redevelopment and infrastructure that are responsive to the context and that best promote the opportunities and assets identified in the Opportunity Map (Map 2).

# OPPORTUNITY MAP

Redevelopment Area Boundary



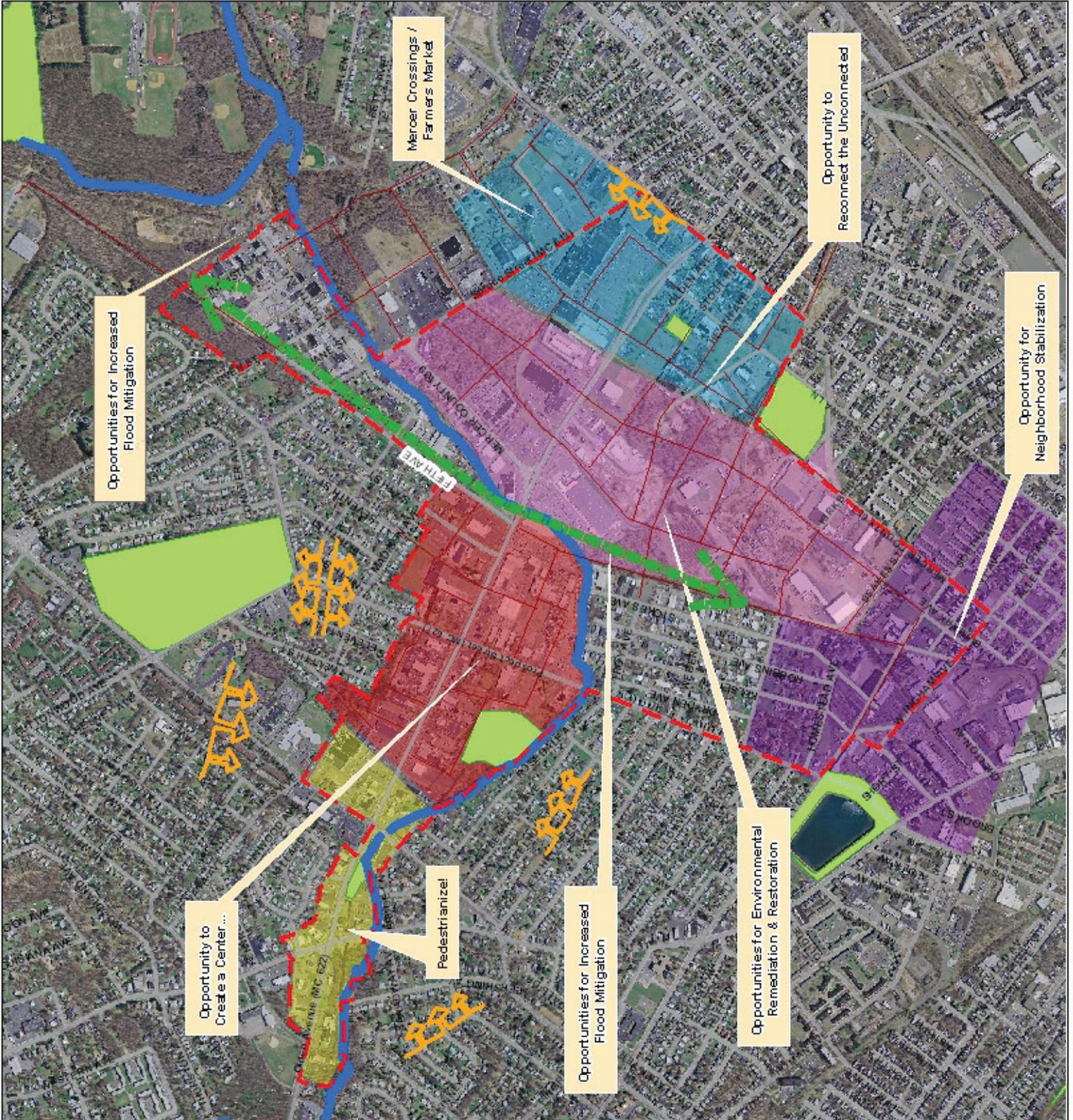
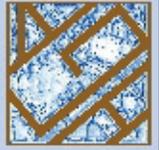
Parks & Open Space



Neighborhood Access



Rails-to-Trails (5th Street Extension)



Opportunities for Increased Flood Mitigation

Opportunity to Create a Center...

Pedestrianize!

Mercer Crossings / Farmers Market

Opportunities for Increased Flood Mitigation

Opportunities for Environmental Remediation & Restoration

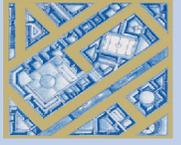
Opportunity to Reconnect the Unconnected

Opportunity for Neighborhood Stabilization

# Olden Avenue Redevelopment Plan OARP SubAreas

### SubArea

- #1 - Pennington Rd Neighborhood Commercial
- #2 - Prospect St Mixed Use Community
- #3 - Spruce St Small Business/Light Industry
- #4 - Mercer Crossings
- #5 - Stout Ave Neighborhood
- #6 - Calhoun St Regional Commercial/Industrial
- #7A - Prospect Park Small Business/Light Industry
- Area #7B - Prospect Park Neighborhood
- Area #8 - Arctic Parkway Transitional



# THE REDEVELOPMENT AREA & SUBAREA DESIGNATIONS

The Olden Avenue Redevelopment Area consists of a total of over 800 separate lots comprising 584 acres of land and is organized into Sub-areas as depicted in the OARP Sub-area Map (Map 3). This Plan applies within the Olden Avenue Redevelopment Area only and not to any area depicted outside of the boundaries designated by the Township Council as depicted in the Redevelopment Area Map (Map 1) and description.

The Existing Land Use Map (Map 4) and Existing Lot Sizes Map (Map 5) depict the current status of all lots within the redevelopment area as of the drafting of this Plan. It is through these characteristics, as well as the road patterns and surrounding neighborhoods, that the Plan was then able to reconstitute the Sub-areas and develop strong standards and incentives for development within each.

The Standards for Development provide a statement of intent, objectives, and development standards for each Sub-area. The applicable Sub-area intent and objectives

will serve as the basis that every project must strive to implement, regardless of whether any deviations to the standards in the Plan are sought.

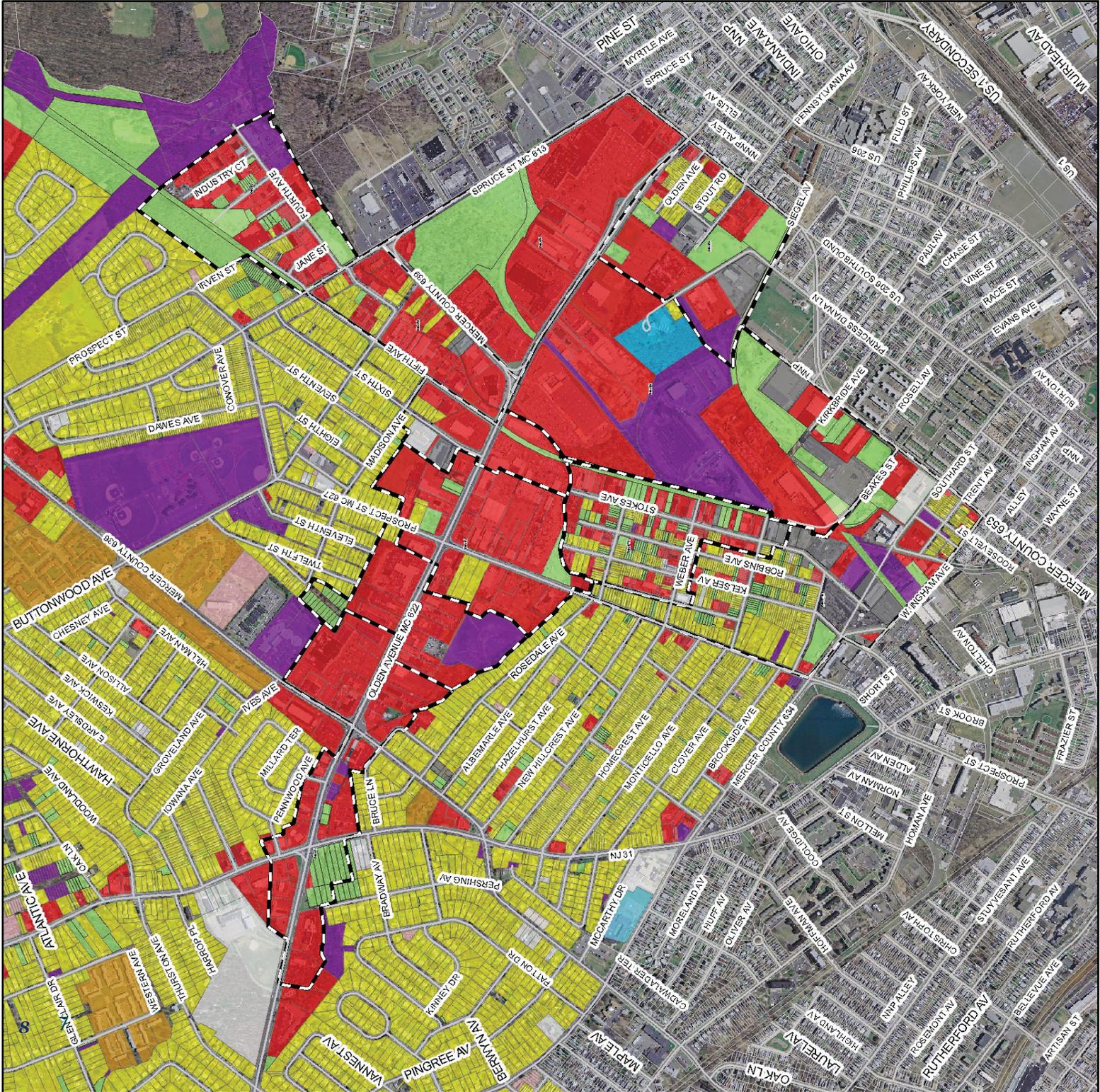
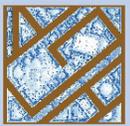
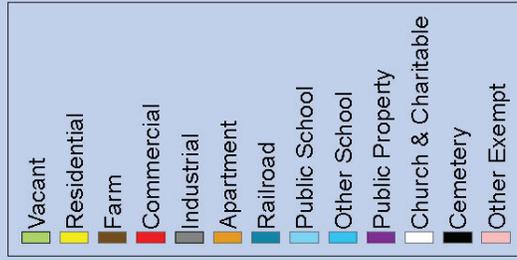
Through Redevelopment Agreements, projects will move from concept to eventual Planning Board applications. Upon designation, all development sought within the Plan’s jurisdiction will begin at the Ewing Township Redevelopment Agency (ETRA) level. Coordinated with the Township’s Site Review Committee to be deemed complete for the Planning Board, the Agency will review and recommend the prospective development application so that it may work its way through the Township’s Planning Board’s approval process in an efficient and expedient manner. In order to effectuate this Redevelopment Plan, the Local Redevelopment and Housing Law (N.J.S.A 40A: 12A, 8-9), also permits ETRA to enter into redevelopment agreements.

The Area can be described as having nine Zones with distinctive characteristics that have been called “Sub-areas” for the purposes of this Redevelopment Plan. Each Area’s vision and development standards are described fully in the Plan:

- Sub-area 1 Pennington Road Neighborhood District
- Sub-area 2 Prospect Street Mixed Use District
- Sub-area 3 Spruce Street Small Business District
- Sub-area 4 Mercer Crossings District
- Sub-area 5 Stout Avenue Neighborhood District
- Sub-area 6 Stokes/Calhoun Regional Commercial/Industrial District
- Sub-area 7a Prospect Park Small Business District
- Sub-area 7b Prospect Park District
- Sub-area 8 Arctic Parkway Transitional District

Map 4

# Olden Avenue Redevelopment Plan Existing Land Use



# STANDARDS FOR DEVELOPMENT

## Generally

The land uses and building requirements set forth in these Standards apply to the redevelopment of individual lots that conform, or are combined to conform to the lot size requirements of this Plan. Properties can be combined through a variety of binding legal instruments, including mutual easement agreements, Real Estate Investment Trusts (REITs), cooperative partnerships (limited partnerships, limited liability corporations, etc.), or even cross access easements to connect parking lots and/or walkways. The Township shall also have the ability to support redevelopment projects with alternative, non-conforming parcel configurations that substantially meet the goals and requirements of the Plan and do not negatively affect the development potential of adjoining properties or vision of this Plan.

Legal uses in existence in the Olden Avenue Redevelopment Area as of the date of the adoption of this Plan that are rendered non-conforming as a result of the Plan shall be considered legal pre-existing non-conforming uses. Such uses shall continue to be regulated under development approvals existing as of the date of the adoption of this Plan. The failure of any legal pre-existing non-conforming use to conform to specific Sub-area land use and bulk standards shall not result in the prohibition of such use. Expansion of such legal pre-existing non-conforming uses shall be permitted as indicated and will be subject to compliance with the applicable Sub-area Design Standards and General Design Standards set forth by Sub-area. Upon review of a development application submitted for a parcel upon which exists a legal pre-existing non-conforming use, the Planning Board shall have the authority to grant waivers, in its discretion, from the Sub-area Design Standards and General Design Standards set forth in this Plan, subject to review and recommendation by ETRA and, if applicable, the execution of a redevelopment agreement. Furthermore, the use must be detailed in the applicable Sub-area as a Permitted or Conditional Existing Use in order to obtain standing for such review.

While ETRA will encourage owners of properties containing pre-existing non-conforming uses to conform to the uses and design standards set forth in this Plan, such property owners shall be under no obligation to conform to such uses and design standards, unless they seek to expand. All properties, of course, must continue to comply with the applicable provisions of Township health, safety and land development ordinances. Existing businesses

seeking amended site plan approvals or expansions of legal pre-existing non-conforming uses are also eligible for property tax incentives and other applicable incentives to create those site improvements and alterations constructed in furtherance of this Plan.

The goal of the Township and ETRA is to create a “spirit of community” in the Olden Avenue Redevelopment Area whereby all existing and new property owners’ work in concert toward improving of the entire area.

## Affordable Housing

*In accordance with the LRHL, there are no designated affordable housing units in the OARP area, nor does the Plan identify the relocation of any residential land use market or otherwise. This Plan however will seek a minimum of 20% set aside for all residential projects.* Once COAH’s direction is set, the Plan will then provide affordable housing opportunities in accordance with the Council on Affordable Housing’s rules at the time an application for development is made. All development of affordable housing and contributions will be in accordance with the Ewing Township Housing Element & Fair Share Plan and other applicable requirements identified and enforced by the Township Planning Board in its approval.

## Off-Tract Improvements

Define mandatory contributions to the Area’s infrastructure through Redevelopment Agreements.

## Parks/Open Space

Advance implementation of the Parks and Open Space objectives, particularly for those properties adjacent to the Shabakunk Creek floodplain as well as those spaces depicted on the Environmental & Open Space Opportunities Map (Map 8) found in the Parks and Open Space Section of this Plan.

## Environmentally Friendly or “Sustainable” Design

Use of energy efficient site design including but not limited to LEED or Energy-Star Standards to reduce the ‘heat-island’ effect as well as to reduce stormwater runoff and improve water quality beyond reliance on traditional detention basin techniques. Alternative energy technologies such as solar panels and geo-thermal heating installations will also be encouraged.

## Applying the Plan's Development Standards

The Standards set forth in this Plan apply to all property in the redevelopment area. The regulations contained in this Plan supersede Township zoning except where this plan is silent with regard to a particular development standard, in which case the Ewing Township Land Development Ordinance will apply.

Pertinent information for all Development proposals are located within the Plan as follows;

-General Goals of the Plan .....	4
-Specific Sub-area Intent, Objectives, Land Use & Bulk Standards.....	12-42
-General Design Standards .....	43-48
-Infrastructure Standards.....	49-74

In application, the General Goals are what the Plan seeks to achieve holistically and comprehensively for the entire Plan area. The specific Sub-area Intent, Objectives, Land Use & Bulk Standards are the standards by which each individual project must adhere to in order to receive approval from the Planning Board and gain the financial benefits the Redevelopment Agency and Township is prepared to offer. The General Design Standards apply to all projects and the Infrastructure Standards may also, depending on the project's specific location. Should any conflicts exist in applying the Sub-area regulations and the General Design Standards, the Sub-area regulations will prevail.

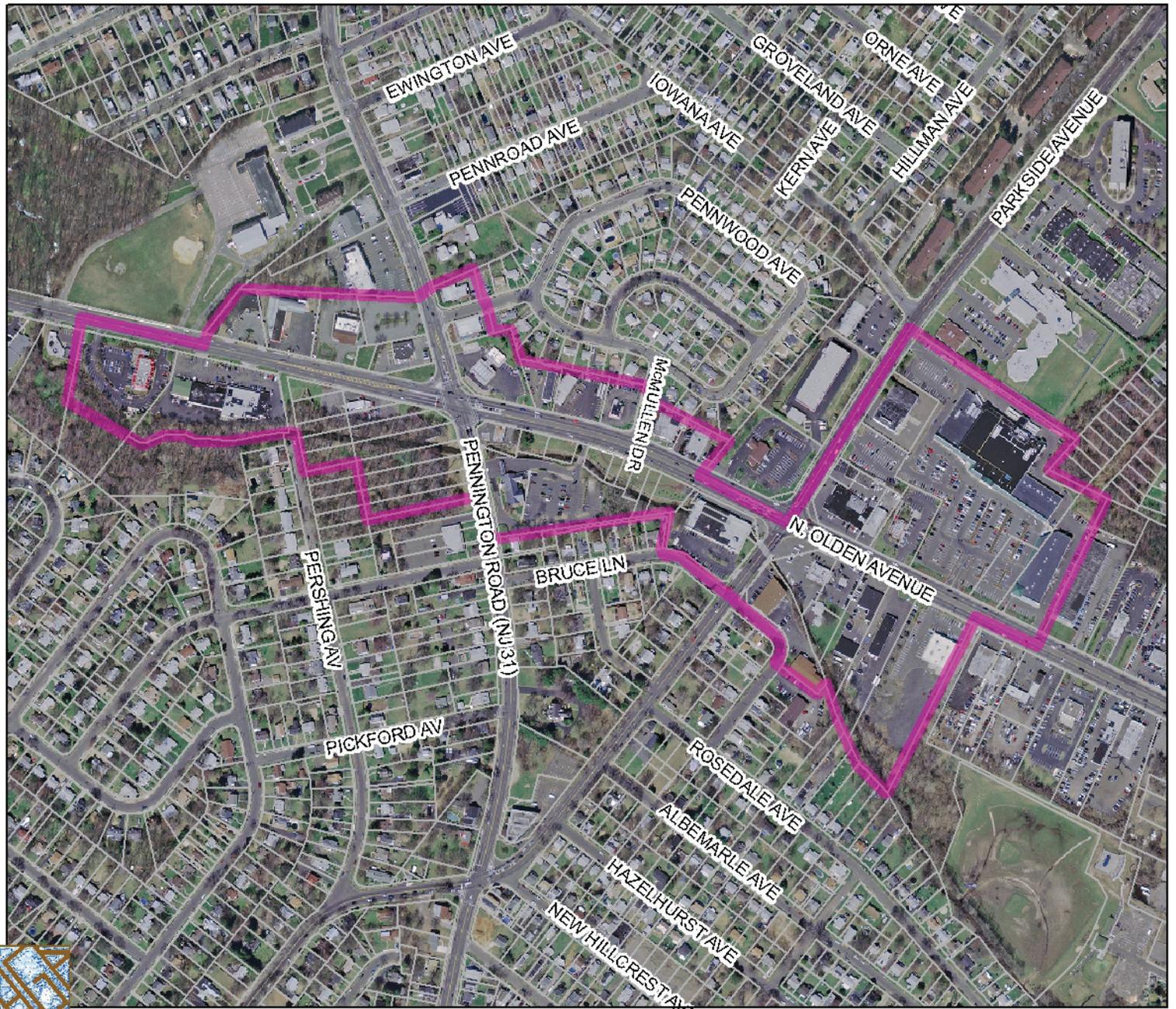
ETRA may, at its discretion, review and recommend to the Planning Board any deviation of a standard for a specific development project through a substitute provision specified in a Redevelopment Agreement, or for a publicly funded infrastructure project, where it documents that such standard would conflict with the ability to achieve a General Goal or Sub-area Intent or Objective. Nevertheless, this Plan seeks to put the Township and its Boards on the same page so as to create a predictable marketplace for both short and long-term investment.





SHABAKUNK CREEK  
PARK

Est. 2003



**Subarea #1 Aerial Photo**



SUBAREA

## Subarea 1 - Pennington Road Neighborhood District

### Intent

Sub-area 1 is the northwestern gateway into the redevelopment area. Aesthetic and infrastructure improvements must work to welcome consumers in. Creating better connections from adjacent neighborhoods to goods and services along Olden and Pennington Avenues is a major goal. To accomplish this, the Agency is committed to working with all property owners, including those outside of the Plan's jurisdiction to make it happen. This process has already begun with Shabakunk Creek Park, the Utilities Relocation Project at Olden and Parkside Avenues, as well as the installation of sidewalks by Incarnation Church on its side of Olden Avenue. However, much more can be achieved with minimal effort such as initiating a public/private partnership with Incarnation Church that makes better use of what is essentially a lawn along Olden Avenue to develop park space with trees, gathering spaces with recreational facilities that supports the economic initiatives of this Plan. To promote the safety of pedestrians and bicyclists, reducing risk of injury from turning motor vehicles, future drive-thru services will be limited, and consolidation of driveway curbcuts will be sought.

A portion of this Sub-area has been characterized by years of flooding from the Shabakunk Creek. To alleviate the situation, the Township has developed a flood mitigation strategy based on improving capacity in the Shabakunk flood plain and providing a system of detention facilities. While the Township's flood control plan does not involve additional use or acquisition of private property within the Olden Avenue Redevelopment Area, a number of private commercial properties have been severely affected by flooding in the past and may become available for future flood control. The Ewing Township Redevelopment Agency will continue to assist flood impacted property owners within the Redevelopment Area through proper site design, intergovernmental partnership, and potential regional stormwater mitigation measures.

### Objectives

- Encourage development patterns that facilitate retail sales of goods and services with easy access to adjacent residential neighborhoods.
- Connect adjacent neighborhoods to shopping opportunities through streetscape and pedestrian amenities.
- Work with Incarnation Church to create passive and active open space along Olden Avenue.
- Continue to improve stormwater flow along the Shabakunk Creek to reduce flooding while also providing additional open space opportunities.



*Projects such as the new CHASE Bank have implemented some of the Plan's key design elements (Although the address on the sign is a simulation.)*



*The Township's FEMA Project helped purchase flood prone, dilapidated structures in favor of a park that enhance flood protection efforts.*

## Land Use

### Principal Uses -

- Retail, and Business Services. (Such as those found in the Township's BH zone with exception to new and used motor vehicle sales and repair services.)
- Restaurant and Cafes
- Existing Auto Repair Facilities
- Day Care (Family and Child)

### Conditional Uses

- 1) Residential. **Conditioned Upon:**
  - Such uses are located above commercial use.
  - All parking requirements for the site are met or subject to cross access agreement.
  - Separate and safe pedestrian access is provided from curb-line to use.
- 2) Expansion of Existing Auto Repair Facilities. **Conditioned Upon:**
  - Such uses are existing.
  - Landscaping requirements and applicable buffers are satisfied to the Board's satisfaction.
  - Adherence to the applicable standards in the Design Standards Section. Waivers may be granted subject to review and recommendation by ETRA.
  - Sign and lighting standards are met.
- 3) Neighborhood Shopping Centers. **Conditioned Upon:**
  - Such uses are existing
  - Landscaping requirements and applicable buffers are satisfied.
  - Sign and lighting Standards are met.
- 4) Uses Identified in Township Land Development Ordinance 215-35 (11 and 12)
- 5) Drive Thru services are prohibited unless meeting the conditions set forth in *Building Placement and Design section*.

### Bulk Standards

#### a. Principal Building.

- 1) Lot area: 17,500 sq. ft. - minimum
- 2) Build to Line: 20' feet (with no parking in front) 50' build-to with front yard parking proposed. *For Uses seeking to rehabilitate pre-existing non-conforming structures ETRA may recommend to the Planning Board a waiver of the Build-to requirement. However, Architectural Elements of the proposal are*

*subject to review and approval in accordance with the standards identified in the Design Standards Section of this Plan.*

- 3) Side yard: each 5 feet - minimum
- 4) Rear yard: 10 feet - minimum
- 5) Height: 1-1/2 stories or 20' (3-1/2 stories or 35' feet upon Residential Conditional Use permit and/or Office located Above). *For Uses seeking to rehabilitate pre-existing non-conforming structures ETRA may recommend to the Planning Board a waiver of the Build-to requirement. However, Architectural Elements of the proposal will be subject of review and approval in accordance with the standards identified in the Design Standards Section of this Plan.*

#### b. Accessory Building.

- 1) Distance to side lot line: 5 feet - minimum
- 2) Distance to rear lot line: 5 feet - minimum

#### c. Lot Coverage.

- 1) *Total impervious coverage: 75% maximum. 65% for those properties located adjacent to the Shabakunk Creek.*
- 2) *Additional coverage up to 10% may be granted for properties located adjacent to the Shabakunk Creek if offset by contributions to other stream improvements such as, but not limited to stream bank stabilization, habitat restoration, or flood storage.*

#### d. Floor Area Ratio

- 1) 0.50 maximum. 1.0 maximum for Residential Conditional Uses and/or Office Above retail.

### Building Placement and Design

- Design ingress, egress and parking to be shared to minimize curb-cuts.
- Ensure that sidewalks and streetscape amenities are installed or can be installed upon completion of the applicable roadway design.
- Accommodate pedestrian access from the front in ways that minimize conflicts with vehicular access.
- *Drive-thru services must meet the following conditions;*
  - i. They existed concurrently with the existing use at the time of this Plan's adoption, Or,*

ii. On-site circulation, including pedestrian, as well as ingress and egress issues concerning adjoining roadways are satisfied by the Township Development Professional as well as the County Engineer, And,

iii. Direct ingress and/or egress onto Olden Avenue may be restricted or limited. Primary egress from a drive-thru must be located along a sidestreet, such as 6th Street, 5th Street, or other street created as a means of implementing the roadway network of the Plan's Transportation Section. Major routes, such as Parkside Avenue, Pennington Road, Arctic and Prospect Street may be similarly utilized, but the egress on these roadways must be located a minimum of 250' from the intersection and subject to approval of the engineer of record for such roadway's jurisdiction. Secondary ingress/egress points onto

Olden may be considered so long as such point of access is shared by two (2) or more properties through easement. Reconfiguration of the curb-cut and parking may be necessary upon negotiation with subsequent development proposals by adjacent property owners.

- Separation of Parking from Public Streets. Along each street line as defined bounding the district a seven (7') foot wide strip shall be provided, landscaped and hardscaped at a minimum of three (3') feet in height, except for sidewalks and access cut outs. The landscaped strip shall be separated by continuous concrete curbing except at access-ways and coordinated with Pedestrian access points, or use of creative stormwater management design.
- Outdoor patio and seating areas are encouraged.



*Promotion of a strong pedestrian environment is a long-term goal of the Plan. Aesthetic improvements through strong streetscape standards have been documented to calm traffic and enhance economic development initiatives.*





**Subarea #2 Aerial Photo**

-  Area #2
-  Area 2 CORE

0 500 1,000  
Feet

# SUBAREA

## Subarea 2- Prospect Street Mixed-Use District

### Intent

Located in the heart of the Redevelopment Area, this Sub-area has tremendous opportunity to transform itself into a walkable community center of commerce that focuses on placemaking and includes housing opportunities (including student ), parks, new dining, retail and entertainment opportunities and public gathering spaces.

Although many automotive land uses are currently located in this Sub-area, their presence does not preclude or inhibit the ability to create a place that includes the existing business community while presenting opportunities for change through well-designed development and improving the environmental quality of the Shabakunk Creek through additional open space and flood protection. High value new development will also work to solidify and increase the Township's ratable base. A pedestrian-oriented community is envisioned for this Sub-area. While existing businesses will be grand-fathered and encouraged to rehabilitate, in order to redevelop and expand, those uses must do so within the framework set forth by this Plan.

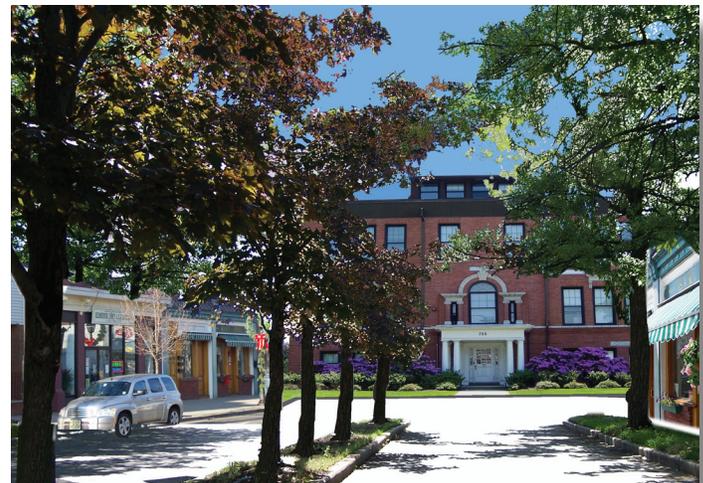
The integrated design approach discussed throughout the Plan is underscored in this Sub-area, particularly in the CORE area as indicated on Sub-area #2 Aerial Photo Map where the standards are more tightly defined. Each parcel's design will work together to create a unique place thus establishing a strong ratable base. This approach will only be accomplished upon realization by property and business owners that the increased development opportunities of this Plan are worth the effort of additional investment through the partnerships likely to be needed.

### Objectives

- Establish and promote a CORE of mixed-use development opportunities that work to create a center of commercial and residential activity for the eastern third of the Township.
- Create opportunities for property/business owners and to reinvest in the Township thus strengthening the ratable base.
- Reduce curb-cuts along North Olden Avenue through shared access and parking arrangements.
- Enhance deeded open space with the creation of additional public spaces, designed through redevelopment projects for residential enjoyment.
- Improve flood control along the Shabakunk Creek and reclaim floodplain land where existing uses degrade environmental quality.
- Coordinate the development of existing auto-related land uses in a way that allows the efficient use of land



*The present Boulevard-type entrance into a site along the east side of Prospect Street begins to reveal a glimpse of this Subarea's potential for transformation into a mixed-use community. The simulation below is merely one way it could look. A satellite Campus to TCNJ perhaps?*



*The Township's Flood Basin, once a golf driving range, can be further converted to become valuable public park space.*

while respecting environmental features and the intent of this Sub-area's future growth potential.

- Strengthen buffers between sensitive environmental features and deleterious land uses.

## Land Use

### Principal Uses

- Mixed-use buildings that include retail sales and business services and residential and/or office space on upper floors.
- Multi-family residential, including stacked townhomes, flats and condominiums, 55 and older communities.
- Institutions of higher learning including student housing
- Artist live/work studios
- Existing Auto Repair facilities
- Existing Automotive Sales (New and Used)
- Recreational Facilities

### Accessory Uses in Mixed-Use Buildings

- Schools and Training Facilities (Such as public and private universities; job training)
- Business and Professional Office (Above Retail only)
- Retail, and Business Services. (Such as those found in the Township's BH zone with exception to new and used motor vehicle sales and repair services.)
- Restaurant and Cafes
- Health and fitness facilities
- Day Care (Family and Child)

### Conditional Uses-

- 1) Stand alone Business and Professional Office
- 2) Stand alone Retail, and Business Services

### 3) Stand alone Health and fitness facilities

#### ~Conditioned Upon( for all above Conditional Uses):

- Property fronting North Olden Avenue
- Site Plan coordination with adjacent properties
- Sign, landscaping, streetscape and lighting Standards are met.
- On-site Pedestrian Circulation plans are submitted

### 4) Expansion of existing Auto-related land uses. (No new uses of these types permitted.) Conditioned Upon:

- A 50' foot buffer that includes a minimum of 10' foot wide vegetative buffer (consisting of trees, shrubs and other woody species), from the top of the Shabakunk Creek bank as per FEMA maps. Public access easements may also be required.
  - Agreements with adjacent land uses to share access and functional use of parking, loading and storage where appropriate and feasible.
  - The interface with residential uses, current and envisioned, is addressed through design features that allow seamless integration and/or buffering.
  - Ensure that sidewalks and streetscape amenities are installed or can be installed upon completion of the applicable roadway design.
  - When necessary, storage of disabled vehicles shall be fully screened from public view and shall maintain a 75" buffer from the Shabakunk Creek. Salvage yards are not permitted to expand.
  - Sign and lighting Standards are met.
- 5) Uses Identified in Township Land Development Ordinance 215-35 (11 and 12)
  - 6) Drive Thru services are prohibited unless meeting the conditions set forth in **Building Placement and Design** section.



**ILLUSTRATIVE ONLY:** The section of Olden Avenue within Subarea #2 is plagued by incompatible design approaches to the parcels along it. The Red dots are curb cuts that could be eliminated in favor of shared driveways in Green w/yellow outline when new development is proposed. In essence, the amount of curb-cuts can be cut in half thus improving access and circulation along the Avenue.

Another important point worth considering is the "sea" of asphalt and lack of inter-connectiveness. The "Subarea #2 Conceptual Massing" diagram graphically depicts how concepts of this Plan can be utilized to create synergistic development.



*Deleterious land uses, particularly those along the Shabakunk Creek, have outstanding environmentally friendly development potential.*



*Lack of a uniform streetscape and design standards has led to a disorganized hodgepodge of signage.*  
 Photo by Cathren Kull



*Pedestrian use of the Avenue is hazardous at best.*



*A goal of the plan is to facilitate redevelopment of land that has sat vacant for years such as the former DEP offices along Prospect Street*



### Subarea #2 Conceptual Massing Plan

The Area #2 Conceptual Massing Plan envisions ONE way the area could eventually develop under the provisions of this Plan. The one depicted here utilizes existing buildings and allows for cross-access agreements and reduced curbcuts. New streets and private drives, that coordinate with the Conceptual Road Network identified in this document, are envisioned to assist businesses with shipping and receiving needs as well as create a walkable environment.

### Bulk Standards

#### a. Principal Building.

- 1) Lot area: 22,500 sq. ft. - minimum
- 2) Build to Line:
  - i- OUTSIDE CORE AREA: Build to Line: 20' feet (with no parking in front). 50' build to for projects with front yard parking and landscaping in accordance with applicable Township parking regulations.
  - ii- WITHIN CORE AREA: New buildings shall be placed 10' feet minimum - 15' feet maximum from the front property line in the CORE area and on Corner lots (where all property lines on streets are front property lines), including Prospect Street,

and all new roads identified on the Conceptual Road Network depicted in Map 6. A minimum of 45% of the frontage must be built-to with all landscaping requirements mandatory as it relates to the parking facilities that may be located in the front yard.

*For Uses seeking to rehabilitate pre-existing non-conforming structures ETRA may recommend to the Planning Board a waiver of the Build-to requirement. However, Architectural Elements of the proposal will be subject of review and approval in accordance with the standards identified in the Design Standards Section of this Plan.*

- 3) Side yard: 10 feet minimum (CORE is permitted 0 lot lines where appropriate.)

- 4)Rear yard: 10 feet minimum
- 5)Height: 1 ½ stories or 20’ minimum. CORE Area: 2 stories or 25’ minimum, 4 ½ or 45’sstories maximum. 6 ½ stories or 75’ maximum if parking is located under principal structure. *For Uses seeking to rehabilitate pre-existing non-conforming structures ETRA may recommend to the Planning Board a waiver of the Build-to requirement. However, Architectural Elements of the proposal will be subject of review and approval in accordance with the standards identified in the Design Standards Section of this Plan.*

**b. Lot Coverage.**

- 1) Total impervious coverage: 75% maximum for all property adjacent to Shabakunk Creek.
- 2) Additional coverage up to 10% may be granted if offset by contributions to other stream improvements or innovative urban stormwater design features.

**c. Floor Area Ratio:**

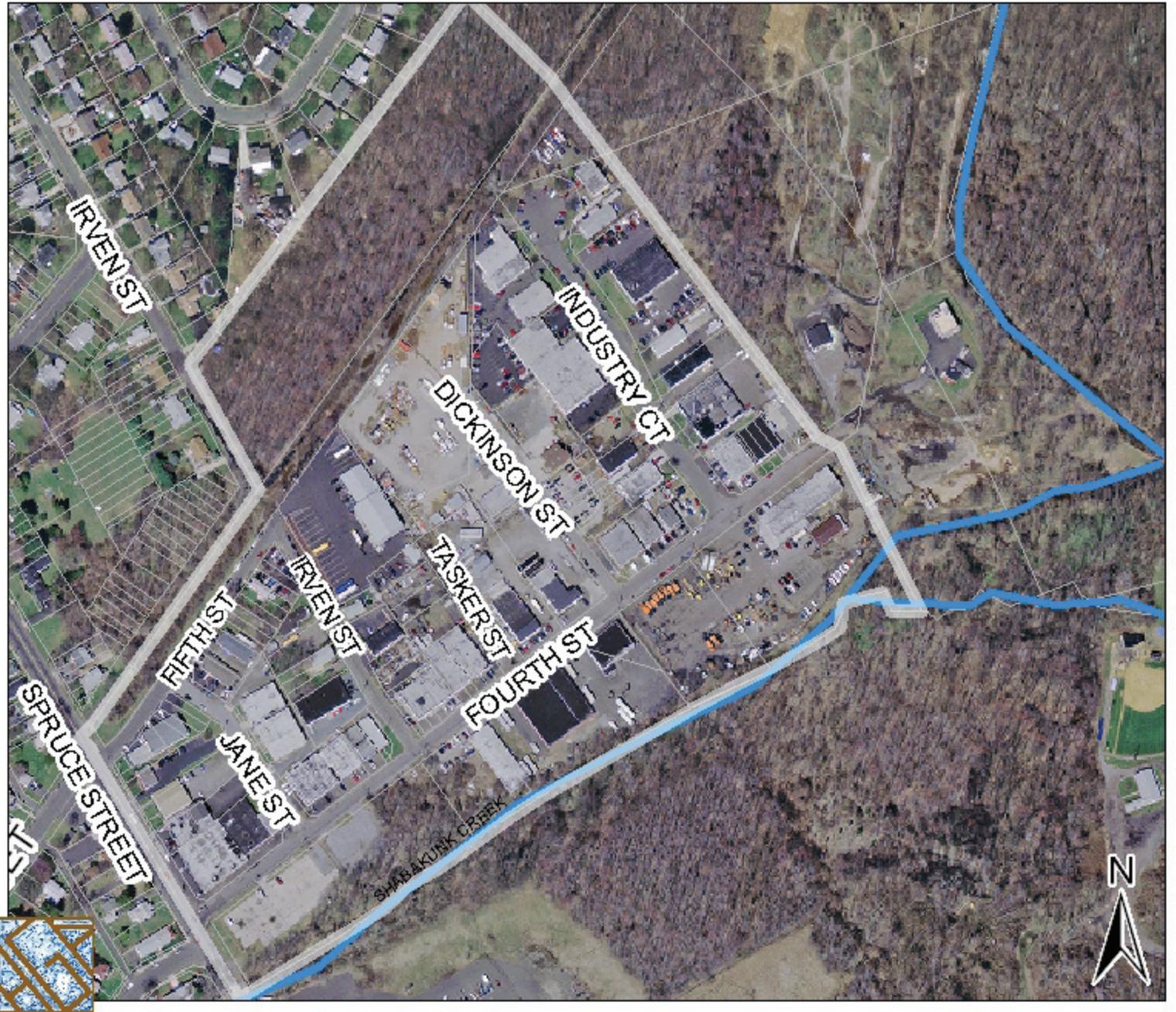
3.0 maximum for Mixed use in the CORE Area. Outside of CORE 0.5 single-use; 1.25 for mixed-use.

**Building Placement and Design**

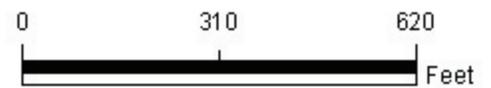
- Orient buildings to the street to create a strong pedestrian environment through unified streetscape improvements, giving high priority to pedestrian access both to and through the site.
- All multi-family residential and institutional land uses must front on a public street (new or existing). “Eyes on the street” development techniques must be demonstrated.
- Landscape all street connections to adjacent neighborhoods.
- For all development adjacent to existing and proposed parks, open space, and public land uses either within Ewing Township and the City of Trenton will incorporate site design practices that locates the site’s required open space adjacent to the public parks network. Such space may be dedicated to the Township subject to acceptance.
- All development on a street (not alley), both public and private shall be treated architecturally as a front
- New development shall utilize the Conceptual Road Network depicted in Map-6. Create all new streets as extensions of the existing street network in the area, constructed according to Township specifications and be turned over to the Township upon build out.
- Drive-thru services are prohibited unless meeting the following conditions;
  - i. They existed concurrently with the existing use at the time of this Plan’s *adoption, Or,*
  - ii.On-site circulation, including poedestrian, as well

- as ingress and egress issues concerning adjoining roadways are satisfied by the Township Development Professional as well as the County Engineer, And,
- iii. Direct ingress and/or egress onto Olden Avenue may be restricted or limited. Primary egress from a drive-thru must be located along a sidestreet, such as 6th Street, 5th Street, or other street created as a means of implementing the roadway network of the Plan’s Transportation Section. Major routes, such as Parkside Avenue, Pennington Road, Arctic and Prospect Street may be similarly utilized, but the egress on these roadways must be located a minimum of 250’ from the intersection and subject to approval of the engineer of record for such roadway’s jurisdiction. Secondary ingress/egress points onto Olden may be considered so long as such point of access is shared by two (2) or more properties through easement. Reconfiguration of the curb-cut and parking may be necessary upon negotiation with subsequent development proposals by adjacent property owners.

- Reopen culverts and tunneled streambanks in association with redevelopment so long as it is long found to be of benefit to stormwater/water quality control. This shall not preclude construction of bridges or other crossings subject to DEP approval.
- For property adjacent to the Shabakunk Creek, use alternative paving materials such as “Grasspave TM” or other types of porous paving material agreed to upon negotiation of the Redevelopment Agreement or Planning Board approval and in consultation with the Township Engineer (refer to Page 45 for an illustration of Grasspave TM).
- Orient all buildings to the street to create a strong pedestrian environment.
- Require public access easements where appropriate for restaurants and residential uses.
- Blank street-walls are prohibited for all façades along street frontages or adjacent to public open and other public spaces.
- Capture and re-use greywater to the extent feasible.
- Connect and integrate with adjacent neighborhoods through stream crossings and/or access to greenways established and conceptualized by the Plan.
- Provide a Pedestrian Circulation Plan that meets goals of this Plan where appropriate.



**Subarea #3 Aerial Photo**



SUBAREA

## Subarea 3 - Spruce Street Small Business District

### Intent

Sub-area 3 is intended to remain predominantly light industrial in character, with a small area of retail businesses located between Fourth and Fifth Streets along both sides of Spruce Street. The intent of this Sub-area is to:

- Encourage existing businesses to grow and expand,
- Provide additional economic development within the Sub-area,
- Provide proper buffers to surrounding residential neighborhoods.

Economic development will require improved access. The extension of Fifth Street alongside the former trolley right-of-way will eliminate dead end streets and provide access to additional vacant land. More importantly, access to Whitehead Road Extension will also be facilitated through the Fifth Street extension. The Fifth Street area will also provide alternate routes of travel for bicycle, pedestrian and truck traffic properly separated throughout the entire area.

### Objectives

- Facilitate the growth of commercial and light industrial uses in this area while recognizing the need to protect and enhance environmental quality.
- Improve circulation by extending Fifth Street alongside the former trolley right of way to eliminate dead end streets and provide access to remaining vacant land. Ensure that adequate buffers to surrounding neighborhoods remain intact.
- Ensure proper traffic controls and provide pedestrian and bicycle access to Olden Avenue along the new Fifth Street extension and/or along the Shabakunk Creek as part of the Ewing/Lawrence Greenway initiative.
- Improve flood control along the Shabakunk Creek and reclaim floodplain land where existing uses degrade environmental quality

### Land Use

#### Principal Uses-

#### Frontage on Spruce Street Only:

- Retail, and Business Services. (Such as those found in the Township's BH zone with exception to new and used motor vehicle sales and repair services.)
- Day Care (Family and Child)

#### All Other Property except Spruce Street:

- Manufacturing, processing, producing, fabricating or warehousing operations
- Structures and uses devoted to research, experimentation or engineering involving scientific investigation, engineering study, project development and similar activities.
- Auto-related industries
- Offices and warehouses.
- Training facilities (such as gyms, obedience schools, etc.)
- Public facilities.

#### Conditional Uses

- Uses Identified in Township Land Development Ordinance 215-35 (10)

### Bulk Standards

#### Frontage on Spruce Street Only:

##### a. Principal Building.

- 1) Lot area: 17,500 sq. ft. - minimum
- 2) Build to Line: 20-25'
- 3) Side yard: each 5 feet - minimum
- 4) Rear yard: 10 feet - minimum
- 5) Height: 1-1/2 stories or 20' (3-1/2 or 35' upon Residential Conditional Use permit and/or Office located Above)

##### b. Accessory Building.

- 1) Distance to side lot line: 5 feet - minimum
- 2) Distance to rear lot line: 5 feet - minimum

##### c. Lot Coverage.

- 1) Total impervious coverage: 75% maximum. 65% for those properties adjacent to the Shabakunk Creek.
- 2) Additional coverage up to 10% may be granted for properties adjacent to the Shabakunk Creek if offset by contributions to other stream improvements such as, but not limited to, stream bank stabilization, habitat restoration, or increased flood storage.

##### d. Floor Area Ratio

- 1) 0.50 maximum. 1.0 maximum for Mixed-use Office Above retail.

#### All Other Property except Spruce Street:

##### a. Principal Building.

- 1) Lot area: corner lot 8,000 sq. ft. - minimum
- 2) Lot width: 80' feet - minimum
- 3) Setback 10-15 feet (unless otherwise noted)
- 4) Side yard each- 5 feet - minimum

- 5) Rear yard 5 feet minimum
- 6) Height: 1-1/2 stories or 20' not to exceed 25 feet
- 7) When abutting a residential district, side and rear yard requirements shall conform to requirements of applicable adjacent residential district.

**b. Accessory Building.**

- 1) Distance to side lot line 5 feet minimum
- 2) Distance to rear lot line 5 feet -minimum
- 3) Height: 1 story not to exceed 15 feet -maximum

**c. Lot Coverage.**

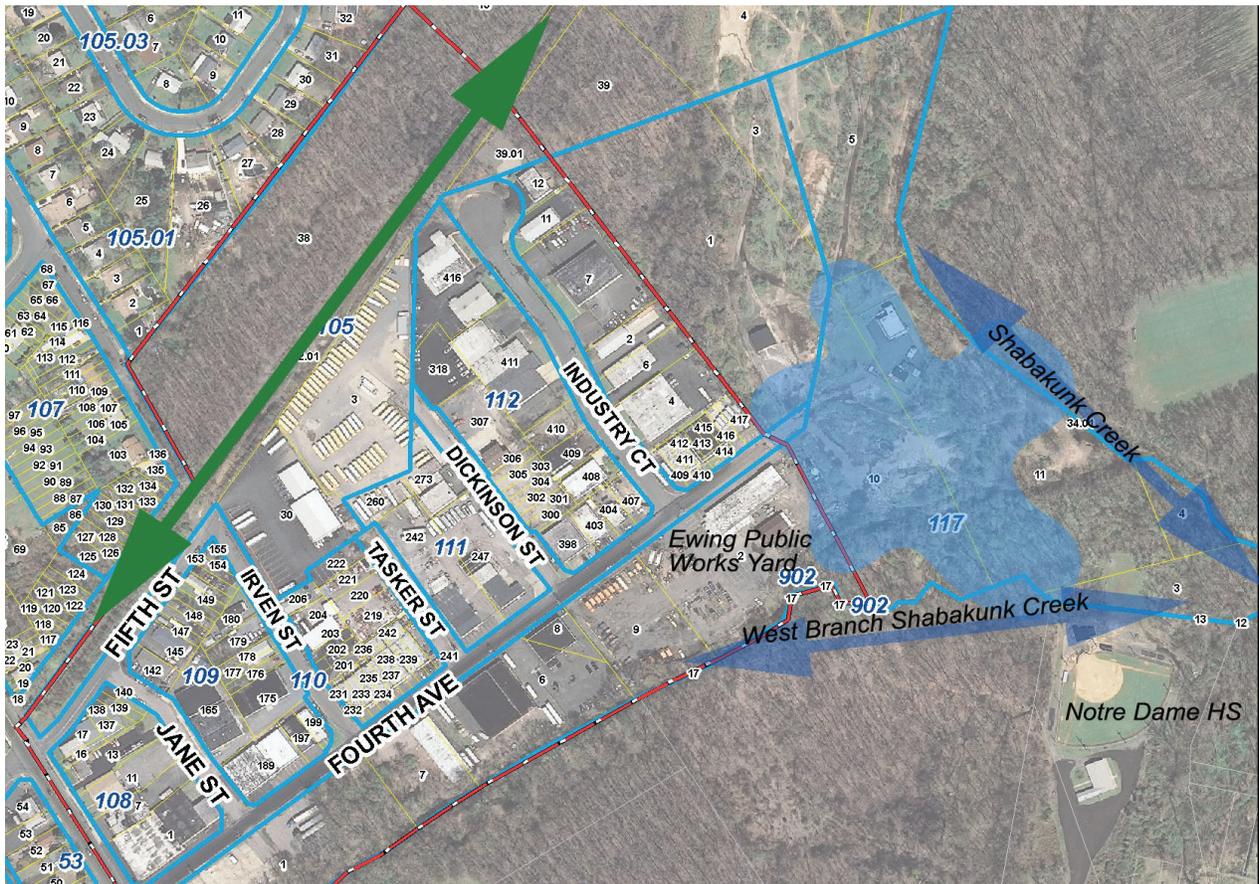
- 1) Total impervious coverage: 75% maximum. 65% for those properties located adjacent to the Shabakunk Creek.
- 2) Additional coverage up to 10% may be granted for properties located adjacent to the Shabakunk Creek if offset by contributions to other stream improvements such as, but not limited to stream bank stabilization, habitat restoration, or flood storage.

**d. Floor Area Ratio**

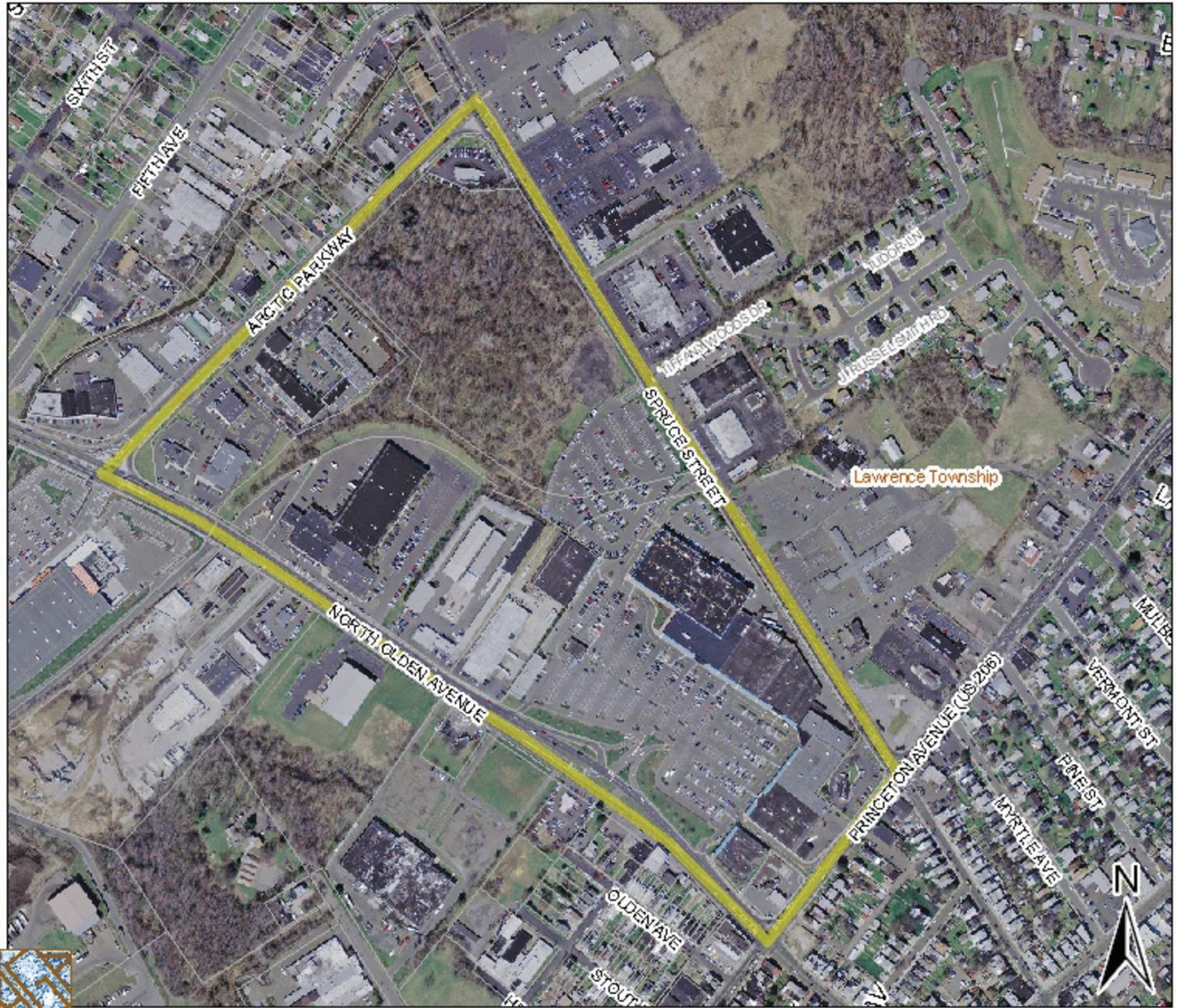
- 1) 0.50 maximum.

**Building Placement and Design**

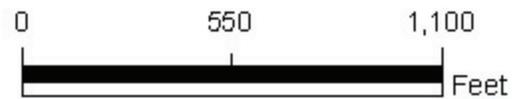
- Provide street trees of types and in locations specified by the Township Planning Board in consultation with the Environmental Commission.
- Establish and maintain vegetated buffers of landscaped berms with deciduous and evergreen landscaping with a minimum 50' feet width adjacent to residential uses and zones.
- For properties along 4th Street and Industry Court, design redevelopment reduce stormwater run-off, reclaim flood plain land, improve flood control and environmental quality of the Shabakunk Creek.
- For property adjacent to the Shabakunk Creek, use alternative paving materials such as "Grasspave TM" or other types of porous paving material agreed to upon negotiation of the Redevelopment Agreement or Planning Board approval and in consultation with the Township Engineer.
- Drive thru services are prohibited



*The Plan seeks to improve business conditions by improving flood conditions and an improved the quality of life throughout... In Sub-area 3, two branches on the Shabakunk meet and opportunities to improve regional flood condition exist...*



**Subarea #4 Aerial Photo**



# SUBAREA

## Subarea 4 - Mercer Crossings District

### Intent

Sub-area 4 consists of the entire block bounded by Arctic Parkway, Olden Avenue, Spruce Street and Princeton Avenue. Redevelopment and rehabilitation opportunities will be encouraged through the design of cohesive retail centers with controlled access from North Olden Avenue or Arctic Parkway, redesigned parking, and landscaping along all street frontages.

The Sub-area is located adjacent to the Trenton Farmer's Market, Halo Farms, and the North Trenton/ St Hedwig's Area; safe and efficient walking access from the surrounding neighborhoods is a key component to economic development success. Coordinating with the City of Trenton and with Lawrence Township will be key.

Economic development opportunities will be increased by using the remaining segments of abandoned rail right-of-way that effectively connects Olden Avenue, Spruce Street, and Arctic Parkway. The potential to create additional road frontage will foster additional economic development opportunities. An environmental assessment may also be undertaken to determine the extent and value of wetlands and other lands for development that these new street could unlock. Regional flood control and/or public park space will also be considered.

Development of Sub-area 4 parcels should be coordinated in design, particularly with respect to the objectives of Mercer Crossings initiative. More efficient utilization of land along both side of Spruce Street will facilitate a more pedestrian friendly design approach that encourages the traditional retail centers to be upgraded into "lifestyle" centers. The area's characteristics provides the opportunity to include mix of uses such as major department stores, entertainment centers, smaller retail stores well as some limited residential opportunities. Additionally, new uses will be oriented to the street and projects will be design to create a strong pedestrian environment, both within the site and outside of it, thus providing safe and convenient pedestrian access throughout.

### Objectives

- Develop remaining segments of abandoned rail right-of-way and logical adjacent parcels into new points of access and economic development opportunities.
- Improve integration with the Trenton Farmers Market area of Lawrence Township in order to provide better access for the high percentage of walkers coming from North Trenton neighborhoods.
- Create strong retail environment through design that not only attracts additional retail and office space opportunities but enhances the quality of life for the surrounding area as well.
- Develop a stronger road network that provides options for motorists and pedestrians to utilize other routes of access with a focus on the reduction of vehicular congestion and pedestrian conflicts. New roads will coincide with the potential extension of Whitehead Road and other circulation initiatives related to the ULI "Mercer Crossings" study objectives and coordinated with subsequent Mercer County studies.

### Land Use

#### Permitted Uses

- Lifestyle Shopping Centers (Shopping centers that promote pedestrianism and contain provisions such as outdoor cafes, public gathering spaces, covered walkways, etc.)
- Retail, and Business Services. (Such as those found in the Township's BH zone with exception to new and used motor vehicle sales and repair services.)
- Restaurants



*The ULI "Mercer Crossings" Report revealed great potential for the Capital Plaza shopping center. In-fact, redesigned, the Plaza can be a linchpin that connects the farmer's Market to the redevelopment potential of Area #5*

- Office; executive, administrative, educational or professional and medical services.
- Mixed use buildings
- Existing Lumber and Hardware supplies

#### Accessory Uses

- Residential above commercial (*Not permitted at ground level*)
- Retail and business services
- Banks and Fiduciary Institutions
- Health and Fitness Facilities
- Restaurants and Cafes
- Public Uses (Community, recreation, human services)
- Day Care
- Schools and Training Facilities

#### Conditional Uses

- 1) Uses Identified in Township Land Development Ordinance 215-35 (11 and 12)
- 2) Drive Thru services are prohibited unless meeting the conditions set forth in Building Placement and Design section.

#### Bulk Standards

##### a. Principal Building.

1. Lot area: 2 acres - minimum
2. Build to Line:
  - i) Along Olden Avenue, and Spruce Street, 30 feet minimum - 35' feet maximum from the front property line.
  - ii) Along Intersections and all new streets. 10 feet minimum - 15' feet maximum
  - iii. For Uses seeking to rehabilitate pre-existing non-conforming structures ETRA may recommend to the Planning Board a waiver of the Build-to requirement. However, Architectural Elements of the proposal will be subject of review and approval in accordance with the standards identified in the Design Standards Section of this Plan.
3. Side yard: each 5 feet - minimum
4. Rear yard: 10 feet - minimum
5. Height: 3 ½ stories or 35' maximum, 4 ½ stories or 55' where parking is located under principal structure. For Uses seeking to rehabilitate pre-existing non-conforming structures ETRA may recommend to the Planning Board a waiver of the Build-to requirement. However, Architectural Elements of the proposal will be subject of review and approval in accordance with the standards identified in the Design Standards Section of this Plan.

##### b. Accessory Building.

1. Distance to side lot line: 5 feet - minimum
2. Distance to rear lot line: 5 feet - minimum

##### c. Lot Coverage.

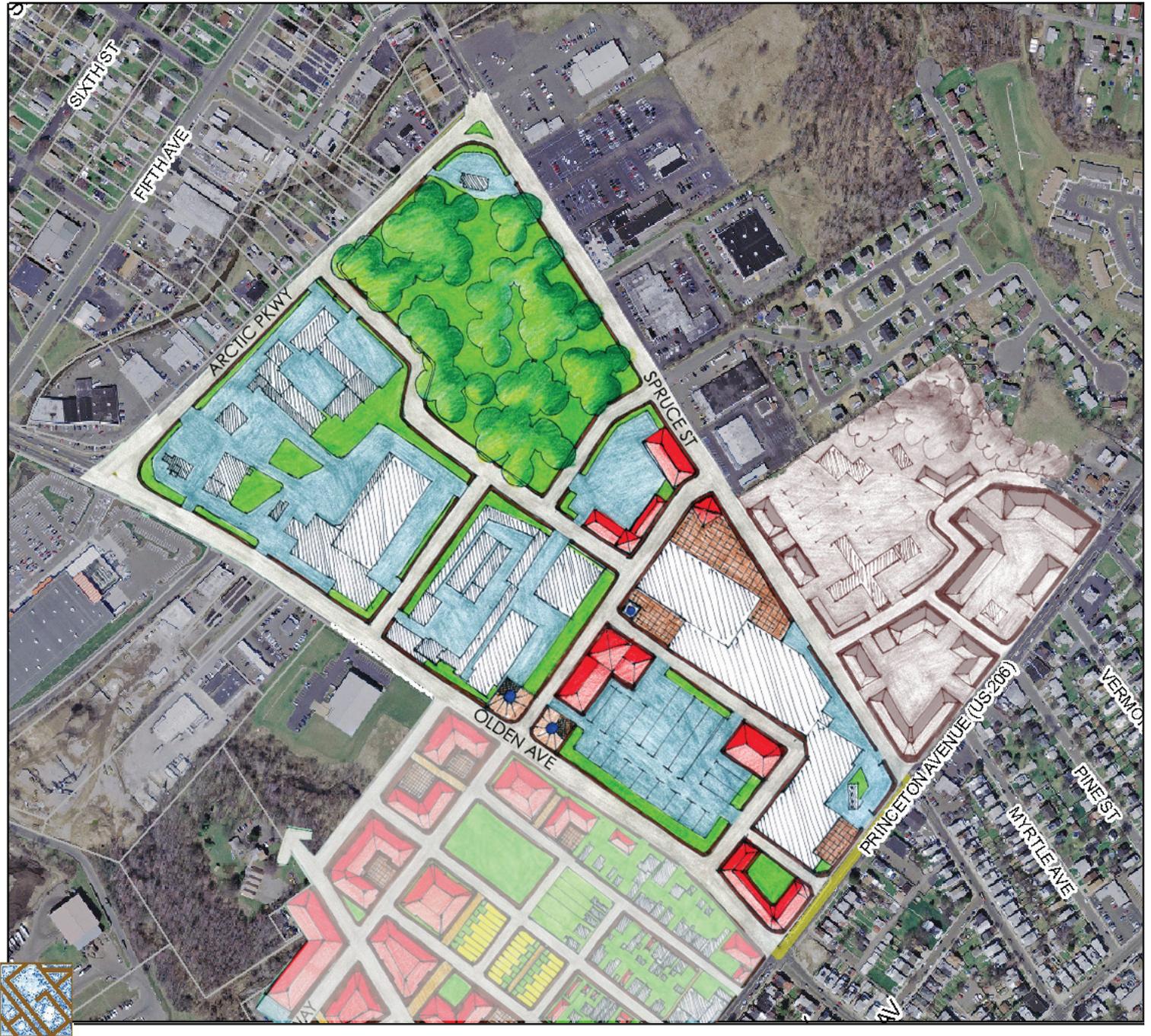
- 1) Total impervious coverage: 75% maximum. 65% for those properties located adjacent to the Shabakunk Creek.
- 2) Additional coverage up to 10% may be granted for

properties located adjacent to the Shabakunk Creek if offset by contributions to other stream improvements such as, but not limited to stream bank stabilization, habitat restoration, or flood storage.

##### d. Floor Area Ratio: 2.0 maximum

#### Building Placement & Design

- Orient buildings to the street to create a strong pedestrian environment through unified streetscape improvements giving top priority to pedestrian access both to and through the site.
- Drive-thru services are prohibited unless meeting the following conditions;
  - i. They existed concurrently with the existing use at the time of this Plan's adoption, Or,.
  - ii. On-site circulation, including pedestrian, as well as ingress and egress issues concerning adjoining roadways are satisfied by the Township Development Professional as well as the County Engineer, And,
  - iii. Direct ingress and/or egress onto Olden Avenue may be restricted or limited. Primary egress from a drive-thru must be located along a sidestreet, such as 6th Street, 5th Street, or other street created as a means of implementing the roadway network of the Plan's Transportation Section. Major routes, such as Parkside Avenue, Pennington Road, Arctic and Prospect Street may be similarly utilized, but the egress on these roadways must be located a minimum of 250' from the intersection and subject to approval of the engineer of record for such roadway's jurisdiction. Secondary ingress/egress points onto Olden may be considered so long as such point of access is shared by two (2) or more properties through easement. Reconfiguration of the curb-cut and parking may be necessary upon negotiation with subsequent development proposals by adjacent property owners.
- Blank streetwalls are prohibited for all façades along street frontages.
- Connect and integrate with adjacent neighborhoods in accordance with a Pedestrian Circulation Plan consistent with the Goals of this Redevelopment Plan.
- Orient buildings to the street to create a strong pedestrian environment.
- Provide landscaped buffer of at least 30' feet in width around all loading areas
- All development will capture and re-use greywater to the extent feasible.
- All development will provide stormwater controls through the creative use of natural wetland areas as deeded public open space and/or vegetative buffers where deemed appropriate. Design will be integrated into the overall development concept.

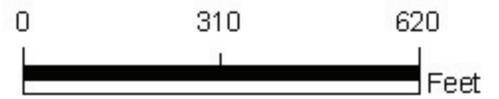


#### Area #4 Conceptual Massing Plan

*Extending Calhoun Street and the surrounding street grid will enable this area to be an economically viable and attractive place that will take advantage of the surrounding marketplace and create better opportunities for all communities in the Mercer Crossings area. Even the Farmer's market, which underutilizes its potential could not only be a magnet but benefit from a New approach.*



**Subarea #5 Aerial Photo**



SUBAREA

## Subarea 5 - Stout Avenue Neighborhood District

### Intent

Subarea 5 consists of the area along Olden, Stout and Heath Avenues south of North Olden Avenue and west of Princeton Avenue. This Subarea consists primarily of residential row houses with mixed use (commercial uses with upper floor residential units) fronting Princeton Avenue and the Pierce Rubber Factory located at 1500 Heath Avenue.

The rehabilitation of existing residential uses will be encouraged while providing a variety of additional housing options, including the adaptive reuse of industrial sites. The opportunity to expand this neighborhood by taking advantage of its strategic location in the Redevelopment Area is tremendous. Given the neighborhood's access to existing parks and open space, Brownfield sites and underutilized property make for prime redevelopment opportunities.

Redevelopment of this area will also require strong buffers between residential land uses and the industrial areas within subarea 6.



*Vacant Parcels along the Avenue hinder the progress of the Area*



*Caroline Stokes DayCare Center*

### Objectives

- Rehabilitate the existing neighborhood through the Township's Housing Rehabilitation program proposed in the Fair Share Housing Plan.
- Provide new in-fill housing development in keeping with the neighborhood's unique character.
- Provide options for the reuse of commercial and industrial property that improve the quality of life both within the subarea and contribute to surrounding neighborhoods.
- Promote mixed-use development in a manner that strengthens existing neighborhoods and draws on existing community assets.
- Integrate additional park space and enhancement of existing spaces for passive and active uses. Improve community access to these areas.
- Work with the City of Trenton to stabilize the entire region.

### Land Use

The mixed-use properties along Princeton Avenue are particularly suited for the Sub-area. Uses incompatible with adjacent residential neighborhoods however, such as auto repair garages, used car lots, auto salvaging or other commercial or industrial uses that are better suited to light industrial zones, would become legal non-conforming uses and therefore encouraged to be rehabilitated/ redeveloped into neighborhood oriented mixed-use structures.

### Permitted Uses

- Multi-family residential
- Institutions of higher learning including but not limited to related student housing
- Private and Public schools and satellite learning centers
- Existing Light Industry and automotive related businesses

### Conditional Uses

1) *One and Two family Residential and Townhomes.*

#### *Conditioned Upon:*

- Property located adjacent to uses similar in nature or architectural compatibility.
- Property located on Heath, Stout, Brian and/or Olden Streets or within a one (1) block radius of said streets.

2) *Mixed-use (Residential, and/or Office Professional*

*ABOVE retail). Conditioned Upon:*

- Property fronting North Olden Avenue or Princeton Avenue or future extensions of Calhoun Street
- All other standards and intent of Sub-area #5 are met.

3) *Religious Institutions and Houses of Worship. Conditioned Upon:*

- All ancillary uses require separate use permits (office, classrooms, etc.)
- Establishing a Pedestrian Circulation Plan and installation sidewalks on all street frontages
- Property shall front any existing or new street
- Parking is prohibited in the front yard area

4) *Expansion of existing Auto-related land uses and Light Industry (No new uses of these types permitted.) Conditioned Upon:*

- Agreements with adjacent land uses to share access and functional use of parking, loading and storage where appropriate and feasible.
- The interface with residential uses, current and envisioned, is addressed through design features that allow seamless integration and/or buffering.
- Ensure that sidewalks and streetscape amenities are installed or can be installed upon completion of the applicable roadway design.
- When necessary, storage of disabled vehicles shall be fully screened from public view. Salvage yards are not permitted to expand.
- Sign and lighting Standards are met.

## **Bulk Standards**

### **a. Principal Buildings**

1. Lot Area

i) Single Family:

Minimum: 40 feet width and 4,000 square feet in lot area

Maximum: 50 feet in width and 5,000 square feet in lot area.

ii) Two-Family:

Minimum: 60 feet and 6,000 square feet in lot area.

Maximum: 70 feet in width and 7,000 square feet in lot area.

iii) Mixed-use: 22,500

iv) Multi-family Residential: 2 acres, Additionally,

2. Build to Line:

i) Front setback: 30 feet minimum - 35' feet maximum from the front property line along Olden Avenue.

ii) Front setback: 10 feet minimum - 15' feet

maximum at intersections and on all new streets.

iii) Mixed-use development: Front setback: 0 feet minimum - 10' feet maximum from the front property line along Olden and Princeton Avenues. Maintain a minimum of 15' within the right-of-way for streetscape and sidewalk provisions and streetscape.

iv) Prevailing setbacks may be averaged for in-fill and development on the same or intersecting streets.

3. Side yard: each 5 feet - minimum

4. Rear yard: 10 feet - minimum

5. Height: 3 ½ stories or 35' maximum, 4 ½ stories or 55' where parking is located under principle structure.

### **b. Accessory Building.**

1. Distance to side lot line: 5 feet - minimum

2. Distance to rear lot line: 5 feet - minimum

### **c. Lot Coverage.**

1) Total impervious coverage: 60% maximum.

2) Additional coverage up to 10% may be granted for properties located adjacent to the Shabakunk Creek if offset by contributions to other stream improvements such as, but not limited to stream bank stabilization, habitat restoration, or flood storage.

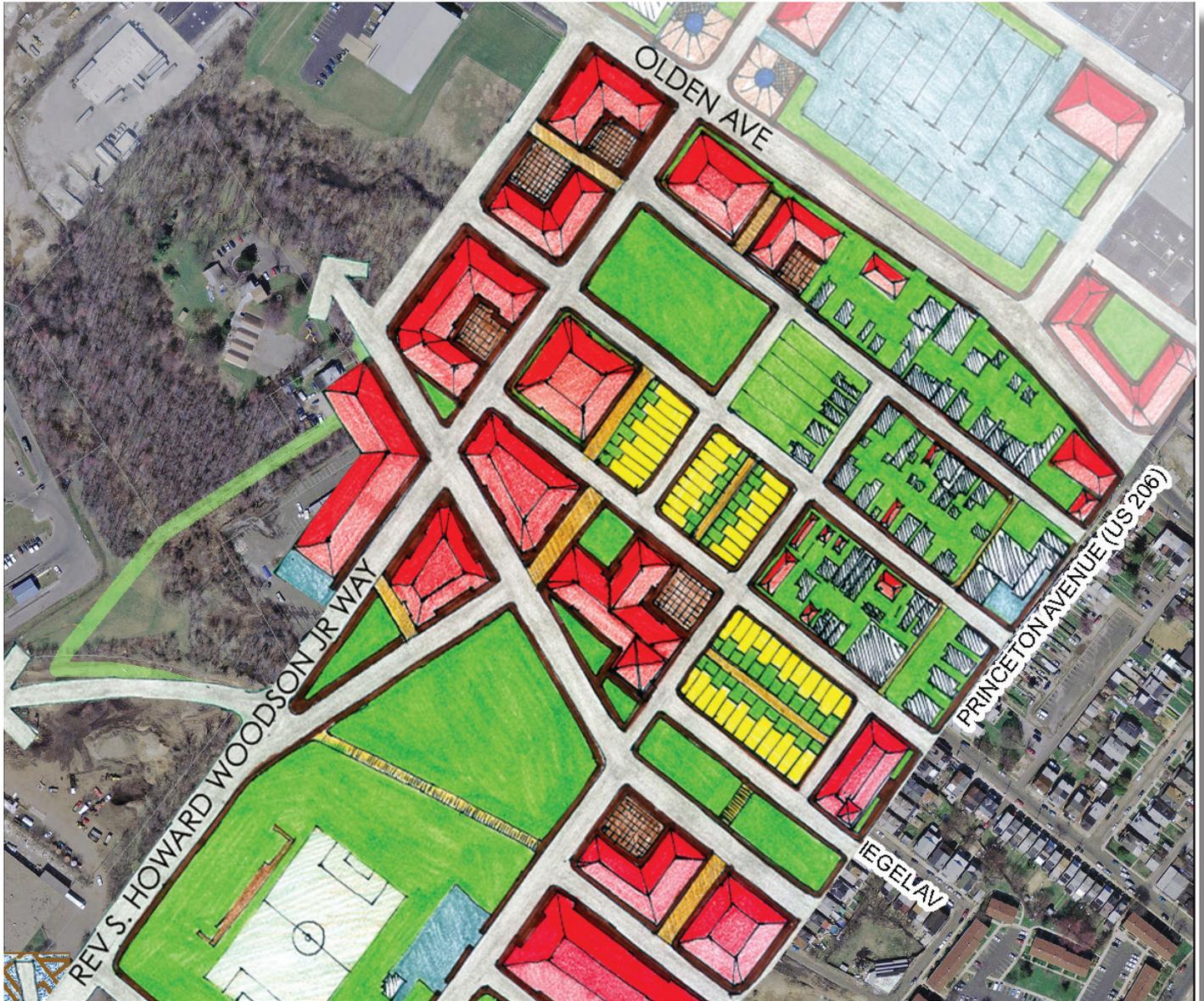
### **d. Floor Area Ratio**

1. Residential in-fill development. Single-Family and Two-family subject to the FAR Sliding Table in Appendix C.

2. One point five (1.5) maximum, 1.0 maximum for multi-family.

## **Building Placement and Site Design**

- Orient buildings to the street to create a strong pedestrian environment through unified streetscape improvements, giving top priority to pedestrian access both to and through the site.
- All multi-family residential and institutional land uses must front on a public street (new or existing). "Eyes on the street" development techniques must be demonstrated.
- Landscape all street connections to adjacent neighborhoods.
- For all development adjacent to existing and proposed parks, open space, and public land uses either within Ewing Township and the City of Trenton will incorporate site design measures to include required open space as additional, publicly accessible park space that may be dedicated to the Township subject to acceptance. All open space (new and existing) must be accessible to the public
- All development on a street, both public and private shall be treated architecturally as a front
- Create all new streets as extensions of the existing street network in the area, constructed according to Township specifications and be turned over to the Township upon build out.
- Capture and re-use greywater to the extent feasible.



**Area #5 Conceptual Massing Plan**

*This subarea is in a unique position whereby underutilized assets such as Father Rocco Park, the Caroline Stokes Day Care Center and surrounding thriving neighborhoods can be brought together in a way that creates a solid livable community. Circulation improvement such as Calhoun Street Extension (Rev Howard Woodson Jr Way) can create walkable streets while improving traffic flow*



## Subarea 6 - Stokes/Calhoun Regional Commercial/Industrial District

### Intent

This Sub-area will create new redevelopment parcels for a variety of commercial and industrial uses consistent with the current uses in the area. However, the future of these uses remains in question as the industrial sector in the United States continues to weaken with exception of flex industrial space with high-capacity truck access. As has been witnessed in other areas of the State, heavy industrialized areas can change toward uses vastly different from today's. The Township possesses the ability to utilize Brownfield remediation laws to create additional development opportunities as new taxes generated from redevelopment are able to be applied toward cleanup costs. As circumstances progress, ETRA, the Ewing Township Planning Board and Ewing Township Council may seek to revisit this matter to determine the most appropriate reuse of the industry intensive portions of this Sub-area.

Notwithstanding the long-term future for this area, the Plan seeks to allow industrial uses within this Sub-area to operate and improve their properties and expand in a manner that is respectful of surrounding neighborhoods. Because of the history of industrial uses, land filling and auto salvaging within this Sub-area, some redevelopment parcels may require some remediation of soil contamination.

Properties that front North Olden Avenue will also be permitted to redevelop in a manner that works to unify both side of the Avenue and allowing access from adjacent neighborhoods both existing and proposed.

### Objectives

- Create new redevelopment parcels, which facilitate short-term growth and buffers surrounding the existing industrial areas from residential neighborhoods while recognizing the long term potential for change.
- Allow industry to thrive, while ensuring adequate buffers to residential neighborhoods are provided.
- Create additional opportunities for Open Space by reclamation of land that also works to improve environmental quality and stormwater reduction for the entire region.

### Land Use

Because of the history of industrial uses, permitted land uses in Sub-area 6 will include large site industrial uses such as recycling, warehousing and distribution, light industrial and related commercial uses so long as the property does not front Olden Avenue or Calhoun Street. In addition, the development of "flex-space" developments that combine manufacturing, distribution, retail, office and hotel uses, including extended stay business hotels, are encouraged.

Additionally, property that directly fronts North Olden Avenue or Calhoun Street will be permitted to redevelop in a manner that is more consistent with adjacent Sub-area 4.

### Permitted Uses

#### *1) Lots that Front Olden Ave or Calhoun Street*

- Lifestyle Shopping Centers (Shopping centers that promote pedestrianism and contain provisions such as outdoor cafes, public gathering spaces, covered walkways, etc.)
- Mixed use buildings
- Existing Lumber and Hardware supplies
- Retail, and Business Services. (Such as those found in the Township's BH zone with exception to new and used motor vehicle sales and repair services.)
- Restaurants

#### *2) Lots without frontage on Olden Ave or Calhoun Street (conditioned upon proper buffering from all residential uses and streets)*

- Manufacturing, processing, producing, fabricating~ or warehousing
- Research. Experimentation or engineering involving scientific investigation, engineering study, project development and similar activities not involving the manufacturing, sale, processing, warehousing, distribution, or fabrication of material, products or goods except as incidental to the principal permitted uses.
- Office; executive, administrative, educational or professional and medical services.
- Hospital for small animals (dogs, cats, and the like) including kennel, provided yards are enclosed.
- Combinations of two (2) or more compatible uses permitted within one (1) unit.
- Public safety facilities.
- Planned developments of 5 acres or more, subject to Redevelopment Agreement.

### Accessory Uses

#### 1) For properties that Front Olden Ave or Calhoun Street (or future Calhoun Street Extension):

- Residential above commercial (Not permitted at ground level)
- Retail and business services
- Banks and Fiduciary Institutions
- Health and Fitness Facilities
- Restaurants and Cafes
- Public Uses (Community, recreation, human services)
- Day Care
- Any accessory use on the same lot which is customarily incidental to the principal permitted use.

#### 2) For properties without Frontage on Olden Ave or Calhoun Street, conditioned upon proper buffering from all residential uses and streets.

- Cafeteria, lunchroom, first aid and/or medical facilities operated by or for the employer for the exclusive use of employees or guests.
- Recreational areas for employees, indoors and outdoors.
- Garages and storage buildings.

### Bulk Standards

#### 1) For properties that Front Olden Ave or Calhoun Street (or future Calhoun Street Extension),

##### a. Principal Building.

1. Lot area: 2 acres - minimum
2. Build to Line:
  - i) Front setback for buildings along Olden Avenue: 50 feet maximum from the property line of the prevailing setback
  - ii) Front setback for buildings along all new streets or interior access drives: 10 feet minimum - 15 feet maximum from the front property line.
3. Side yard: each 5 feet - minimum
4. Rear yard: 10 feet - minimum
5. Height: 3 ½ stories or 40' maximum, 4 ½ stories or 55' where parking is located under principal structure.

##### b. Accessory Building.

1. Distance to side lot line: 5 feet - minimum
2. Distance to rear lot line: 5 feet - minimum

##### c. Lot Coverage.

- 1) Total impervious coverage: 70% maximum. 65% for those properties located adjacent to the Shabakunk Creek.

- 2) Additional coverage up to 10% may be granted for properties located adjacent to the Shabakunk Creek if offset by contributions to other stream improvements such as, but not limited to stream bank stabilization, habitat restoration, or flood storage.

##### d. Floor Area Ratio:

- 0.5 maximum. 2.0 maximum for mixed-use

#### 2) For properties without Frontage on Olden Ave or Calhoun Street,

##### a. Principal Building.

1. Lot area: 2 acres -minimum
2. Lot Width: 250 feet -minimum
3. Front yard: 75 feet -minimum
4. Side yard: one 35 feet -minimum
5. Side yard: total of two 100 feet -minimum
6. Rear yard: 75 feet -minimum□
7. Height: 3-1/2 stories or 35' (4 ½ stories or 55' with parking under principal use)

##### b. Accessory Building.

1. Distance to side lot line: 25 feet ~ minimum
2. Distance to rear lot line: 25 feet -minimum
3. Height: 1 story -not to exceed 15 feet -maximum

##### c. Lot Coverage.

1. Total building coverage shall not exceed thirty five (35%) percent of total lot area.
2. Total impervious coverage: 70% maximum for all property adjacent to Shabakunk Creek.
3. Additional coverage up to 10% may be granted if offset by contributions to other stream improvements or innovative urban stormwater design features.

### Building Placement & Design

- Orient buildings to the street to create a safe and convenient pedestrian environment through unified streetscape improvements giving top priority to pedestrian access both to and through the site for non-industrial uses.
- Blank streetwalls are prohibited for all façades along street frontages.
- Drive-thru services are prohibited unless meeting the following conditions;
  - i. They existed concurrently with the existing use at the time of this Plan's adoption and front Olden Avenue, Or,
  - ii. On-site circulation, including pedestrian, as well as ingress and egress issues concerning adjoining roadways are satisfied by the Township Development

Professional as well as the County Engineer, And,  
iii. Direct ingress and/or egress onto Olden Avenue may be restricted or limited. Primary egress from a drive-thru must be located along a sidestreet, such as 6th Street, 5th Street, or other street created as a means of implementing the roadway network of the Plan's Transportation Section. Major routes, such as Parkside Avenue, Pennington Road, Arctic and Prospect Street may be similarly utilized, but the egress on these roadways must be located a minimum of 250' from the intersection and subject to approval of the engineer of record for such roadway's jurisdiction. Secondary ingress/egress points onto Olden may be considered so long as such point of access is shared by two (2) or more properties through easement. Reconfiguration of the curb-cut and parking may be necessary upon negotiation with subsequent development proposals by adjacent property owners..

- Connect and integrate with adjacent neighborhoods where appropriate through stream crossings and/or access to greenways established and conceptualized by the Plan.
- For any properties adjacent to the Shabakunk Creek or tributaries use alternative paving materials such as "Grasspave TM" or other types of porous paving material agreed to upon negotiation of the Redevelopment Agreement or Planning Board approval and in consultation with the Township Engineer (refer to Page 45 for an illustration of Grasspave TM).
- Provide landscaped buffer of at least 30 feet is required around all loading areas
- Capture and re-use greywater to the extent feasible.

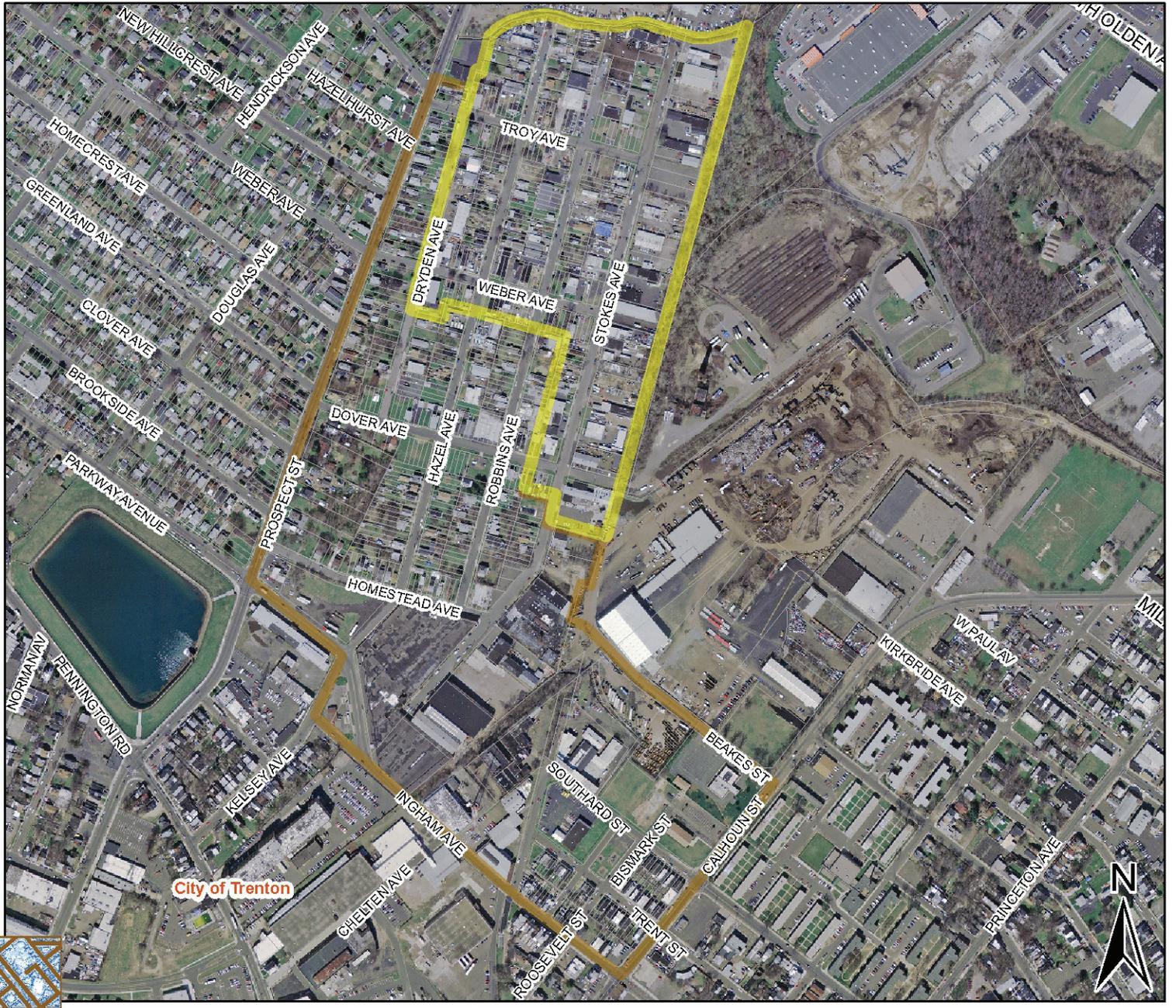
### **Additional Building Requirements**

The suggested redevelopment parcels in Sub-area 6 are adjacent to an area of low elevation that has been the most severely flooded area of Ewing Township. A continuing dialogue will be maintained between the Ewing Township Planning Board and the Mercer County Planning Director regarding the County's concern over new development within the Redevelopment Area having excessive lot coverage that will delay the improvement of flooding conditions down stream in Trenton. As such, it is a requirement of this Redevelopment Plan that in order to gain the additional density permitted in this plan, each redevelopment site shall provide excess capacity, or contribute to, regional stormwater detention design beyond what is required either in the Ewing Township

Land Development Ordinance or the New Jersey Residential Site Improvement Standards. Such allowances will be subject to negotiation within the redevelopment agreement and NJDEP regulations.

Furthermore, due to the Sub-area's proximity to residential land uses in Ewing and Trenton, landscaped buffers of 50' feet minimum in size must be developed and maintained to shield residential land uses and zoning from deleterious land uses. This includes landscaped berms with deciduous and evergreen landscaping, including sound walls if deemed appropriate. Compatible land uses will be integrated within these residential land uses. Specifically;

- a. All buildings and uses shall be served by public water and sewage and a lot may contain more than one (1) principal building.
- b. Yard Requirements Adjoining Residential Districts. On lots adjoining residential districts, no building shall be located closer fifty (50') feet from such district boundary and be buffered with a vegetative screen..
- c. Landscaping Along Streets, Roads or Highways. Each property shall be appropriately landscaped particularly (100') feet of any street intersection said distance to be measured from the intersection of the right-of-way lines at the corner affected and the closest point of such proposed driveway. No entrance or exit, at the curb line only shall be closer than twenty (20') feet from a side lot line. Entrance and exit sizes, locations and construction shall also be in accordance with requirements of the governmental agency' having jurisdiction over the facility upon which the permitted use has frontage.
- d. Loading Docks and Service Areas. No loading dock or service area may be on any street frontage. Provision for handling all freight shall be on those sides of any buildings which do not face any street or proposed streets.
- e. Outdoor Storage Areas. No use or accessory use shall be constructed to permit the keeping of articles, equipment, goods or materials in the open exposed to public view, adjacent residences or a residential district. When necessary to store or keep such materials in the open, the area shall be enclosed with a screen or buffer planting strip and be situated not closer than fifty (50') feet from a residential district line, public right-of-way or Greenway.
- f. Conservation. Existing woodlands, windbreaks and watersheds shall be preserved as far as the standards of conservation practice require.



**Subarea #7 Aerial Photo**

- #7a
- #7b

SUBAREA

## Subarea 7a & 7b - Prospect Park Districts

### Intent

Sub-area 7 is known as the Prospect Park section of Ewing Township. It is currently heavily developed for residential uses as it is for commercial uses, as the workforce for the local businesses traditionally lived in the same neighborhood. An exception to this land use pattern occurs along Prospect Street, where the properties fronting Prospect Street include Business Highway uses even though a majority of the properties are residential homes. Sub-area 7 lies within the “area in need of rehabilitation” designation.

### *Sub-area 7A- Prospect Park Small Business/Light Industrial*

North of Weber Avenue is intended to enhance the environment for business and business services. to reduce the impacts of noise, odors and other land use conflicts, residential landowners are encouraged to sell their properties to adjacent businesses. The Agency will be available to assist and encourage these property owners relocate to new in-fill housing opportunities on existing vacant lots within Prospect Park between Weber and Homestead Avenue (Sub-area 7B- Prospect Park Neighborhood.)

### *Sub-area 7B - Prospect Park Neighborhood*

The intent of Sub-area 7B is to provide a residential context that can facilitate mortgage financing for existing homeowners along Dryden, Hazel and Robbins Avenues while still allowing for commercial uses and to use property tax incentives to encourage uses as permitted in the Light Industrial zoning for the area along Dryden, Hazel and Robbins Avenues north of Weber Avenue.

The use of property tax incentives within Sub-area 7B to facilitate rehabilitation and new construction of housing within the area between Weber Avenue and Homestead Street will support the stabilization of the neighborhood while allowing the private marketplace room to continue transforming abandoned and underutilized industrial

properties in this area into a true residential neighborhood. Furthermore, the extension of the Sub-area boundary into the neighborhood bounded by Ingham and Calhoun Streets will work to improve the quality of life throughout the Sub-area. Connecting these two areas through street extensions and streetscape improvements is envisioned to complement industrial re-use efforts and make this area a better place to live, work and play.

### Objectives

#### *Sub-area 7A- Prospect Park Small Business/Light Industrial*

- Facilitate the expansion of commercial uses along Dryden, Hazel and Robbins Avenues north of Weber Avenue.
- Mandate better site design through increasing landscape provisions that work toward reducing the “heat island” effect and decreasing stormwater run-off.
- Improve access to and from industrial land uses while reducing the impact of truck traffic on surrounding neighborhoods.
- Capture and re-use greywater to the extent feasible.

#### *Sub-area 7B - Prospect Park Neighborhood*

- Encourage the rehabilitation of existing industrial uses in the area while promoting a neighborhood friendly design approach.
- Provide for new infill housing opportunities on existing vacant lots within Prospect Park between Weber and Homestead Avenue.
- Work with the Township’s Housing Plan to promote homeownership and neighborhood stabilization from Weber Avenue south through to the Ingham and Calhoun Street neighborhood.

## Land Use

### Permitted Uses—7A

- Manufacturing, processing, producing, fabricating or warehousing operations
- Research, experimentation or engineering involving scientific investigation, engineering study, project development and similar activities.
- Motor vehicular sales and services. (No junk or long-term storage or salvage)
- Office; executive, administrative, educational or professional and medical services.
- Warehousing.
- Public facilities.

### Permitted Uses—7B

- One and Two- family dwellings
- Residential Townhomes
- Multifamily residential (including 55 and older residents)
- Artists lofts
- Private and Public schools and satellite learning centers

### Conditional Uses in 7B

1) *Mixed Use (residential above retail and business services). Conditioned Upon location along key roadways intersections of;*

- Calhoun Street & Ingham
- Parkway & Prospect Street

2) *Light Industrial- In accordance with area 7A Permitted Uses above. Conditioned Upon;*

- No front yard storage of any kind.
- Side Yard storage shall be screen and landscaped.
- Vegetative Buffering from all residential land uses.
- Street trees and 5' minimum sidewalks.

3) *Religious Institutions and Houses of Worship.*

#### *Conditioned Upon:*

- All ancillary uses require separate use permits (office, classrooms, etc.)
- Meeting all off-street parking requirements
- Establishing a Pedestrian Circulation Plan and installing sidewalks on all street frontages

- Property shall front any existing or new street
- Parking is prohibited in the front yard area

## Bulk Standards

### -Area #7A

#### a. *Principal Building.*

1. Lot area: 8,000 square feet minimum
2. Lot Width: 80 feet -minimum
3. Build to Line: 10-15 feet
4. Side yard: each 5 feet - minimum
5. Rear yard: 10 feet - minimum
6. Height: 2 ½ stories or 40' maximum.

#### b. *Accessory Building.*

1. Distance to side lot line: 5 feet - minimum
2. Distance to rear lot line: 5 feet - minimum

#### c. *Lot Coverage.*

- 1) Total impervious coverage: 75% maximum. 65% for those properties located adjacent to the Shabakunk Creek.
- 2) Additional coverage up to 10% may be granted for properties located adjacent to the Shabakunk Creek if offset by contributions to other stream improvements such as, but not limited to stream bank stabilization, habitat restoration, or flood storage.

#### d. *Floor Area Ratio: 0.25 minimum; 0.5 maximum.*

### -Area #7B

#### *Single-family, Two-family and Townhouse*

#### a. *Principal Building.*

1. Lot area: 4,000 square feet minimum (6,000 square feet for two-family)
2. Lot Width: 40 feet -minimum (60' for two-family)
3. Front yard: 0-25 feet \*
4. Side yard: Five (5'). Twelve (12') feet aggregate
5. Rear yard: 25 feet -minimum
6. Height: 4-1/2 stories or 45' (5 ½ stories or 60' with parking under principal use)

#### b. *Accessory Building.*

1. Distance to side lot line: 5' feet ~ minimum
2. Distance to rear lot line: 5' feet -minimum
3. Height: 1 story -not to exceed 15 feet -maximum

*c. Lot Coverage.*

1. Total building coverage shall not exceed thirty-five (35%) percent of total lot area.
2. Total impervious coverage: 70% maximum for all property adjacent to Shabakunk Creek.
3. Additional coverage up to 10% may be granted if offset by contributions to other stream improvements or innovative urban storm water design features.

*d. FAR: See FAR Chart.*

\* Variation in Front yard is dependant on neighborhood character.

*Multi-family*

*a. Principal Building.*

1. Lot area: 22,000 square feet minimum
2. Lot Width: 80 feet -minimum
3. Front yard: 0-15' feet (stoops encouraged)
4. Side yard: 15' adjacent to like structures. ½ the height of the structure when adjacent to all other residential types.
5. Rear yard: 25 feet -minimum
6. Height: 4-1/2 stories or 45' (5 ½ stories or 60' with parking under principal use)

*b. Accessory Building.*

1. Distance to side lot line: 5' feet ~ minimum
2. Distance to rear lot line: 5' feet -minimum
3. Height: 1 story -not to exceed 15 feet -maximum

*c. Lot Coverage.*

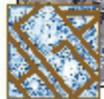
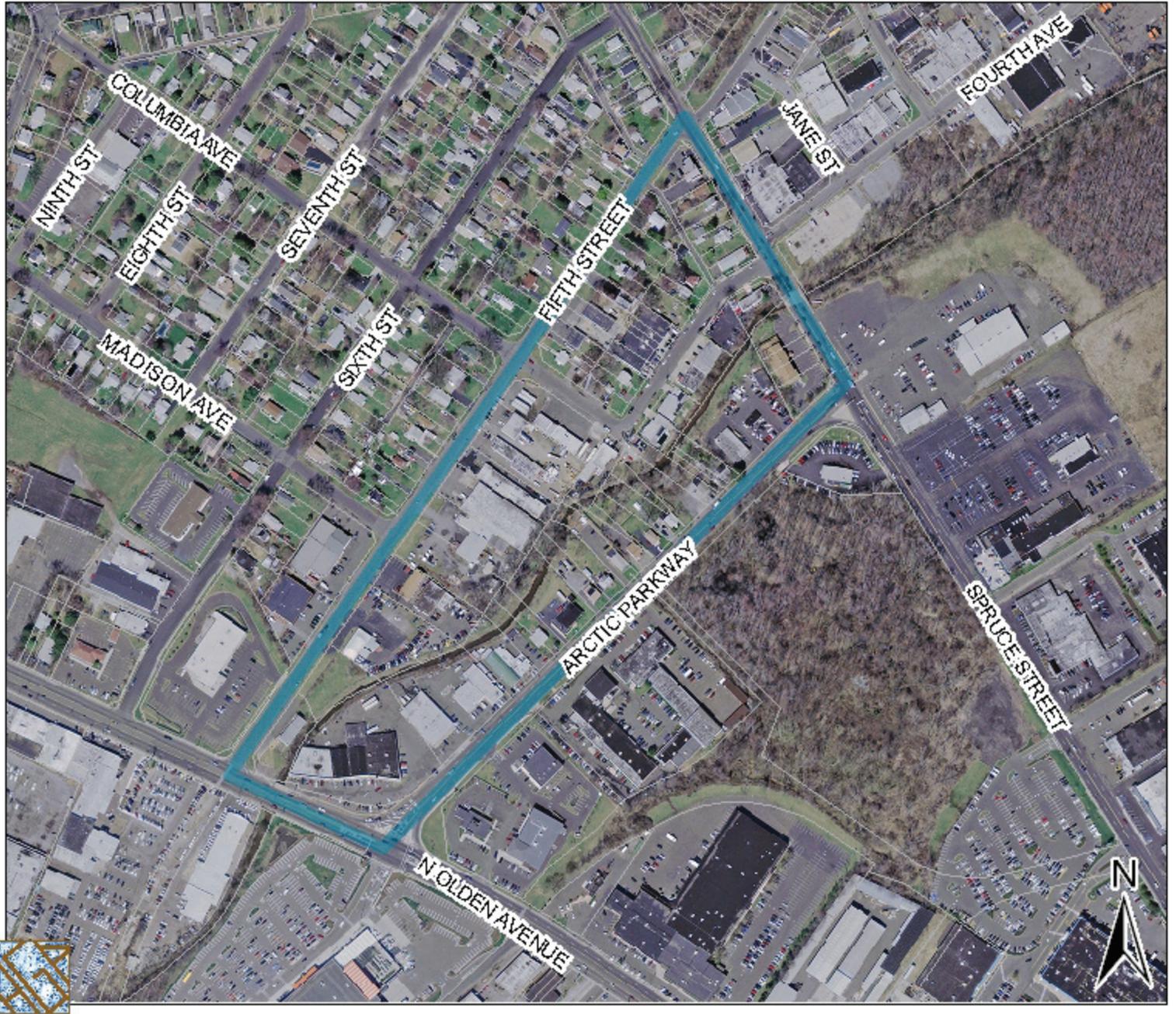
1. Total building coverage shall not exceed sixty (60%) percent of total lot area.
2. Total impervious coverage: 70% maximum for all property adjacent to Shabakunk Creek.
3. Additional coverage up to 10% may be granted if

offset by contributions to other stream improvements or innovative urban storm water design features.

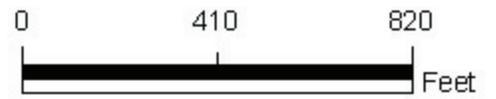
*d. Maximum FAR 1.0*

**Building Placement & Design**

- Property in Sub-area #7a shall maintain 30% of lot area for landscaping.
- For industrial property, establish and maintain vegetated buffers of 50' feet minimum from adjacent residential zones including landscaped berms with deciduous and evergreen landscaping.
- For property adjacent to the Shabakunk Creek use alternative paving materials such as "Grasspave TM" or other types of porous paving material agreed to upon negotiation of the Redevelopment Agreement or Planning Board approval and in consultation with the Township Engineer. (refer to Page 45 for an illustration of Grasspave TM)



Subarea #8 Aerial Photo



SUBAREA

## Subarea 8 Arctic Parkway Transitional Area

### Intent

The properties, and their semblance, in this Sub-area are unique. The Sub-area is bordered by Fifth Street, Spruce Street, Arctic Parkway and Olden Avenue. Traversing through the center of this Sub-area, which contains residential, industrial, retail and professional office land uses is the Shabakunk Creek. Given the nature of the areas that surround this block, as well as the flooding potential of the creek, this Sub-area will be treated differently than the other areas.

The intent of this Sub-area is to facilitate smooth transitions between the mixes of land uses that exist on all sides of it, particularly the residential neighborhoods. In addition, all development will provide additional shading of the Shabakunk Creek to complement the Township's water quality project and where feasible, will provide additional flood reduction measures.

### Objectives

- Improvement of flooding along the Shabakunk Creek and reclamation of the floodplain where land uses degrade environmental quality
- Encourage development patterns that facilitate retail sales of goods and services along Arctic Parkway and Fifth Street that recognize adjacent residential neighborhoods and commercial areas.
- Reduce curb-cuts along North Olden Avenue through shared access and parking arrangements.
- Create strong relationships to adjacent neighborhoods through streetscape and pedestrian access to shopping opportunities in both the Sub-area and adjacent Sub-areas.

### Land Use

#### Principal Uses

- Office, professional office, and research
- Retail, and Business Services. (Such as those found in the Township's BH zone with exception to new and used motor vehicle sales and repair services.)
- Mixed use
- Existing light industrial uses
- Existing Residential Land Uses
- Restaurants and Cafes

#### Accessory Uses

- Residential above commercial (Not permitted at ground level)
- Day Care
- Schools and Training Facilities

#### Conditional Uses

- 1) *Expansion of Existing Light Industrial uses and automotive related business. Conditioned Upon*
  - A 50' foot buffer that includes a minimum of 10' foot wide vegetative buffer (consisting of trees, shrubs and other woody species), from the top of the Shabakunk Creek bank as per FEMA maps. Public access easements will also be necessary.
  - The interface with current residential uses is adequately addressed through design features that allow seem-less integration, buffering and landscaping.
  - Street trees and 5' minimum sidewalks.
  - Sign and lighting Standards are met.
- 2) *Expansion of Residential Land uses. Conditioned Upon*
  - The interface with current industrial uses is properly buffered and landscaped per the Planning Board's recommendations.
- 3) Drive Thru services are prohibited unless meeting the conditions set forth in **Building Placement and Design** section.

#### Bulk Standards

##### a. Principal Building.

- 1) Lot area: 12,000 sq. ft. - minimum
- 2) Build to Line:
  - i- 20' minimum - 25' feet maximum from the front property line along Olden Avenue, and Spruce Street..
  - ii- New Light Industrial uses. 10-15'.
- 3) Side yard: each 5 feet - minimum
- 4) Rear yard: 10 feet - minimum
- 5) Height: 3 ½ stories or 35' mixed-use. 1 ½ stories or 25' otherwise.

**b. Accessory Building.**

- 1) Distance to side lot line: 5 feet - minimum
- 2) Distance to rear lot line: 5 feet - minimum

**c. Lot Coverage.**

- 1) Total impervious coverage: 60% maximum for all property adjacent to Shabakunk Creek.
- 2) Additional coverage up to 10% may be granted if offset by contributions to other stream improvements or innovative urban stormwater design features.

**d. Floor Area Ratio**

- 1) 0.50 maximum. 1.25 mixed-use.

**Building Placement and Design**

- For property adjacent to the Shabakunk Creek, use alternative paving materials such as “Grasspave TM” or other types of porous paving material agreed to upon negotiation of the Redevelopment Agreement or Planning Board approval and in consultation with the Township Engineer.
- Capture and re-use greywater to the extent feasible.
- Provide shading for the stream corridor with tree species approved by the Township.
- Connections to adjacent neighborhoods where appropriate through stream crossings and/or access to park space or greenways established and conceptualized by the Plan where appropriate.

Drive-thru services are prohibited unless meeting the following conditions;

- i. They existed concurrently with the existing use at the time of this Plan’s adoption, Or,.
- ii. On-site circulation, including pedestrian, as well as ingress and egress issues concerning adjoining roadways are satisfied by the Township Development Professional as well as the County Engineer, And,
- iii. Direct ingress and/or egress onto Olden Avenue may be restricted or limited. Primary egress from a drive-thru must be located along a sidestreet, such

as 6th Street, 5th Street, or other street created as a means of implementing the roadway network of the Plan’s Transportation Section. Major routes, such as Parkside Avenue, Pennington Road, Arctic and Prospect Street may be similarly utilized, but the egress on these roadways must be located a minimum of 250’ from the intersection and subject to approval of the engineer of record for such roadway’s jurisdiction. Secondary ingress/egress points onto Olden may be considered so long as such point of access is shared by two (2) or more properties through easement. Reconfiguration of the curb-cut and parking may be necessary upon negotiation with subsequent development proposals by adjacent property owners.



*Some simple streetscape improvements along Arctic Parkway can go far toward the improvement of the pedestrian environment throughout the Area.*

## DESIGN STANDARDS

The Design Standards of this Plan apply to all projects in the Redevelopment Area. When combined with the Sub-area regulations, the intent is create an environment in which business thrives and people feel comfortable and safe on foot, bicycle, automobile or bus. This approach is designed to encourage people to spend money on the goods and services offered along Olden Avenue and throughout the Redevelopment Area. Walkability will improve as the Plan is systematically implemented. While vehicular circulation will remain important, the emphasis will be on calming traffic and creating destinations to achieve a better, more sustainable and aesthetically pleasing business district.

### Parking

This plan seeks to reduce the impact parking requirements have on the amount of impervious surface coverage within the Olden Avenue floodplain. Portions of the Redevelopment Area are envisioned as “park-once”, where multiple tasks can be accomplished with one “drive and park.” Depending upon the quality of the historic fill any property adjacent to the Shabakunk Creek may employ alternative paving materials such as “Grasspave™” or other types of porous paving material agreed to upon negotiation of the Redevelopment Agreement or Planning Board approval and in consultation with the Township Engineer.



An example of “Grasspave™”.  
Source: [www.stoneagepavers.com](http://www.stoneagepavers.com)

### Minimum Parking Ratios

Shared parking is encouraged and permitted to be presented to the Planning Board where appropriate. Traditionally, parking standards are derived through peak period demand, which many-times represent a “one-percent” design solution. What occurs the other 99 percent of time is what this Plan seeks to address - quality of life, not quantity of parking spaces. Mixed-use development is the principal way that parking reductions and shared parking can be sustained; particularly along auto-dependant corridors like Olden Avenue. In all other cases the parking standards in the Township’s Land Development Ordinance will apply. All shared parking models shall utilize the Urban Land Institute’s Shared Parking Manual.

#### (a) Residential Parking Standards

(1) The following standards shall apply to shared parking in mixed-use structures, accessible to Bus service: 1.3/unit average. Further organized by unit type:

- (i) Studio apartments- 1.00 spaces per unit
- (ii) 1 Bedroom- 1.20 spaces per dwelling unit
- (iii) 2 Bedroom- 1.50 spaces per dwelling unit
- (iv) 3 Bedroom — 1.75 spaces per dwelling unit
- (v) Senior Citizen - 1.00 space per dwelling unit

(2) For all other residential parking, the New Jersey Residential Site Improvement Standards shall apply.

#### (b) Non-Residential Parking Standards

(1) Where shared arrangements can be achieved and supported

- (i) Neighborhood Retail — Two (2) spaces per 1,000 square feet
- (ii) Shopping Center Retail - Three (3) spaces per 1,000 square feet
- (iii) Office space (business)— One (1) space per 500 square feet
- (iv) Professional Office - One (1) space per 350 square feet. For medical, dental and similar offices with patients, add Three (3) spaces per doctor.
- (v) Restaurant - One (1) space per every 4 seats
- (vi) Warehouses - One (1) space per employee on maximum shift
- (vii) Houses of Worship - One (1) space per every 3 seat
- (viii) Day care facilities - One (1) space per employee. AND one (1) space per 1000 square feet.

(2) For all other non- residential parking, the Ewing Township Land Development Ordinance shall apply.

*Parking Location & Design*

- (a) Shared multi-use parking is required wherever feasible to reduce overall parking requirements. Future cross-access easements will be sought for projects in order to set the stage for future shared arrangements as adjacent properties propose projects.
- (b) Provide fully or partially below grade structured parking for all development over 3 1/2 stories, where feasible.
- (c) For parking at the first floor and above the first floor, structures shall be fully clad from public view and located behind active uses while minimizing conflicts among pedestrians, bicycles and vehicle and providing a secure and visible environment for users of the parking facility where feasible.
- (d) When parking structures are not screened by active uses, clad the structure to match the building above, including but not limited to, appropriate ground level window treatments with translucent but not transparent glazing no smaller than three feet by three feet starting no higher than three feet above ground.
- (e) Locate access to all parking facilities along a street or shared access drive. Additionally:
  - (1) *Space curb cuts shall be a maximum of one (1) for every 100' feet of street frontage. Should the size of a development parcel prohibit this spacing, provide shared entrance provisions, or easements that allow future opportunities, in accordance with #2 (below) to ensure that ultimately the curb cut will be eliminated when feasible.*
  - (2) *Interconnect existing and future parking facilities on adjacent parcels to ensure more efficient design and accessibility.*
  - (3) *Separate parking from public streets by providing a landscaped planting strip at least seven (7) feet in width along each street line. Such strips must be coordinated sidewalks and pedestrian accessways. Separate this landscaped strip from the parking area by continuous concrete curbing. Breaks in curbing may be permitted in connection with stormwater alternatives as well as at accessways - pedestrian and vehicular.*
  - (4) *Locate entrances and exits upon a public street not less than fifty (50') feet from any street intersection, this distance to be measured from the intersection of the right-of-way lines at the corner affected and the closest point of such proposed driveway. Entrance and exit sizes, locations and*

*construction shall also be in accordance with requirements of the governmental agency having jurisdiction over the street on which the permitted use has frontage. Entrances and exits may be required to be shared with adjacent land uses.*

- (5) *Screen or buffer along each side and rear property line which adjoins a single-family residential district in the township or a similar district in an adjoining municipality, landscaped in accordance with a site plan acceptable to the Planning Board. A screen or buffer planting strip shall consist of massed evergreens and shrubs of such species and size as will produce an effective screen at time of planting. The planted screen shall be the responsibility of the applicant to carry out this program and to promote such maintenance and care as is required to obtain the effect intended by the original plan. Should a screen not be required, a seamless and integrated design approach will be employed with adjacent properties*
- (6) *Provide a minimum of (1) shade tree for every (18) eighteen parking spaces.*
- (f) *Meet all additional Landscaping requirements of LDO Section 215-55.*
- (g) *Plantings to be maintained in accordance with the approved site plan.*
- (h) The Planning Board may require a portion of the parking requirement in designated areas to remain



*Cladding is an important architectural element when garages are proposed.*

unpaved, maintained as lawn and/or reserved (“banked”) for temporary or future use as warranted by demand.

**Loading and Outdoor Storage**

- (a) There are no requirements for loading for residential uses.
- (b) Only supermarkets and industrial facilities are required to have off-street loading berths.
  - 1) First 8,000 square feet — zero (0) berth
  - 2) Next 10,000- One (1) berth
  - 3) Each additional 200,000 or fraction thereof - One (1) berth
- (c) No new long-term salvage or storage of vehicles permitted
- (d) Enclose all loading areas for non-residential uses and keep their doors closed except when loading and unloading. Keep trash and recycling materials inside the loading area prior to pick-up when located adjacent to residential structures or in mixed-use buildings. No loading areas shall face or be visible from any street.
- (e) No loading dock or service area shall be on any street frontage. All freight handling shall be on those sides of any buildings which do not face any street or proposed streets.
- (f) No use or accessory use shall be constructed to permit the storage of articles, equipment, goods or materials in the open exposed to public view, adjacent residences or a residential district. When necessary to store or keep such materials in the open, the area shall be fenced with a screen or buffer planting strip and be situated not closer than fifty (50’) feet from any residential district line.
- (g) Plant and maintain landscaping on those portions of all yards not used for parking, loading, unloading or service as required upon site plan approval.

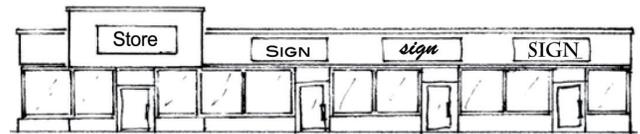
**Signage**

Signs define the character and the quality of a commercial corridor. Signage can add variety and a positive, coherent image to the street. Additionally, signage along the roadway is an important component of doing business.

The key to successful signage is clarity through simplicity, which can be afforded through understandable graphics that are presented legibly with controlled use of color and illumination. Furthermore, the reduction of visual clutter is important to ensure a safe, efficient, and aesthetically pleasing business district. Signage shall be compatible with the building and surrounding block and shall be readable by pedestrians or slow moving vehicles.

The following sign standards will apply within the Redevelopment Area and are designed to be consistent with those set forth by the Ewing Township Land Development Ordinance 215-37. These standards are intended to work in concert with the proposed design characteristics of the road network that serves this Plan.

- (a) Signage used on commercial structures shall:
  - **Permanent signs must be constructed of durable materials, such as wood, plastic or metal, professionally lettered with applied, incised, painted, printed or molded lettering.**
  - Use matte or dull finish to reduce glare
  - Use no more than three (3) colors, (principal, lettering and border) and tastefully arranged
  - Use subtle, indirect lighting.
  - Top lit signage shall use shielded light sources that are compatible with the building. No new backlit signs are permitted.



- (b) Each business establishment with one or more independent entrances in a retail or office center will be entitled to one building façade sign per street frontage. The site plan submitted to the Planning Board, applicants must include building elevation drawings that incorporate locations designed into the building facades for identification signage that is part of the architectural context of the building. The size

and graphics will also be identified on the signage plan that will be subject to the approval of the Planning Board.

(c) No freestanding signs will be permitted. Ground signs at that curb level will be the only permitted new sign types. Existing signs may be refaced but not expanded for change in occupancy only..

(d) Ground or Monument Signs must be incorporated into the landscape design and meet the following criteria:

(1) Signs are limited to one monument sign per road frontage between three feet in height to six feet in height (3-6') maximum depending upon the required sight triangle. Such a sign may be double-sided but is limited to 32 square feet in size, excluding logo as well as any pedestal or base under the monument.

(2) The monument sign must be constructed from finished masonry, metal, stone or etched glass in neutral or earth tone colors, except that alternative materials designed and intended to have an equivalent appearance may be approved by the Planning Board during site plan review.

(3) Monument signs may only be illuminated with spotlighting, no internal illumination. Incorporation of sign illumination into an overall landscape lighting design is strongly encouraged for Planning Board review.

(4) Monument sign text is limited to the name of the office center, retail center or other use and must include the street number used by the site for identification and/or mail delivery.

**(5) Landscaping**

i. Ground signs shall be appropriately landscaped at the base of such signs. Appropriate landscaping is defined as flowers, shrubs, ground cover and landscape techniques used in the following manner:

- (1) To enhance the sign design.
- (2) To conceal sign supports or light fixtures.
- (3) To shield sign illumination from adjacent properties and roadways.

ii. Evergreen plant materials of sufficient size when installed shall be required for all landscaping serving concealment and/or shielding purposes. Plant selection shall be based on screening ability, adequate hardiness and freedom from such problems as winter leaf desiccation and seasonal insect infestation.

(e) Where a single building entrance is shared by two or more business establishments, a on-site directory identification sign may be used to identify the name and location of each business. Directories may also be incorporated into the signage plan under (a) above.

(f) Storefront windows must be used for orderly display of merchandise and will not be permitted to become cluttered with temporary signage, flyers, leaflets and other material and enforceable by the Zoning Official or designee. Signs in windows are permitted but not more than 30% of the total window area per side may be utilized as such.

(h) Each new business establishment will be permitted one temporary banner announcing a grand opening for a period of two (2) calendar months from the first full or partial day it is open to the public. The banner must not be larger than 5% of the facade area of the building occupied by the new business and will not be located anywhere else on the site other than the front business facade.



New, uniform Sign standards will greatly improve the appearance of the Avenue

**Landscaping**

The landscape and lighting standards contained in the Ewing Township Land Development Ordinance shall apply to all development in the Redevelopment Area with the following supplemental standards:

- (a) Include a minimum of the equivalent of 15 shade trees or 6-inch caliper per acre of parcel area in into the overall site landscape plan, not including those required of parking areas. (Two 3 inch caliper trees will equate to one 6 inch caliper tree and three 1 inch caliper ornamental trees will equate to one three inch caliper shade tree. The minimum planting sizes shall conform to the requirements of the Township’s Shade Tree Ordinance.
- (b) All projects will be landscaped and maintained in accordance with LDO Section 215-57 Landscaping; and 215-55. Circulation and Parking Design Standards
- (c) Use a mixture of evergreen trees and ornamental shrubs for buffers created pursuant to the building requirements of this Plan, except where crossed by pedestrian walkways.
- (d) Plant street trees along the Right of Way of all streets at a minimum of 30’ on-center and conforming to this Plan’s Roadway Design requirements (graphically represented in Figure A) and in consultation with the Township’s Shade Tree Ordinance.
- (e) Along each side and rear property line which adjoins a single-family residential district in the township or a similar district in an adjoining municipality, a screen or buffer planting strip may be required consisting of massed evergreens and shrubs of such species and size as will produce an effective screen at time of planting. The screen or buffer strip shall be landscaped in accordance with a plan acceptable to the planning board. The planted screen shall be the responsibility of the applicant to carry out this program and to promote such maintenance and care as is required to obtain the effect intended by the original plan. Should a screen not be required, a seem-less and integrated design approach will be employed with adjacent properties
- (f) For property adjacent to the Shabakunk Creek use alternative paving materials such as “Grasspave TM” or other types of porous paving material agreed to upon negotiation of the Redevelopment Agreement or Planning Board approval and in consultation with the Township Engineer.

- (g) For property adjacent to the Shabakunk Creek provide shading for the stream corridor with tree species approved by the Township.
- (h) Provide connections to adjacent neighborhoods where appropriate through and along stream crossings and/or access to park space or greenways established and conceptualized by the Plan.
- (i) For locations within a flood plain area, an emergency evacuation plan is required.

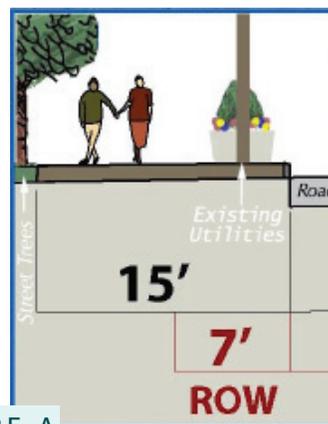


FIGURE A



**SHORT-TERM**  
By merely adding street trees begins to soften the street and detracts from the utility poles.

**CURRENTLY**  
Currently, signage and lack of streetscape creates a harsh environment and one that is not pedestrian friendly to the surrounding neighborhoods





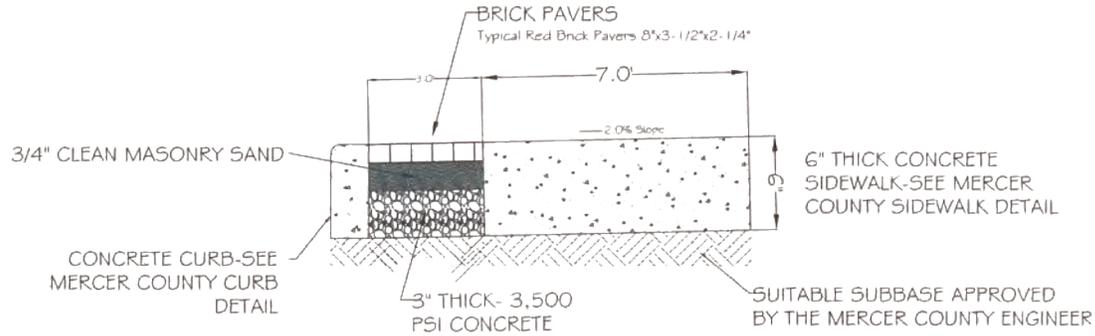
**LONG-TERM**  
Eventually, public and private investment will work together to create a solid place for pedestrians and businesses.

*North Olden Avenue provides an opportunity to establish a visual theme for the right-of-way areas between the street-line and the curb, as well as new landscaping and signage criteria, that works to coordinate new development and redevelopment.*

**Streetscape**

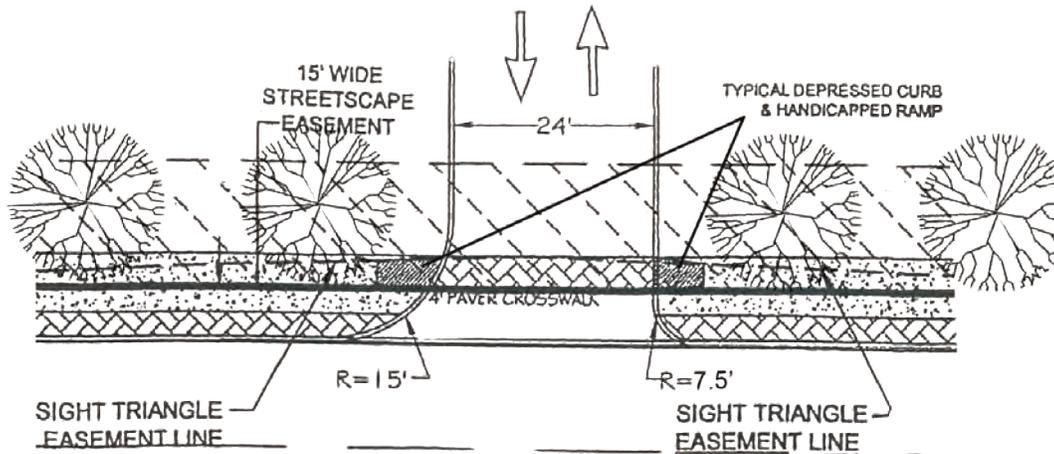
All streetscape shall be designed to present a uniform look along the Avenue. As such, the following design standards shall be applied to all projects seeking Site plan approval. A minimum of Fifteen (15') in accordance with Figure A and applied as follows;

- i. Three (3') foot landing zone. Brick.
- ii. Seven (7') foot sidewalk. Concrete
- iii. Planting Strip / Landscape Easement (5') feet Minimum. Princeton Elm street Trees planted Thirty (30') on-center and In accordance with Town- ship standards. Alternatives may be considered by the Planning Board.



**OLDEN AVENUE SIDEWALK DETAIL**

NOT TO SCALE



Street Trees	Symbol	Scientific name	Common name	Size	Comments
30' OC	UA	<i>Ulmus americana</i> "Princeton"	PRINCETON ELM	3-3-1/2" Cal.	B # B

Graphic Courtesy of Princeton Engineering.

## **Façade Design**

The standards in this section shall supplement the standards of the Ewing Township Land Development Ordinance. In cases of conflict, these standards shall supersede the standards of the Ewing Township Land Development Ordinance.

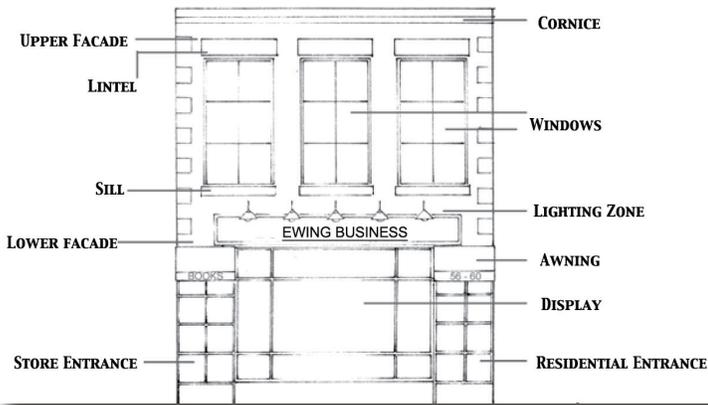
### *Storefronts*

Developing and maintaining a coherent streetscape is crucial for a successful commercial districts. Because storefronts are a key element of streetscapes and aesthetic and welcoming element within each block, storefront design shall be coordinated in the context of the entire building façade and not treated as being isolated and independent.

At least 50 percent of the ground floor façade area shall be fully or partially transparent for visibility from the sidewalk, with at least 30 percent being fully transparent glass. On corner lots, the secondary storefront façade should be consistent with the alignment, location and amount of glazing of the primary storefront window façade.

- Retain and restore original storefront features if possible and appropriate.
- Use elements of the storefront, signage, graphics, awnings and colors as a unifying image within a block.
- Maintain and promote a pedestrian scale within the building and block character.

The following graphic is an example of a well-composed storefront. It incorporates several elements that are crucial to the coherence and appeal. The entrance is been placed off-center and is highlighted by using the suspended canopy to call attention to it. A clear glass window to the right of the entrance acts as the display window and allows a view inside where displays may be located. The wooden framing of the window is a simple and classic way of dividing large areas of glass while adding a little detail to the façade. The full height windows to the left of the entrance act as a main component of the façade in the way that they allow a complete view of the inside of the store. Also these windows have been framed to appear to be the focus of the façade. Lastly, the cornice ties the entire store façade together. By addressing with three simple elements, a simple and attractive store façade can be created, such as shown below.



*Entrances*

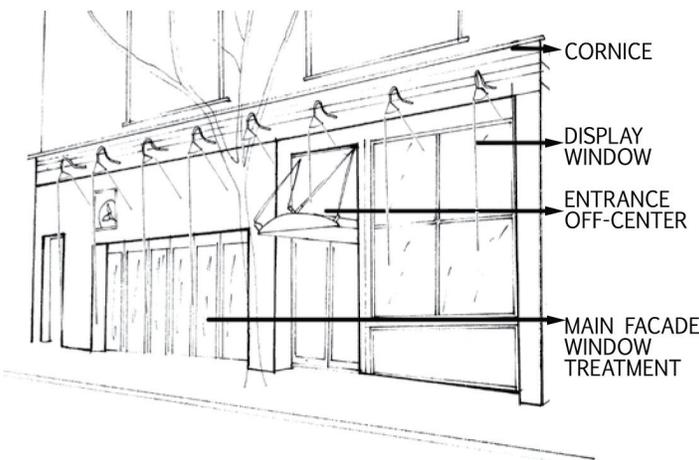
Entrances in storefronts should be obvious and welcoming and an integral part of the façade. In many traditional storefronts, entrances are well defined using such elements as lintels, columns, railings and overhangs. These elements should be retained and restored whenever possible.

- Retain and restore original doors if possible and appropriate.
- Install a door that is compatible in scale, material and shape with the storefront.
- Make entrances to upper stories a secondary element and compatible with the overall design of the storefront.

*Windows*

Windows should be used to provide maximum exposure for the business within a building and to enliven the street environment. Windows are vital in creating an 'inviting' look for potential customers, as well as in stimulating interest toward the products displayed in the store.

- Maintain original windows when possible. Replacement windows should match original window in size and style.
- Use large, clear glass windows. Display windows should be at least 6 feet high.



- Mount storm windows inside windows
- Align the location of upper floor windows to the location of windows and doors on the storefront level.
- Materials that frame windows are harmonious throughout the façade.
- Use windows that are compatible in materials, size and proportions with those on surrounding buildings.
- Clean glass regularly.

Façade entries should become the focal point of the façade by adding a decorative wood door with clear glass panels. Since the upper façade has symmetrical and equally spaced windows, the logical solution would be to have a symmetrical composition of windows and doors on the storefront, keeping in mind the lines of the upper windows. Such a composition gives the façade an established and appealing appearance.



*Lighting*

Lighting plays as important a role, as any other element, in enhancing the appearance of a commercial façade. Lighting techniques can be used to attract attention to a store by 'washing' the storefront with uniform light or by highlighting its elements like the entrance, signage, window displays etc. It can also have an effect on the streetscape itself and it should produce a feeling of liveliness and security for all those on the street.

- Design, size and location of lighting fixtures must be compatible with the overall design of the building.
- Goose neck fixtures or shadowbox lighting is preferable for storefront lighting.
- Subtle, low-intensity light that reveals the true character of the building

- Street address numbers shall be illuminated at all times outside of daylight hours.
- For security, interior illumination shall be provided between 10% and 25% of illumination levels of operating hours, when the building is closed for all spaces visible through windows.
- Lighting shall not be directed to or otherwise cast light or glare on residential properties.

The illustration above shows a simple application of lighting on the façade of the building. The gooseneck light fixtures do not detract from the appearance of the building. Some of the other design elements discussed can be seen here. This building has a clean sleek style with a well-defined entrance. The large windows bring activity to the street level. This building clearly defines the upper and lower façade elements.

### Awnings

Awnings along windows and articulating entrance ways can bring about a unifying look to a commercial corridor while keeping the uniqueness of the building structure. They not only affect the appearance of a storefront but of a streetscape and the pedestrian environment as a whole. It is a decorative element that also has a practical purpose by providing protection from the weather.

- Placed between a storefront's vertical piers fitting the opening in both size and shape.
- Use of fabric (natural or synthetic) that is weather resistant.
- Use of colors that are complementary to the façade and surrounding buildings, preferably one solid color
- Use of a simple, straight design.
- Painted lettering on front valence of awning only.

### Security Grates

Security grates should be avoided, limited to façades that are not visible from public streets. Where used, security



grates shall be retractable, shall be at least 75 percent transparent, and shall be located inside glass windows and doors.

### Vegetative Elements

Vegetative elements can strengthen the appeal of a business and the commercial streetwall overall. They bring a green element to the urban environment.

- Use plantings that are seasonal that yield a high impact and have low maintenance such as annuals, vines and ornamental grasses.
- Use wall mounted or floor mounted planters that don't obstruct pedestrian flow.
- Use sturdy and stable planters that cannot be easily tipped over or destroyed, materials including terracotta, ceramic, stone, and ornamental cement.

### Window boxes

- Use on upper story windowsills and where they appropriately fit on storefronts.
- Use durable boxes, made from painted wood.
- Use plantings and flowers that are seasonal and are easy to maintain.

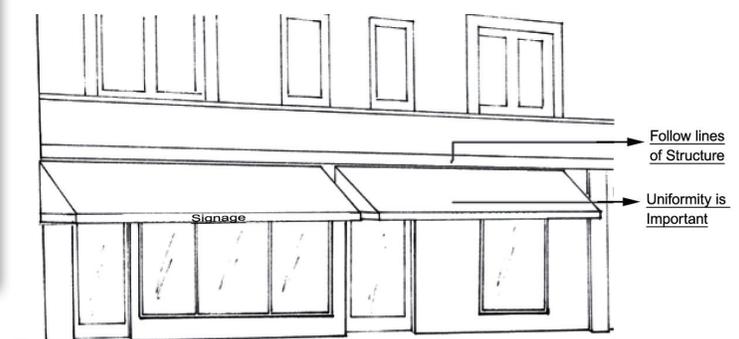
### Materials

The proper choice of materials contributes to the unity of the street environment. All buildings should use durable, natural materials that are of high quality, easy to maintain and compatible with surrounding buildings.

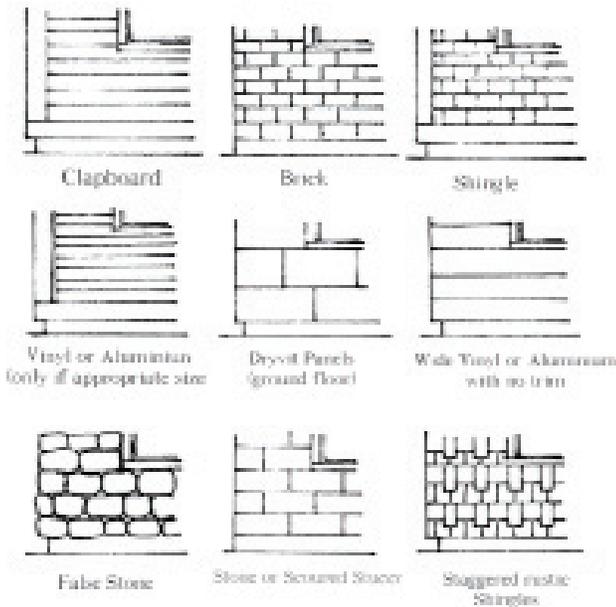
- Repair and retain existing materials whenever possible and appropriate.
- Replace deteriorated materials with materials of like construction.
- Use durable materials such as real brick and stone, tile, real stucco or painted wood with real drop siding, trim and cornices.
- Uncover and retain original exterior walls and roof treatments where appropriate.

### Materials not permitted are:

- Plastic or vinyl siding



- Use of exposed concrete block
- Synthetic stucco



**Color**

Color is an important way to tie a façade together and it conveys the identity and attitude of a store. Every effort should be made to match the original color or use colors that are appropriate to the building period.

- Consider colors that are contextually appropriate to the entire commercial corridor
- Use harmonious colors that bring together all aspects of the façade.
- Use one base color and not more than 2 accent colors.
- Use traditional durable colors **such as but not limited to;** beige, terra cotta, brick red, dark green, dark blue, and various grays.

Color not recommended:

- Using more than three (3) colors.
- Using conflicting colors on a façade except for here it fits on building accents.
- Painting arbitrary decorative lines, bands or graphics that do not relate to the buildings character.

**Design Theme and District Branding**

Creating a design theme and branding the Olden Avenue Business District will promote and market the Redevelopment Area as a place to shop and do business. The Township and Agency will continue to work with the businesses within the district to not only create an identity but have businesses embrace and implement this identity. Items for consideration shall include:

- Identify businesses by block or Subarea with a common theme (i.e. auto-related) and wayfinding signs to help customers navigate the area
- Coordinate with the road redesign plan to create attractive banners, flags, and gateway treatments.
- Establish a business improvement district that will help new and existing businesses find funds for repairs, landscaping, facade improvements, etc., in order to implement this plan.
- Develop a marketing and outreach strategy.

**Wayfinding**

Directional signage, including directional advertising signage, shall be provided consistent with the applicable wayfinding plan approved by the Redevelopment Agency, Planning Board and agency with jurisdiction over the roadway. Wayfinding is a method used to guide people to their destinations. Signs are tools that aid in wayfinding. and indicators such as light, color, materials, and pathways also play a large role. A successful wayfinding program is intuitive and self-navigable, and it protects the overall visual integrity of the site. Wayfinding is specific to its place and visitors. Such a program, as stated above, must be developed with the business community before it can be implemented.



### **Cellular Communications**

New monopoles may be permitted with the Redevelopment Area but will be highly scrutinized. New facilities are required to be collocated on existing monopoles or towers where feasible. Reconstruction of existing poles is permitted but subject to review by the Planning Board. Notwithstanding, all cellular communication facilities shall utilize the following standards.

- 1) Collocation on existing poles or towers.
  - a) Antenna shall be flush mounted and painted to match the existing monopole or tower.
  - b) All supporting equipment and cabinets shall be thoroughly landscaped and buffered from view.
  - c) If reconstruction is necessary, camouflage may be required at the discretion of the Planning Board. Where appropriate, such tactics as flag poles, and public art concepts are preferred.
- 2) Collocation on a building structure or rooftop.
  - a) All antennas and associated equipment will be screened from public view by parapets and/or other architectural treatments. Flush mounted antennas painted to match are not considered proper screening techniques.
  - b) Architecture of the structure must remain in balance.
  - c) All wiring must be concealed from view.
- 3) Upgrades.
  - a) All communications upgrades proposed must ensure that the previous equipment that is no longer in use is removed.



Branding and Wayfinding will be a critical component of rebranding the Olden Avenue Business District

# INFRASTRUCTURE STANDARDS

## Public Infrastructure - Olden Avenue Roadway Design Plan

### Background

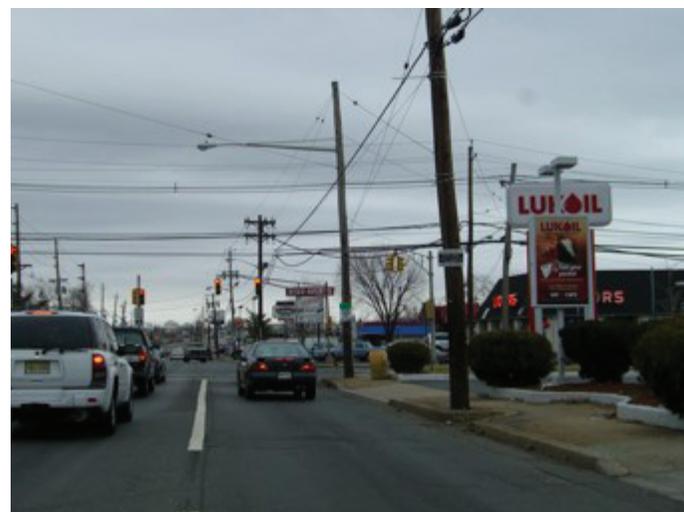
While it remains a well-located shopping destination, the North Olden Avenue corridor is currently a stark, auto-oriented highway “strip” that depends on through-traffic to siphon customers off the road. While attracting customers off the road is paramount to the success of small businesses, there has been little or no consideration given to the aesthetic experience of traveling the corridor, or to the overall functionality and interconnectivity of its roadways for accessing local goods and services. The result has been counterproductive — a roadway design that does not make shoppers want to spend their money there, that encourages travelers to avoid or bypass businesses along Olden Avenue in favor of other, more distant and less convenient destinations, often outside Ewing Township. Land use and transportation policies that work against community cohesion and economic vitality has made it difficult to attractively brand Ewing Township’s traditional business district, resulting in excessive signage and the pursuit of squeezing every last parking space out of undersized sites.

Both the Urban Land Institute’s *Mercer Crossings* report and the Circulation Element of the *Ewing Township Master Plan* call for traffic conditions to be treated using a “Complete Streets” approach that provides mobility options and improves safety along the corridor. The Olden Avenue Redevelopment Plan integrates land use and transportation in a way that calms traffic, accommodates all modes of transportation, eases access to businesses, and respects all users of the corridor.

As detailed in this Plan’s Technical Appendices, the key issue for Olden Avenue today is not its capacity to carry traffic; rather, Olden Avenue has deficiencies in safety and aesthetic qualities that currently prevent it from achieving its potential for sound, sustainable economic development. Over the past 40 years, the corridor’s auto dominated design and the subsequent orientation of businesses to only be accessible to cars and trucks has created a hostile environment for drivers, bicyclists, and pedestrians. As

this Plan’s vision is realized and transportation options implemented, Olden Avenue will transform over time into a road that enhances business for a variety of people in cars, on buses, on bicycle, and on foot. This vision will reposition the corridor as an even stronger economic market with public infrastructure and private investment working together to create a place of pride and profitability benefiting all residents, businesses and institutions in Ewing.

Olden Avenue has evolved to its current condition through a series of decisions that were made over time on a parcel by parcel basis. Each building was sited relative to that parcel’s dimensions, each curb cut reflective of that use’s need. Over time, this has led to a disjointed, less economically competitive corridor. The vision now established to revitalize Olden Avenue as a successful business district depends on new decisions by the Planning Board that carry the spirit and intent of the vision as opportunities arise in the future. Every successful, well-designed business district is the result of a series of well thought out and coordinated decisions and negotiations. Details such as sign placement, curb radii, landscaping, and site design must leverage the investments of neighboring sites. As with all roadway design standards, this Plan seeks designs that accommodate the everyday user, not the infrequent user. While fire and other emergency apparatus will require entry, it is the role and responsibility of the governing body to make sure it partners with others like the fire company to accomplish both safety and vision. They are not, and do not need to be, mutually exclusive.



*What are Complete Streets?*

Complete streets are designed and operated to enable safe access for all users. Pedestrians, bicyclists, motorists, and public transportation users of all ages and abilities (including older people, children, and people with disabilities) are able to safely move along and across a complete street.

There is no one design prescription for complete streets. Ingredients that may be found on a complete street include: sidewalks, bike lanes (or wide paved shoulders), special bus lanes, comfortable and accessible public transportation stops, frequent crossing opportunities, median islands, accessible pedestrian signals, curb extensions, and more. A complete street in a rural area will look quite different from a complete street in a highly urban area. But both are designed to balance safety and convenience for everyone using the road.

([www.completestreets.org](http://www.completestreets.org))

## Summary of the Olden Avenue Roadway Design Plan

The roadway design recommendations developed for this Plan support the future economic development objectives for the Olden Avenue Redevelopment Plan. These recommendations are based on providing a safe comfortable environment for shoppers who arrive by bicycle, on foot, on transit or by car.

**1) Network** - to provide mobility options and reduce congestion, the Plan details how improving the road network in the Redevelopment Area improves the potential for economic development. When coordinated with land use, an enhanced roadway network not only provides better accessibility, but more roadway frontage for development. This network will establish a walkable block structure that will encourage shoppers to travel on foot among local destinations.

**2) Block structure** - to create a more pedestrian scale and to manage speed and access the Plan will over time create a new block structure at a pedestrian scale that enhances pedestrian safety and comfort while working to improve aesthetics along Olden Avenue;

**3) Corridor improvements** - by using the existing curb lines to maintain safety, improve operations and accommodate the many modes of transportation present along the corridor, the Plan limits the need to negotiate purchase for additional right-of-way. Given that Olden Avenue's road profile is currently wider than necessary, it degrades the pedestrian environment. In the short-term, re-striping can be a relatively cost effective approach effectively working toward implementing this Plan's goals;

**4) Intersection design** - features, such as roundabouts, will serve as gateway elements to calm traffic and create a distinct environment. Pedestrian friendly intersection designs with clearly marked crosswalks, curb extensions and pedestrian signals will also improve safety and walkability.

**5) Bicycle provisions** - A continuous bike lane as a shared or dedicated lane will provide a network for bicyclists. For all travelers, this enhanced network will provide choice in routes to destinations, will promote increased physical activity, and will improve safety for everyone by reducing conflicts between bicycles and cars on the roadway, and between bicycles and pedestrians on the sidewalk. Bicycle provisions must be a consideration in land use design as well, such as by providing rack and shelters for bicycle storage and by including changing and showering facilities in work places.

**6) Median Improvements** - A median/ dedicated turn lane on Olden will increase safety by separating through and turning movements more effectively. Roundabouts are recommended as part of this plan if ultimately decided to be a design element upon creation of final design documents, the opportunities for more creative use of the median for some strategic placement of landscaping and traffic calming features can be incorporated as round-a-bouts provide safe and effective u-turn capabilities.

**7) Round-a-bouts** - Round-a-bouts are effective gateway treatments and traffic calming elements to be considered in the final design documents for Olden Avenue. Candidate intersections include Parkside and Arctic Parkway, with Prospect Street also a consideration.

## Roadway Design Goals and Objectives

**1) Calm traffic** - Implement traffic calming techniques that reinforce the economic development strategy. Regional through traffic for destinations other than Olden Avenue must be allowed to flow, but not to the detriment of a vibrant local business district.

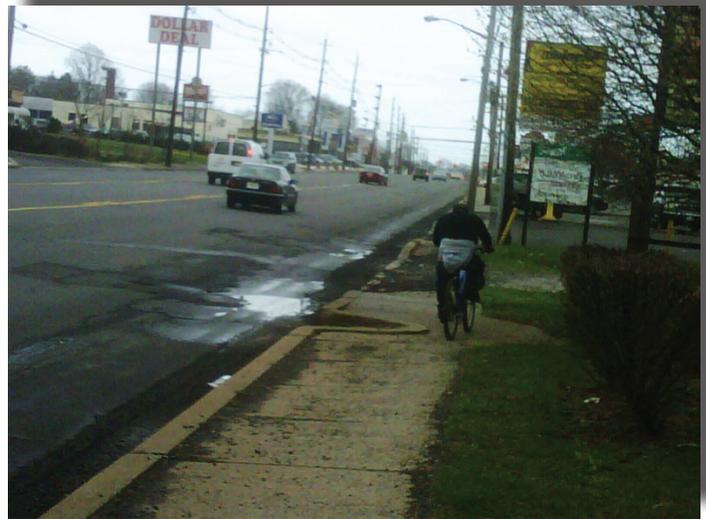
**2) Create a visual identity** - Coordinate design elements including clearly delineated sidewalks, pedestrian-oriented street furniture where appropriate, light fixtures, coordinated landscaping, and buildings oriented toward the street as part of the corridor's business model.

### 3) Maximize pedestrian mobility;

- Provide for safe, delineated pedestrian movements from store to store, site to site in order to reduce the number of vehicle trips requiring access to Olden Avenue.
- Provide safe pedestrian and bicycle access to and from adjacent neighborhoods and municipalities.
- Require streetscape elements along property frontages inside the curb-line, linking walkways to new and existing development.
- Create logical pedestrian linkages between adjacent properties as well as cross-access easements for vehicular connections between properties.
- Coordinate pedestrian-oriented improvements with the public transportation system.

**4) Coordinate access and shared parking** - Provide parking in the front yard ONLY if sufficient space is not available in the rear or side yard area. Develop site plans that have future connectivity to adjacent properties and future road network in order to achieve maximum efficiency in the design and delivery of parking for adjacent business. Ensure proper landscaping with an eye toward innovative paving materials, and shading of surfaces that retain heat.

**5) Implement "green" design practices** - Install trees of acceptable height and caliber to provide shade, pedestrian refuge, and aesthetic improvement as a critical element of streetscape and frontage design. Trees may be grouped to provide clear sight-lines to building facades and identification signage. Use of innovative storm water capture techniques should be included with all development and site design criteria.



*Whether traveling by bicycle, wheelchair, or waiting for the bus, there is a lot that to be improved along the corridor*

## Plan Approach

North Olden Avenue is under the jurisdiction of Mercer County and currently consists of two travel lanes in each direction with a wide striped center median to enable left turns along most of its length through the Redevelopment Area. The County is currently developing an access management plan and code, which is envisioned to function like the State Highway Access Management Code. The ULI August 2004 *Mercer Crossings* report endorsed the conversion of North Olden Avenue into a boulevard with a landscaped median. While design documents for Olden Avenue are yet to be drawn, the vision of this Plan allows a landscaped median in strategic locations subject to mid-block crossing needs and the ability to implement other design elements that enable U-turns, such as round-a-bouts. This Plan was developed with the County of Mercer's input and consideration of:

- *The basic logistical access needs of the business community.*
- *The importance of input from the business community.*
- *Design options available and necessary to not only calm traffic, but to provide efficient flow as well.*
- *Regional traffic conditions and needs.*
- *Trucking needs and travel and access.*
- *Network needed to support and enhance economic development initiatives.*
- *Known transportation and planning techniques design to build a complete community.*

This Roadway Design Plan was also developed with the input of Township residents, and importantly, the business community over the past two years and is coordinated with the opportunities that the Land Use portion of the Olden Avenue Redevelopment Plan presents.



*Graphic Simulation of how Arctic Parkway could be improved upon implementation of this Plan. This Plan contemplates roadway improvements that can then be leveraged by adjacent businesses to create a first class atmosphere with what is in essence minimal changes.*



### Implementation of Roadway Infrastructure

All development in the Olden Avenue Redevelopment Area will be conditioned upon an appropriate contribution of off-tract improvements or easement set-asides where necessary in order to carry out the vision of the Plan. Except as otherwise specified in this Plan, the extent of a developer's contribution for off-site improvements will be outlined in the redevelopment agreement with the Ewing Township Redevelopment Agency (ETRA). ~~and ratified by the Ewing Township Council.~~ If such responsibility is not covered in a redevelopment agreement, the redeveloper's contribution for off-tract improvements will be determined in the same manner as other development projects throughout the Township's normal permit and/or site plan review process through the Planning Board. In addition to the standards outlined in this Plan, the Township may be required to take additional action to implement this Plan. These may include, but are not limited to:

- *Negotiation for and collection of fees from developers for park and infrastructure improvements.*
- *Public investment in open space improvements.*
- *Environmental remediation.*
- *Vacation or realignments of utility and roadway right-of-way and easements.*
- *Negotiations with adjacent municipalities, Mercer County, State of New Jersey and New Jersey Transit.*
- *Improvements outside of this Plan's jurisdiction to facilitate connections between new and existing neighborhoods, mass transit stations and schools*

The vast majority of the Roadway Plan Elements planned for the Olden Avenue Redevelopment Area will be implemented through the pursuit of State and Federal funding given the high cost of retrofitting (particularly Olden Avenue). Retrofitting 50 years of auto-driven design will take time, effort and partnership to work for all users of the road network; pedestrian, auto, bicycle, or transit. However, these retrofits are consistent with the new direction and priorities of the Federal Government and its "Livable Communities" initiative. As coined by the convergence of HUD, USDOT, and the EPA, The Partnership for Sustainable Communities has established six livability principles that will act as a foundation for interagency coordination and programming. These livability principles are being utilized to evaluate the worthiness of a project, such as Olden Avenue, to be funded. This Olden Avenue Redevelopment meets these goals. They are:

**1. Provide more transportation choices.**

Develop safe, reliable and economical transportation choices to decrease household transportation costs, reduce our nation's dependence on foreign oil, improve air quality, reduce greenhouse gas emissions and promote public health.

**2. Promote equitable, affordable housing.**

Expand location- and energy-efficient housing choices for people of all ages, incomes, races and ethnicity to increase mobility and lower the combined cost of housing and transportation.

**3. Enhance economic competitiveness.**

Improve economic competitiveness through reliable and timely access to employment centers, educational opportunities, services and other basic needs by workers as well as expanded business access to markets.

**4. Support existing communities.**

Target federal funding toward existing communities - through such strategies as transit-oriented, mixed-use development and land recycling - to increase community revitalization, improve the efficiency of public works investments, and safeguard rural landscapes.

**5. Coordinate policies and leverage investment.**

Align federal policies and funding to remove barriers to collaboration, leverage funding and increase the accountability and effectiveness of all levels of government to plan for future growth, including making smart energy choices such as locally generated renewable energy.

**6. Value communities and neighborhoods.**

Enhance the unique characteristics of all communities by investing in healthy, safe and walkable neighborhoods - rural, urban or suburban.

## Roadway Plan -Major Plan Elements

The design for Olden Avenue uses four major elements that will help it become a great business district:

- 1) *Network - to provide mobility options and reduce congestion;*
- 2) *Block structure - to create a more pedestrian scale, add economic development potential, and to manage speed and access;*
- 3) *Corridor improvements - to use existing curb lines to maintain safety, improve operations and accommodate the many modes of transportation present daily along the corridor; bicycle, pedestrian, wheelchair, automobile, bus, and truck; and*
- 4) *Intersection design - to use features, such as roundabouts, to serve as gateway elements to calm traffic and create a distinctive environment, and to give priority to pedestrians in traditional intersections.*

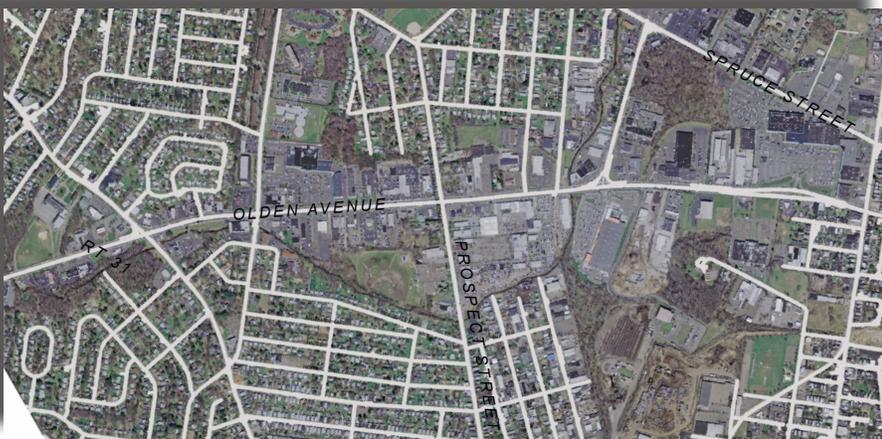
### 1. Network

A good transportation network consists of a hierarchy of interconnected roads and access points. This connectivity is essential to a successful business environment. It reduces congestion by creating more routes to a given destination, and allows regional and through traffic to safely coexist. Without options, through and local traffic co-exist inefficiently, with congestion as a result. If the roadway network is designed to accommodate all modes - sidewalks, bicycle paths and for transit - more mobility options that function for economic growth are created.

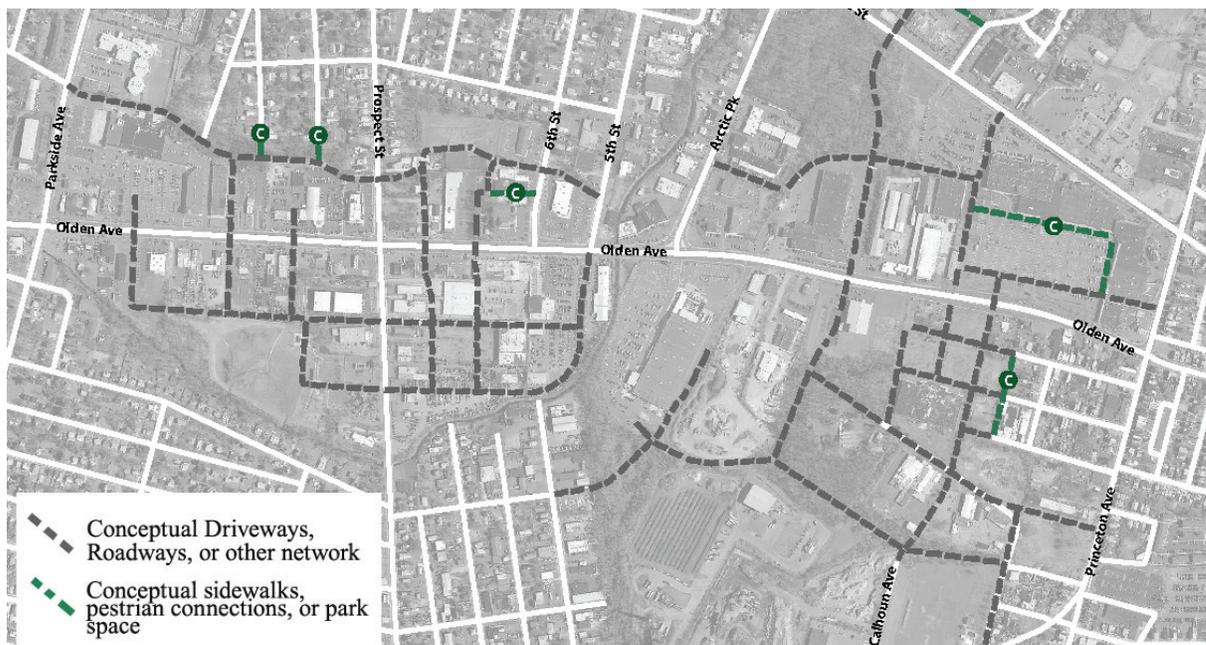
Olden Avenue today is suburban in character and function – a wide roadway dedicated to auto use, large distances between intersections, a dense pattern of driveways, infrequent pedestrian crossing opportunities, and no bicycle facilities. While Olden Avenue’s existing network provides some connectivity, a large area is missing a network. Between Princeton and Parkside Avenues, there are no through connections. This creates a condition where all local and through traffic must use Olden Avenue, resulting in numerous conflicts as through traffic tries to get through as quickly soon as possible while local traffic drives more slowly seeking its destinations.

An enhanced network will relieve Olden Avenue from the burden of being the only route opportunity for local and through trips, and will create economic development opportunities, particularly south of Olden Avenue and east of Arctic Avenue in Subareas 4, 5, and 6. Additional parallel and intersecting street network can be created by creating a denser pattern of side streets.

Pedestrians and bicyclists should feel safe and comfortable on Olden Avenue and its larger network. It is an objective of this plan to provide full bicycle and pedestrian compatibility. Being able to safely walk and bike makes shopping destinations accessible to these modes, which not only adds to the potential buyer base in the corridor but makes arrival by automobile more pleasant as well.



The strong surrounding existing street network that supports Olden Avenue, (highlighted in white) breaks down within the district thus limiting choice and reducing mobility. Opposite, this Plan recommends making the network whole over time in order to support economic development initiatives...



## 2. Block Structure

Olden Avenue’s existing block structure is much too large to support the creation of a truly diverse and successful shopping district. Large blocks facilitate higher operating speeds, concentrate turning movements and can stress performance of major intersections. These conditions degrade a local business environment. Successful traditional business districts have a block structure of 200 to 500 feet. This interval spacing is designed to allow shoppers to walk, bike, drive or take transit, and provides opportunities at intervals to cross Olden Avenue. Some parts of Olden Avenue today have blocks 2,000 feet and beyond in length.

Over time, the pattern of development has created individual driveways to each land use. This patterns breaks up the pedestrian realm, causing unsafe conditions for both vehicles and pedestrians. Primary access to businesses should be at street intersections only. Single purpose driveways should be consolidated, eliminated or minimized in accordance with the standards set forth in this Plan. (See Curb-cuts section below)

Block structure and spacing was developed for this Plan by first examining the existing distances between intersections, relating the proposed block size to the proposed land use and ultimate design character and then developing walkable intervals. Figure 1 depicts how block structure can coordinate with new development and redevelopment potential. Existing buildings and lot

lines were followed where possible to minimize impacts to properties. Intersecting streets were developed to create cross streets. Rear ends of blocks are enclosed by networks paralleling Olden Avenue, providing cross connectivity which will minimize local movements on Olden Avenue.

In the core area between Parkside Avenue and Arctic Parkway, where more dense development is proposed, the existing block interval between 5th and 6th Streets provides a good template for creating a denser block pattern between Arctic Parkway and Prospect Street. Block intervals ranging from 360 feet to 540 feet were developed for the block between Parkside Avenue and Prospect Street. See Figure 2.

Between Arctic Parkway and Princeton Avenue, the land use pattern is and will be less densely developed in the future (Figure 3). While block structure and network was added, 900 foot block intervals were developed to correspond to this context.

Calhoun Street Extension - The extension of Calhoun Street discussed in the Olden Avenue Redevelopment Plan is an essential component of creating new network. Under this plan, the Calhoun Street Extension will cross Olden Avenue and terminate into Capital Plaza. It will provide a major spine and will create major and minor block structure in an area which currently has no network. In doing so, it will improve access to the corridor and create redevelopment opportunities. Minor street systems will make two intersections with Spruce Street.



Figure 2- Block Structure Parkside to Arctic Parkway

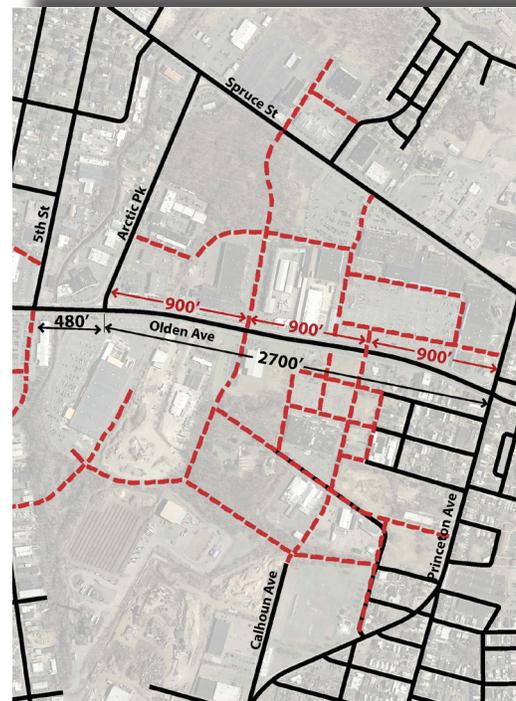


Figure 3- Block Structure Arctic to Princeton Avenue

## Block Structure and Economic Development Opportunities

When properly planned, enhance block structure or points of shared access works to enhance the property values for those affected by the change. Once committed, a property owner can work with his or her neighbor to reorganize access in a way that increases their effective parking capacity through shared design and cross access easements. Obviously, the more parking a site can accommodate in auto-dependent areas such as this one, the more entitlements to develop are available to a property owner.

Concept drawing depict the development potential for two areas within Subarea 2. Figure 4 reveals the Parkside Avenue intersection with focus around “Parkside Plaza” (Shop Rite). Figure 5 conceptualizes the potential for the north side of Olden between 6th Street and Prospect Street. As can be seen in the Land Use Chart and accompanying Parking Efficiency Table, the development potential increases three-fold with rational access and good site design. As was recently coordinated with the “Senior Star” (Figure 6) application for North Olden Avenue, future shared parking and access improvements were included in the final approvals to allow for this much longer-term concept to unfold over time.

Figure 5-



### **PARKSIDE PLAZA – Conceptual Land Use and Parking Potential**

Building 1 (new structure): Footprint: 50' x 60' = 3,000 sft.

If used as office space above commercial – 2.5 stories: 3,000 commercial + 3,000 office = 6,000 sft total, 6+6 =12 parking spaces. 3.5 stories: 3,000 commercial + 6,000 office = 9,000 sft total, 6+6+6=18 parking spaces

Building 2 (new structure): Footprint: 300' (centerline) x 60' = 18,000 sft. – 1.5 stories, 18,000 sft total, 36 parking spaces. If 18,000-sft office space located on upper floor - 36,000 sft total area, 54 parking spaces.

Building 3:

Existing Taco Bell - No change. Already has adequate parking with no sharing.

Building 4 (new structure): Footprint: 60' x 60' = 3,600 sft.

Pad site for commercial uses – 1.5 stories, 3,600 sft total area, 7 (7.2) parking spaces per Plan's shared standards.

If used as office space above commercial (are daycare, artist studios, dance studios considered office space?):

- 2.5 stories: 3,600 commercial + 3,600 office = 7,200 sft total, 7+4 =11 parking spaces.
- 3.5 stories: 3,600 commercial + 7,200 office = 108,000 sft total, 7+7=14 parking spaces

Building 5 (new structure): Footprint: 180' x 80' = 14,400 sft - 1.5 stories (commercial only): 14,400 sft total area, 29 parking spaces per Plan's shared standards. 2.5 stories: 14,400 commercial + 14,400 office = 28,800 sft total, 29+14 = 43 parking spaces. 3.5 stories: 14,400 commercial + 28,800 office = 43,200 sft total, 29+29= 58 parking spaces

Buildings 6 and 7: Existing neighborhood – commercial, to remain Building 6 = 21,600 sft, Building 7 = 88,700 sft

Parking Lot capacity approximately 460 spaces (9' wide x 20' deep)

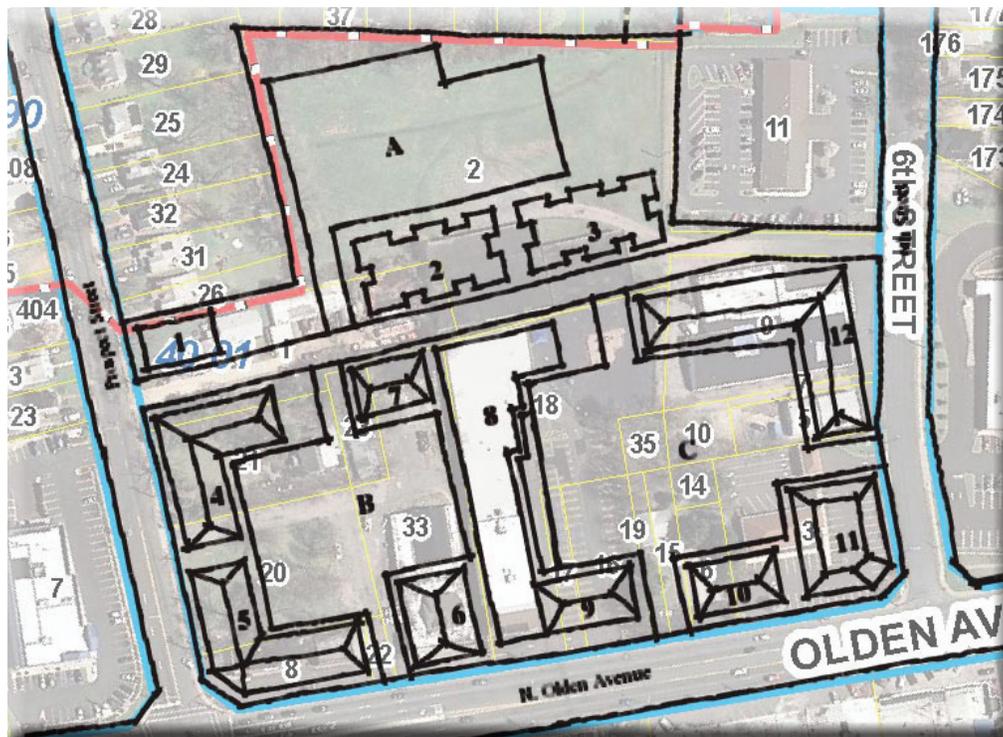


Figure 5- Sixth Street to Prospect (North Side Development Potential) Notice how through simple design measures that orient buildings to the street, that parking availability grows through the efficiency in the layout

**Conceptual Land Use Chart (Figure 5)**

Bldg. No.	Description	Height	Building Size (feet)	Floor Area/Units	Total Area	Parking Spaces	Parking Location
1	Commercial	2.5 stories	80 x 50	4,000	8,000 sft	16	Lot A
2	Condo Building #1	3.5 stories	150 x 80	12,000 (10 units)	36,000 sft (30 units)	39	Lot A
3	Condo Building #2	3.5 stories	150 x 80	12,000 (10 units)	36,000 sft (30 units)	39	Lot A
4	Mixed Use Building	2.5 stories	220 x 60 (centerline)		26,400 sft		
	Ground floor - retail			13,200 sft		27	Lot B
	Upper floor - office/residential			13,200 sft		14	Lot A
5	Mixed-use	2.5 stories	260 x 60 (centerline)		31,200 sft		
	Ground floor - retail			15,600 sft		32	Lot B
	Upper floor - office/residential			15,600 sft		16	Lot A
6	Mixed Use Building	3.5 stories	100 x 80		24,000 sft	32	
	Ground floor - retail			8,000 sft		16	Lot B
	2 Upper floors - office/residential			16,000 sft		16	Lot B
7	Professional Office	2.5 stories	80 x 50	4,000 sft	8,000 sft	8	Lot B
8	Senior Day Care Center (commercial?)	2.5 stories		34,000 sft	68,200 sft	136 *	Lot C
9	Restaurant (max. 60 seats)	2.5 stories	100 x 50	5,000 sft	10,000 sft	15	Lot C
10	Mixed Use Building	2.5 stories	100 x 60		12,000 sft	18	
	Ground floor - retail			6,000 sft		12	Lot C
	Upper floor - office			6,000 sft		6	Lot C
11	Mixed Use Building	2.5 stories	100 x 120		24,000 sft	36	
	Ground floor - retail			12,000 sft		24	Lot C
	Upper floor - office			12,000 sft		12	Lot C
12	Mixed Use Building	2.5 stories	340 x 60 (centerline)		10,000 sft	62	
	Ground floor - retail			20,400 sft		41	Lot C
	Upper floor - office/residential			20,400 sft		21	Lot C

\* Parking requirement per commercial standards at 2 spaces per 1,000 sft – to be revised

**Parking Efficiency Table (Figure 5)**

Lot No.	Area (approx.)	Total Spaces (approx.)	Parking For	
A	47,550 sft	140	Bldg 1. Commercial	16
			Bldg 2. Condos	39
			Bldg 3. Condos	39
			Bldg 4. Upper floor Office	14
			Bldg 5. Upper floor Office	16
B	36,400 sft (280 x 170)		Bldg 4. Ground floor Retail	27
			Bldg 5. Ground-floor Retail	32
			Bldg 6. Commercial / Office	32
			Bldg 7. Office	8
			Remaining Spaces	11
C	64,000 sft (320 x 200)	200	Bldg 8. Day Care Center	35
			Bldg 9. Restaurant	15
			Bldg 10.	18
			Bldg 11.	36
			Bldg 12.	62
			Remaining Spaces	34

\*\* Use revised parking numbers from above table. To be distributed in Lots B and C.

Working the Conceptual development opportunities with this Roadway Plan it is easy to see an Olden Avenue of the future that Ewing Township can be proud of. Notice Shop Rite in the background.



### Street Typology Profiles

Streets should be designed to reflect both the function they serve and the character of the area they exist in and support. Olden Avenue is the main spine of the Redevelopment Area, and two distinct sections are recommended corresponding to the future character as well as the function of the roadway. Three types of streets were developed for new network that will support Olden Avenue: a system connector, the largest roadway type; a local connector or major driveway; and a local alleyway or minor driveway.

### Olden Avenue Profiles

Olden Avenue's character changes as one travels along it, from the more neighborhood commercial feel between Parkway Avenue and Pennington Road, to the more commercial center feel between Pennington Road and Arctic Parkway, to the more highway commercial feel between Arctic Parkway and Princeton Avenue. Figure 6 depicts the cross sections for Olden recommended today with the potential of a third in the future: Princeton Avenue to Arctic Parkway, Arctic Parkway to Pennington Road, Pennington Road to Parkway Avenue. Two typical sections are recommended to address future economic development objectives today.

West of Pennington Road to Parkway Avenue, the recommended typical section is two 11 foot inside lanes, and two 14 to 16 foot outside lanes. The wider dimension provides space to share the lane with bicyclists. East of Pennington to Princeton Avenue, the recommended section has four 11 foot lanes, a 12 foot turn lane, 5 foot bicycle lanes on each side, and a 5 foot minimum sidewalk on each side.

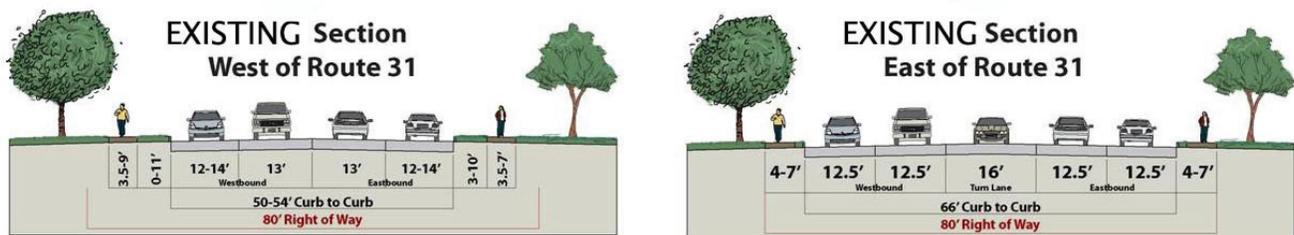
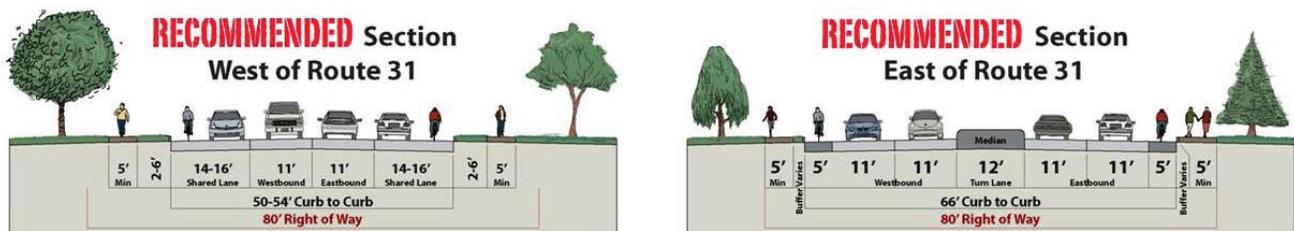


Figure 6 - Olden Avenue Roadway Profiles



*New Network Connector Profiles~*

**~System Connectors**

System connectors are two lane local streets serving local connections. A typical cross section would have a width of 48 feet from curb to curb, and an overall right-of-way width of 70 feet. This roadway type manages speeds of 25 to 35 mph, and has a median width that varies between 10 to 16 feet. System connectors are bike compatible, and have sidewalks on both sides of the road. On street parking is optional and would add 8 feet to the cross section if installed. Figure 8-1

Figure 8-1

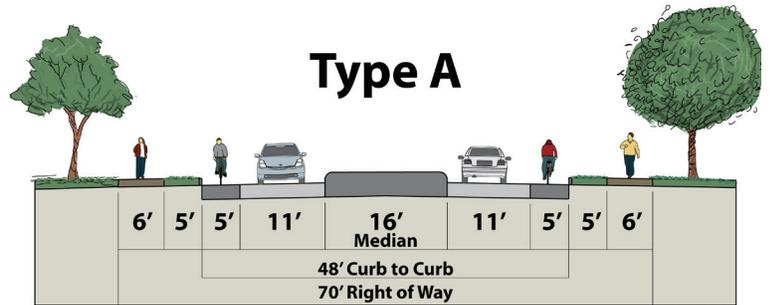


Figure 8-2

**~ Local Connector or Major Driveway**

A Local Connector could be a roadway or a major driveway. It serves local access and circulation and provides two-way travel. This roadway type manages speeds of 25 mph or less, and provides sidewalks on both sides for pedestrians, and bicycle facilities on both sides of the road. Because the road handles low speeds and volumes, bike usage could be provided in the active lanes, with Share the Road signs. Figure 8-2

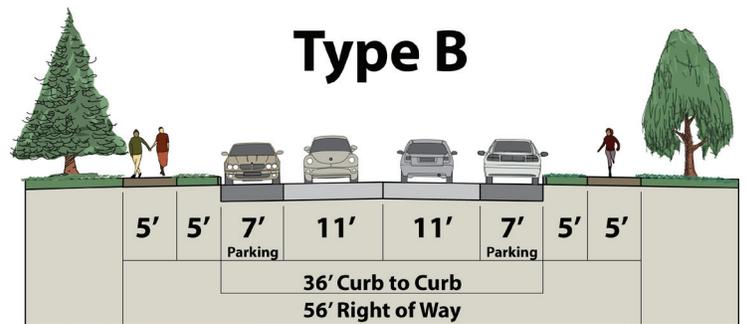
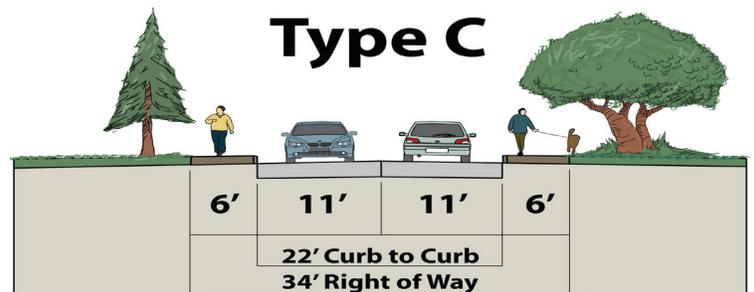


Figure 8-3

**~ Local Alleyway or Minor Driveway**

A local alleyway or minor driveway is the smallest street provided in the network. It is intended to provide access between blocks or uses between blocks only, and would manage speeds of 10 to 20 mph. Because of the very low speeds and volumes for this roadway type, sidewalks could be unbuffered and be placed adjacent to the curb, and bicycles would share lanes with vehicles. Furthermore, these connectors could be pedestrian only - particularly those areas adjacent to existing neighborhoods. Figure 8-3



### Network Recommendations

Recommendations for network are detailed as follows. These recommendations are made to guide redevelopment and negotiation of implementation toward the longer term vision for the District. The illustrations were developed after analysis of local conditions and the practical needs of local business. This network of streets and/or driveways will work to enhance accessibility and mobility. While developed with this in mind, final site plan design may cause alternatives to come forth. As long as the goals of the Plan are met, these adjustments are appropriate.

#### ~ Parkside Avenue to Arctic Parkway

From Parkside Avenue to Arctic Parkway, a network of local connectors provides parallel and perpendicular connections to Olden Avenue. A local alleyway parallel to Olden Avenue connects Parkside Avenue to 5th Street for service access if feasible.. (Figure 9)

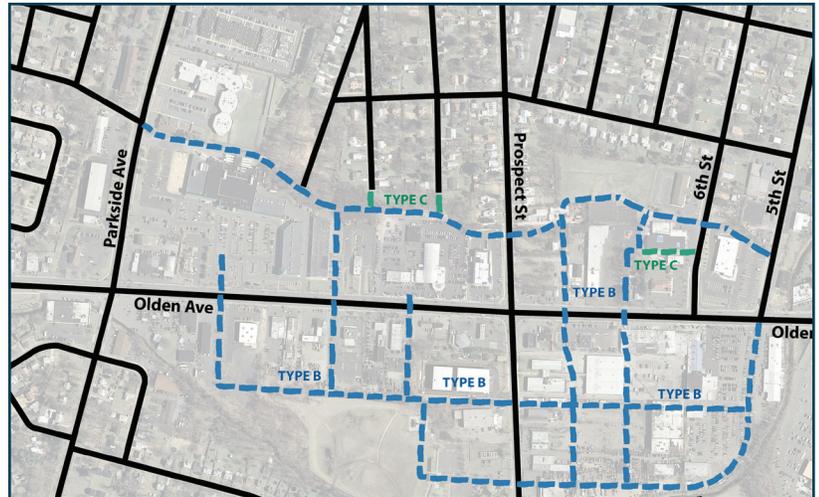


Figure 9 -

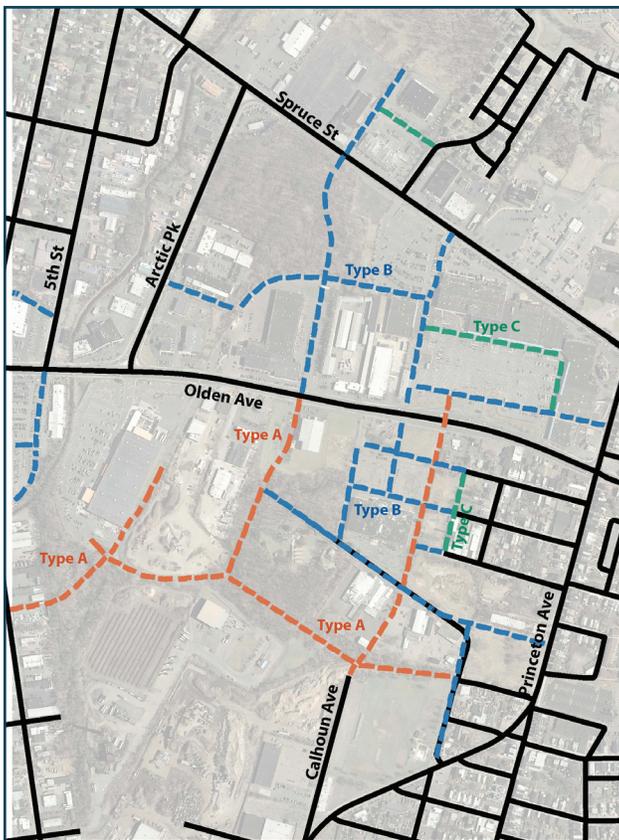


Figure 10 -

#### ~ Arctic Parkway to Princeton Avenue (Figure 10)

From Arctic Parkway to Princeton Avenue, a system connector (Calhoun Street Extended) will provide important connections across and parallel to Olden Avenue with additional network. Local connectors also provide parallel network to Olden Avenue, and local alleyway completes new blocks, providing additional opportunities for economic development. (Figure 10)

#### ~ Fifth Street Extension

Fifth Street Extension is recommended to extend from Spruce Street through to Industry Court in Subarea 3. Pedestrian and bicycle connections to a future greenway connector shall be incorporated into the final design.



### 3. Corridor Improvements

Olden Avenue must provide bicycle and pedestrian accommodations for the entire length of the corridor. A good pedestrian environment is an essential element to developing a successful business environment as it provides a variety of means of access and consumer comfort; even while driving. Pedestrians must feel safe and comfortable walking, whether between establishments or from a parked car to an establishment. A well-designed pedestrian environment improves safety for both pedestrians and drivers. The desired section for the pedestrian realm shall include 5 foot minimum sidewalks, a step strip where a sidewalk is less than 2 feet or where a grass strip exceeds 2 feet.

#### Pedestrian & Bicycle Accommodations (Figure 11)

##### ~Pedestrian

- While Olden Avenue and its associated network have sidewalks for a good part of their lengths, they are often in poor condition or interrupted by utility poles and numerous curb cuts. Sidewalks in the Capital Plaza area are non-existent. Continuous, unobstructed sidewalks should be provided on both sides of Olden Avenue.
- All new development shall provide new sidewalk to connect from the street and curb line sidewalk to the building(s).
- New concrete sidewalk and apron must be provided in all driveways intersecting with Olden Avenue.
- All parking lot repaving must adhere to this standard whether associated with new or existing development.

##### ~Bicycle

Bicycles are accommodated along Olden Avenue in one of two ways: (1) wide outside shared lanes, where bicyclists are accommodated in the wide outside portion of the lane, and (2) separate bicycle lanes, where a dedicated 5 foot lane is provided on the outside of the lane. Between State Route 31 (Pennington Road) and Parkway Avenue, bike compatible shared use lanes are recommended. Between State Route 31 (Pennington Road) and Princeton Avenue, dedicated bike lanes are recommended on either side of Olden Avenue.

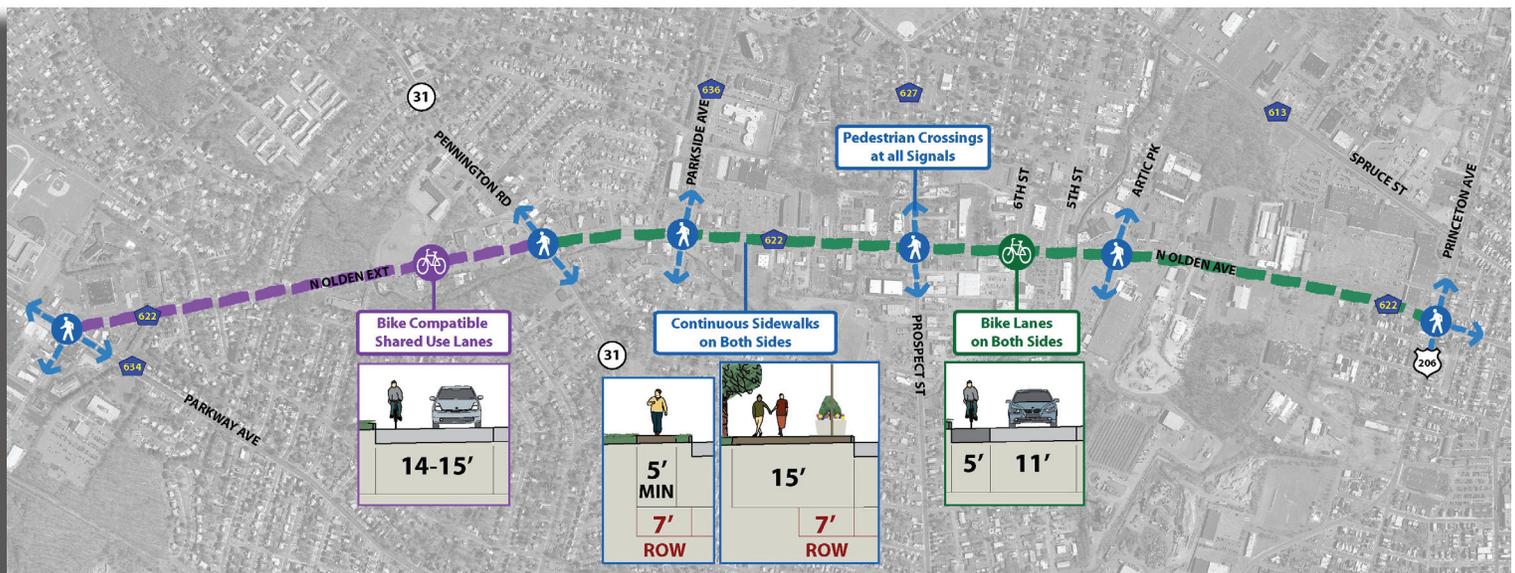


Figure 11 -

### Pedestrian-based Intersection Improvements

Each intersection shall have clearly marked and highly visible pedestrian crosswalks and must meet ADA requirements for universal design. While many types of crosswalk markings are used, the ladder style is the preferred pattern, as it is the easiest to see. Figure 11-2 depicts typical markings with the ladder being the last. Over time, alternative-paving materials should be installed to further define crossings.

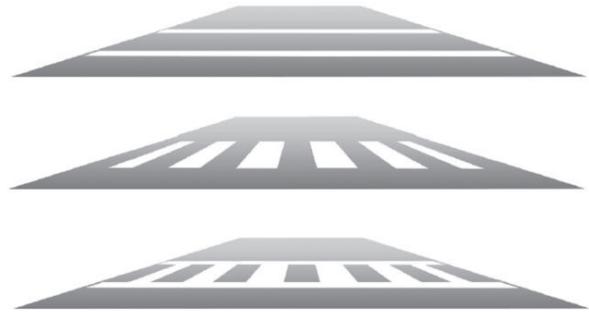


Figure 11-2

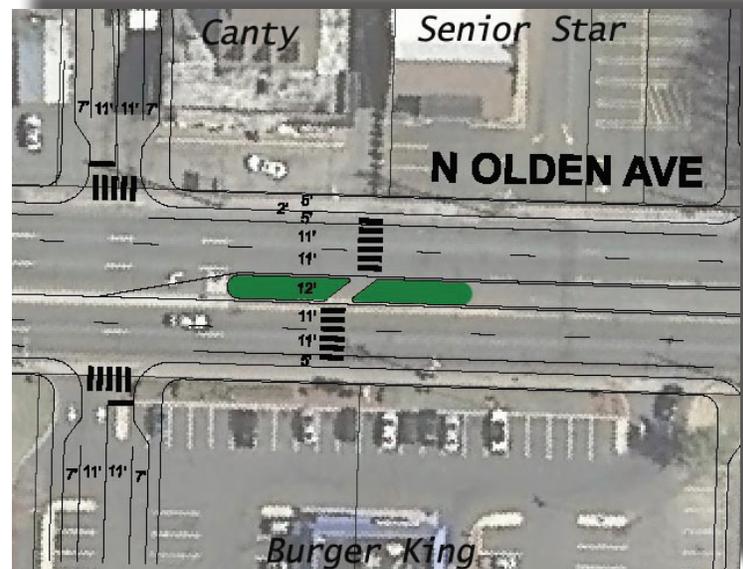
Pedestrian signals should be provided on all directions, and all crosswalks where pedestrians cross should be marked. Signals should be timed to give pedestrians adequate crossing time. Stop bars should be marked at each approach.

### Mid-block Crossings

Mid-block crossings provide formalized, dedicated crossing opportunities for pedestrians outside of intersections. Since the Olden Avenue block structure are so long in nature, mid-block crossings are recommended to not merely provide safe crossings of the Avenue for pedestrians and bicyclists, but they become an element of traffic calming as well. As redevelopment continues, the need for additional mid-block crossings in the core area should be considered. Initially however, mid-block crossings should be considered for installation at:

- Parkside Plaza linking to a NJ TRANSIT bus stop directly across the street.
- In the vicinity of McMullen Drive and Shabakunk Creek Park
- In the block between Arctic parkway and Princeton Avenue.

For minor streets and driveways, smaller turning radii shortens pedestrian crossing distances and lessen the conflict between turning vehicles and pedestrians. Radii should be designed to accommodate the types of vehicles that will frequently turn off Olden Avenue onto the minor street or driveway; radii should be designed to reflect “effective” turning radii, taking into account bike lanes. The Institute of Traffic Engineers (ITE) Context Sensitive Design Guidebook recommends a minimum curb radii or 10’-15’ where high pedestrian volumes exist or are anticipated, turning volumes are low, lane widths can accommodate passenger vehicles with no encroachment on adjacent lanes, passenger vehicles are the majority of turning vehicles, bike or parking lanes provide extra room for turning vehicles, low turning speeds are desired, and occasional encroachment of large vehicles is acceptable.



Prototypical mid-block crossing along a redesigned Olden Avenue

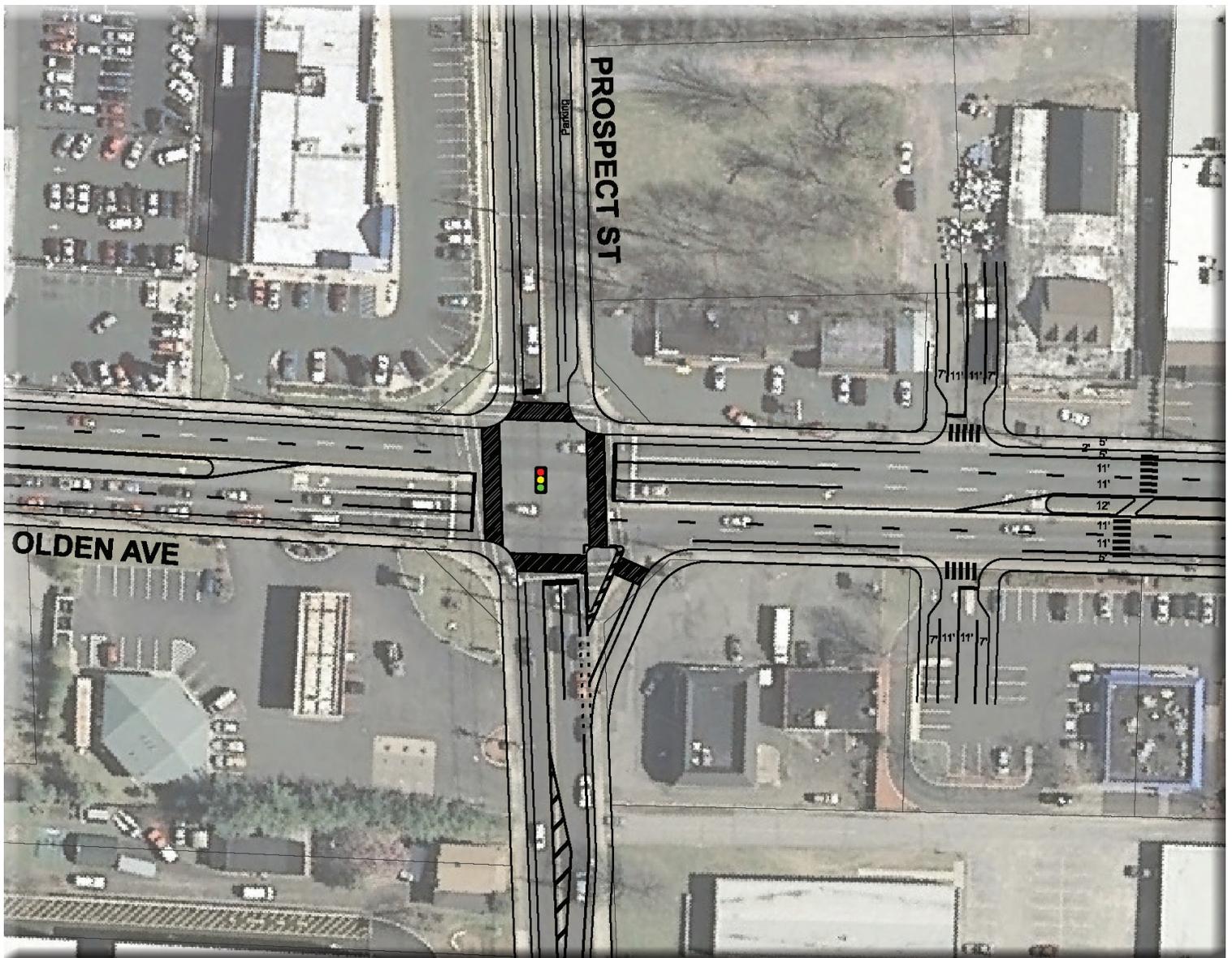


#### 4. Intersection Design

Intersections along Olden Avenue could be one of two types: traditional four-legged intersections controlled by a traffic signal, or a roundabout. This plan recommends that while both types are appropriate, roundabouts can add a key amenity and feature of both access and aesthetic that will work well with the needs of commerce along the corridor.

##### Traditional Intersection

Several features are recommended to make a traditional intersection function more safely and efficiently for both vehicles and pedestrians. The curb radius established at each intersection will affect pedestrian safety. Large curb radii increases the distance and time a pedestrian is in the roadway, and increases the speed that vehicles can make a turn. Curb extensions increase visibility for both drivers and pedestrians, reduce curb radii, serve to slow vehicles and reduce crossing distance for pedestrians. The example presented in Figure 12 is a good example of how lane widths and pedestrian crossings are coordinated.



### Roundabouts

A modern roundabout offers an alternative to a signalized intersection, if factors such as volumes and geometry allow. A roundabout typically has a center island with a mountable ring, which allows trucks and oversize vehicles space to maneuver, and a splitter island and pedestrian refuge at each of the four approaches which allows pedestrians safe crossing. Figure 13 depicts the typical elements of a round-a-bout.

A roundabout can have advantages over a signalized intersection in several ways. A roundabout provides fewer conflicts than a signalized intersection and the geometry lowers vehicle speeds, making it a good choice in a business environment where the goal is to slow traffic. As applied to a four-lane section of road, Figure 14 depicts how it will function.

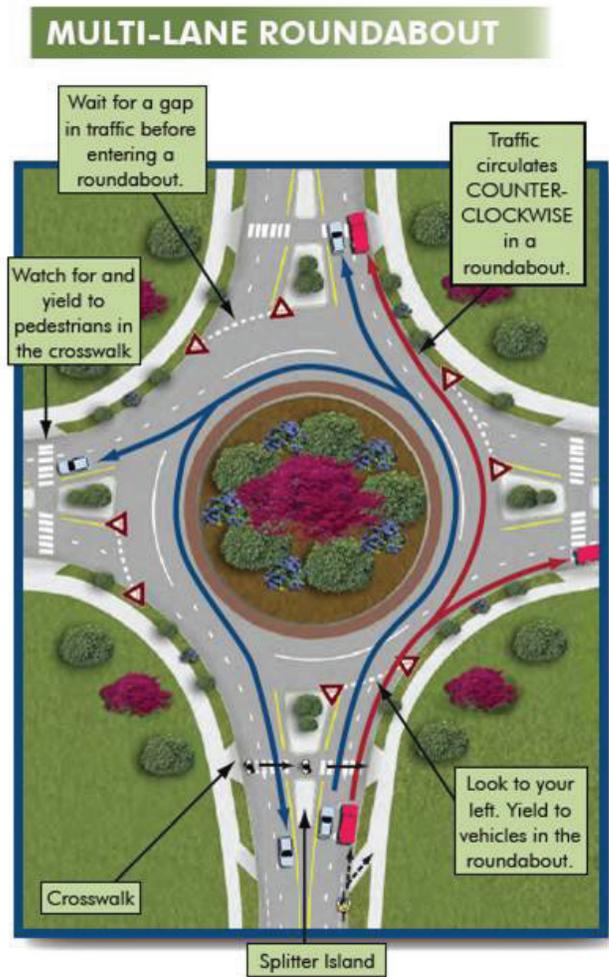
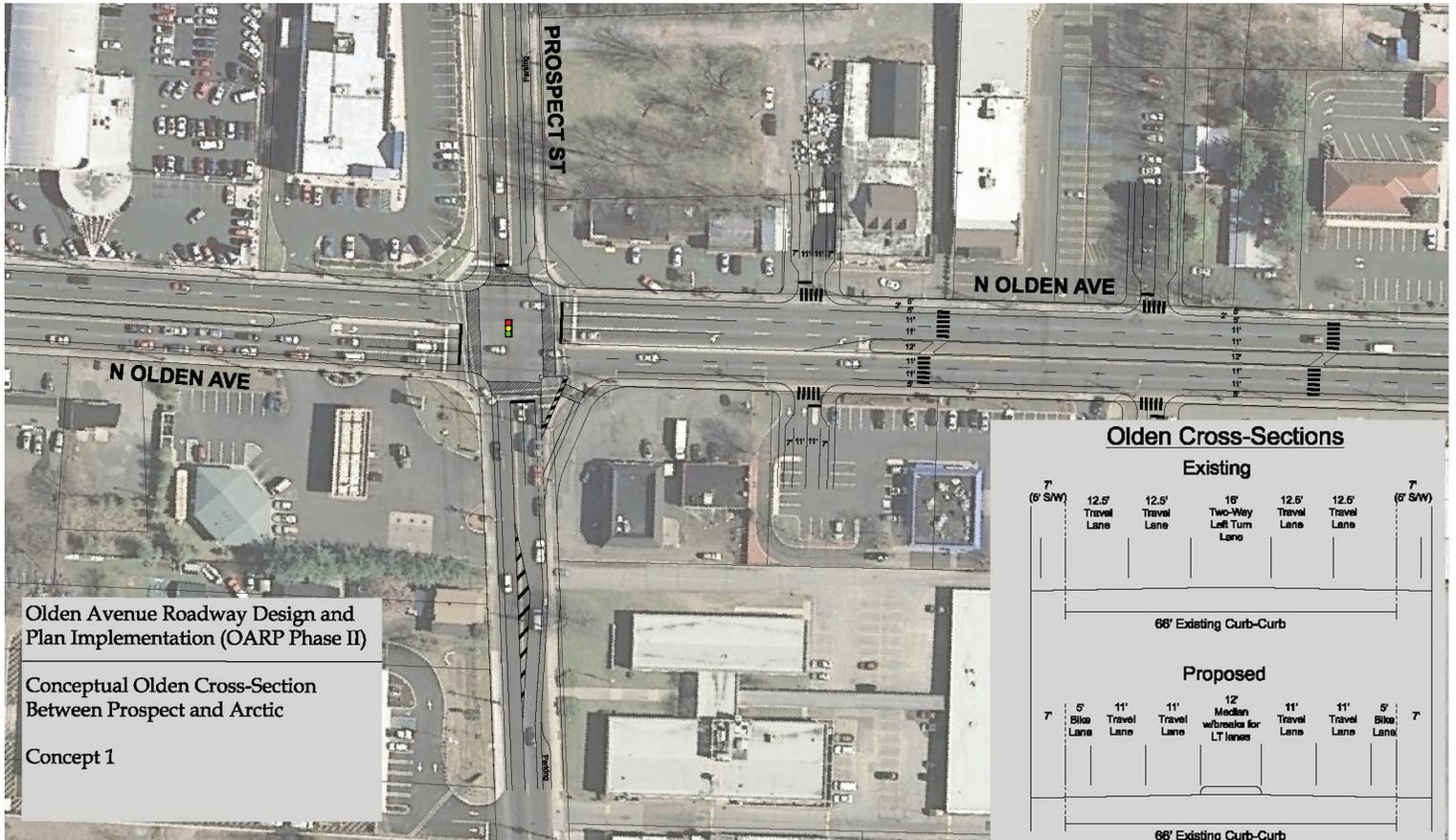


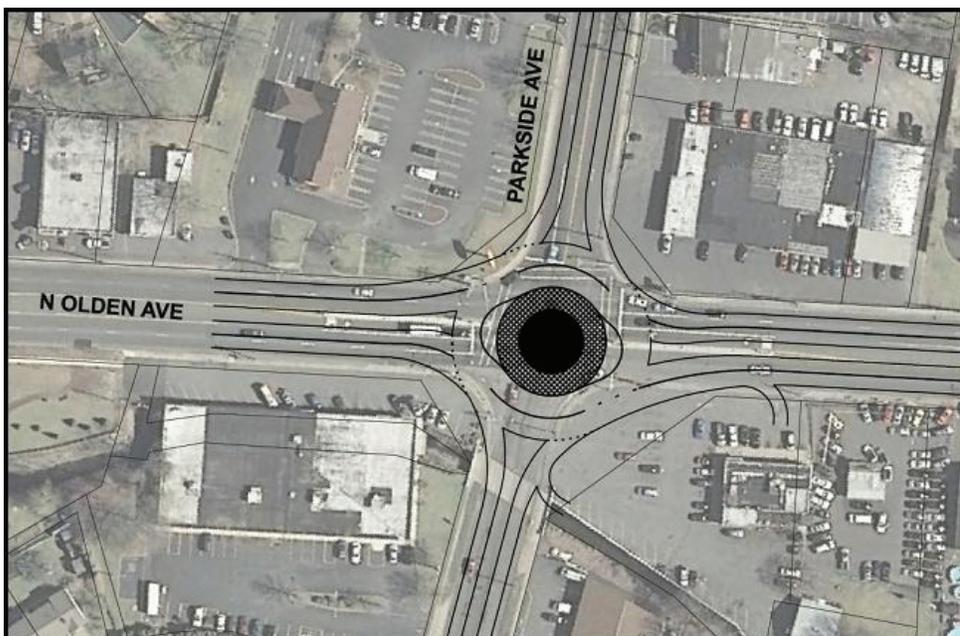
Figure 13

Figure 14

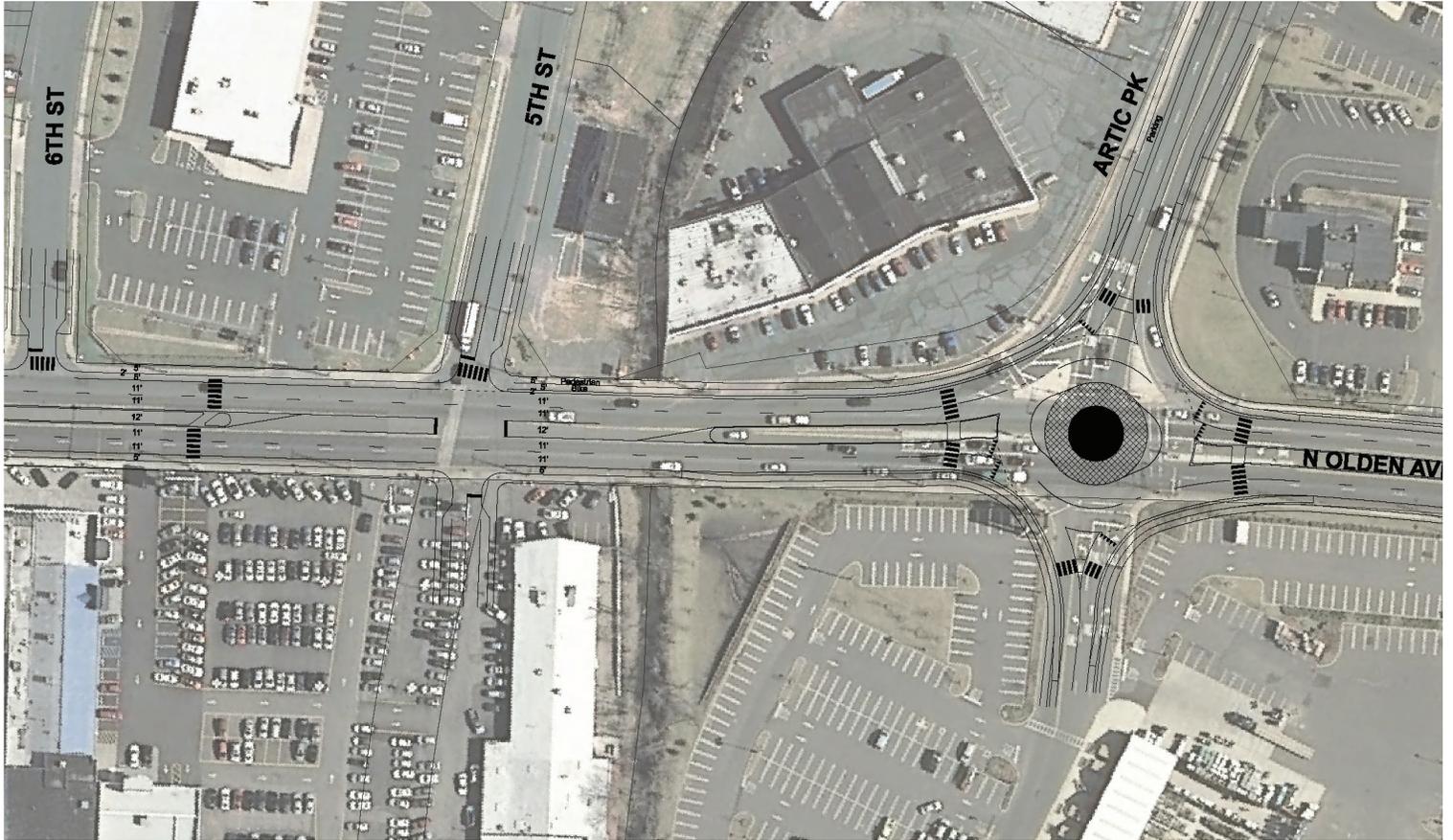




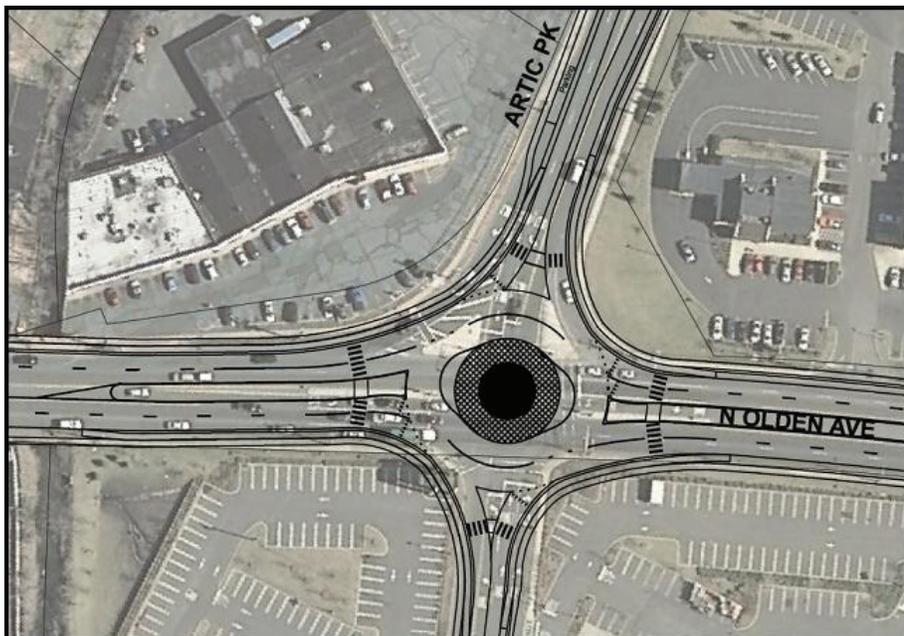
Each intersection in the Olden Avenue corridor was evaluated for its feasibility as a roundabout. While all are candidates, the Parkside Avenue, Prospect Street and Arctic Parkway intersections are the best candidates for roundabouts, based on geometry and traffic volumes with Parkside Avenue and Arctic Parkway as the primary candidates and Prospect Street remaining a highly pedestrian-friendly traditional intersection. This recommendation was coordinated with the future land use planning that envisions the Parkside Avenue and Arctic Parkway round-a-bouts treated as “gateways” into the of the Redevelopment Area Core.



*Converting the Parkside Avenue intersection into a modern round-a-bout will provide an aesthetic gateway treatment for the business district with a very limited, if any, property outside of the existing County-owned right-of-way..*



*The Arctic Avenue intersection with Olden Avenue presents numerous conflicts with access adjacent land uses and motorists attempting to access Olden from Arctic. Converting it to a modern round-a-bout will improve access, calm traffic and provide an aesthetic gateway treatment for the business district.*



## Public Transit

The Public Transportation Map in the Township's Master Plan shows that the Olden Avenue corridor is under-served by public transportation, whether publicly or privately operated. ETRA will continue to work with the State and County to improve public transit in the redevelopment area, including an additional bus route, bus stop shelters and pull-offs. All final decisions on bus stop locations and associated amenities shall be made by the Ewing Township Planning Board in consultation with NJ Transit, County of Mercer and the Ewing Township Redevelopment Agency. Longer-term initiatives should include the continued exploration of jitney and trolley service. This Plan supports the concept of connecting the Bus Rapid Transit project for Route 1 with the Olden Avenue Redevelopment Area. However, a more ambitious exploration of light rail or other mass transit connections like Bus Rapid Transit should never be ruled out as a possibility in the future, especially when considering potential linkages between West Trenton and Trenton Rail Stations.



*The existing bus stop of Olden is in need of improvement. Working in concert with the recommended mid-block crossings, the area has great potential to become a fully functioning corridor for motorists and pedestrians.*

## Overhead Utilities

The unsightliness of overhead wires and poles was cited as one of the factors contributing to a poor visual image for the Olden Avenue corridor. However, the Planning Board and the Ewing Township Redevelopment Agency are realistic in their limited ability to relocate utilities (including connections) underground, and/or to relocate utilities off existing right-of-ways as a policy of this Plan. Given the technical and procedural difficulties and high costs in achieving this, particularly along Olden Avenue, streetscape design along the corridor must work to soften the aesthetic impact of these utilities. To that end, the recommendations for Olden Avenue take into consideration the overhead utilities and proposes to place any future landscaping on the land side of all right-of-way activities. Such a policy will ultimately work to reduce visual impact through the notion of creating a "green" backdrop.

Prior to the issuance of any permits to access North Olden Avenue Extension, Ewing Township shall require a 15-foot right-of-way easement for sidewalks and landscaping, along with the installation thereof, when development is proposed if adequate right-of-way does not exist. The easement will be utilized for new sidewalks, bike lanes, landscaping, signage and other streetscape improvements. The design of the sidewalks and streetscape will be in accordance with this Plan's Roadway Design Section.

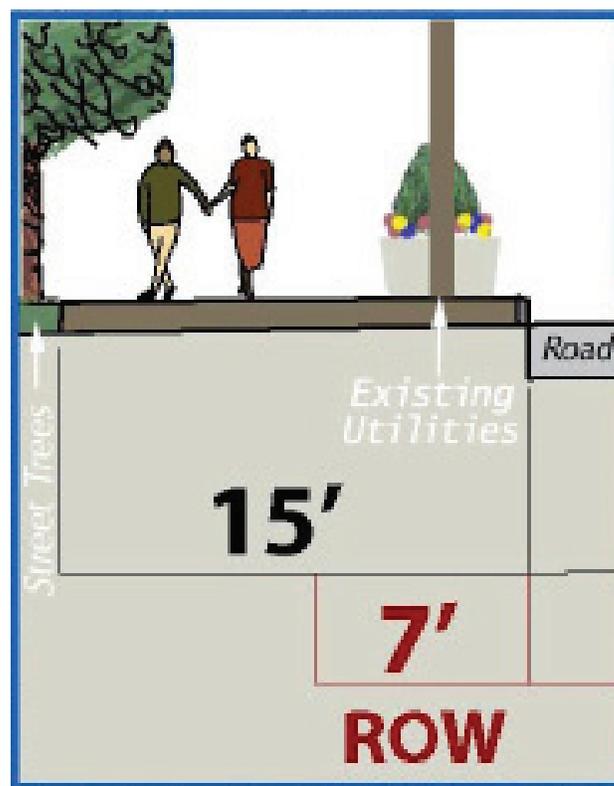


*Bus shelters should be attractive, well-maintained, and can include an artistic element that reflects and celebrates community character.*

The relocation of utilities is an express power of the Redevelopment Agency under the New Jersey Redevelopment and Housing Law. However, the cost of removing poles and wires, digging trenches, installing conduit and restoring the curbing, sidewalk and landscaping are not incurred by the utility company. Such improvements will need to be viewed as capital improvements by the Township if they are to be realized, working in cooperation with the utility companies, Mercer County and individual redevelopers and property owners. The State Board of Public Utilities (BPU) adopted the Smart Growth Infrastructure investment program (SGIIP) under N.J.A.C 14:3-8.12 which serves Planning Area 1 under the State Development and Redevelopment Plan. (Olden Avenue is located in a Planning Area 1.) Under the BPU's program, rules allow for reimbursement to the entity (developer) who is relocating the service. The additional rate-payers (in Ewing's case, added PSE&G customers) in the system is the source of revenue for this reimbursement.

While the BPU utility relocation program works for major redevelopment projects, rehabilitations and small-scale redevelopment that do not add additional rate-payers do not benefit from such a program. In this case, either the Redevelopment Agency or the Township can bond for such an improvement project, and either amortize the bonds through the general tax levy or through service charges paid by redevelopment projects that have executed long-term tax abatements with the Township. Any bonds amortized through service charges for redevelopment projects will not count against the municipal cap on indebtedness. The Mercer County Improvement Authority may also be able to play a role in the financing strategy.

With regard to the responsibility of private property owners for relocating utilities and associated costs, existing property owners who do not redevelop or substantially rehabilitate their properties will not be required to move their utilities or pay for the re-connection at the property line when the overhead wires in the public right of way are buried. All new development and redevelopment projects, as well as substantial rehabilitation of existing properties, will continue to be required to provide streetscape improvements in accordance with the provisions of this Plan. Where feasible, underground placement of utilities is encouraged.

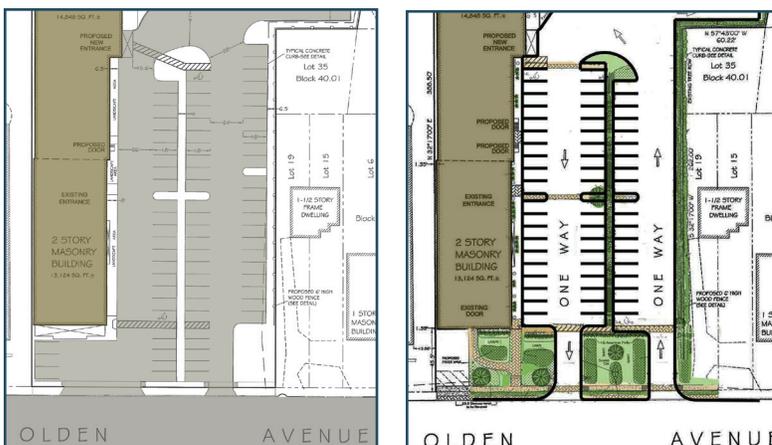


## IMPLEMENTATION OF THE PLAN

Design is a physical indication to a visitor entering a community. Therefore, in order to reflect the pride a community has, it must manifest itself both on the ground and in our streets with regard to how we care for and implement development. Intuitively, when human beings enter a space they know or immediately feel if it is safe and hospitable. As documented throughout this Plan, Ewing Township must work to ensure the feeling it creates within the business district yields favorable reactions - particularly for visitors.

As issues in the Area have developed over several decades, the opportunities this Plan seeks to take advantage of will also occur over time as private investments are made and funding for infrastructure obtained. While the chart on the opposing page outlines a rough listing of projects that require action, is not all-inclusive. The chart does however provide a plan of action that investors seek when looking to invest in a place. Given the uncertainty of funding for specific projects, particularly large infrastructure projects, all steps forward are important.

As this Plan pertains to this roadway infrastructure section, implementing the actual redesign of Olden Avenue will be a difficult, but worthwhile endeavor. Taming this road network will be critical toward creating a rational and healthy ratable base. Part and parcel to taming the roads is how private investment from an economic development perspective is designed. A good example of Plan implementation by the private sector is the recent "Senior Star" application currently being constructed. As can be derived from the graphic, the changes made to the site plan for the project setup the likelihood that future development proposed adjacent to the site can share the driveway and coordinate parking in the rear in such a way that allows more actual spaces to be created through the efficiency in design. The street edge will also be aesthetically pleasing with signage and landscaping working toward implementing the vision of this Plan.



Implementation of the Plan at the Planning Board in 2010 yielded favorable results for the Senior Star Adult Day Care facility. The redesign of the application's site plan (right) was conducted in accordance with this Plan and its subsequent approval set up an aesthetically pleasing street frontage, improved pedestrian safety, and efficient access. The positioning of its driveway was done so to allow for future coordination with adjacent property owners to share access and coordinate an efficient parking layout when development is proposed over time. The graphic on the left was the original submittal which included no aesthetic improvements.

Improvement of Olden Avenue Redevelopment Area will not occur overnight and the Township must do what it can to keep advancing it forward - even if it means simply coordinating simple community clean-up days, or property maintenance sweeps for violations, such as illegal signage.

Projects worth pursuing to advance this Plan include:

- 1.) Working with the County of Mercer and the Area's business community. Create Plan action committees that include members of the business community, surrounding residents, members of other boards and commissions to begin prioritizing efforts that provide the most bang for the buck and leverage collective resources;
- 2) Coordination of a community clean-up/painting days involving youth clubs and community organizations;
- 3) Development of a list of projects that ETRA and the Township can pursue funding or partnerships on, including;
  - a. A streetscape pilot project for Subarea 1 that coordinates with the re-striping of the Avenue to Parkway;
  - b. Work with the Church of the Incarnation to create a public park on the open lawn that fronts Olden Avenue. Research and resolve potential liability issues and pursue funding;
  - c. Designing a Façade improvement program
- 4) Pursue design, and funding for, 5th Street Extension and the Ewing/Lawrence Greenway toward Industry Court;
- 5) Soliciting a non-for-profit (such as ISLES) to pursue development and implementation of a Neighborhood-based Revitalization Plan the purpose of which is to identify projects for the State's Tax Credit program. Stakeholders include the Trent Street neighborhood, Way of the Cross Parish, residents of Donnelly Homes, and the City of Trenton;
- 6) Extend the flood protection program by encouraging the Township to create a regional detention basin behind the public works yard;
- 7) Work with the business community of development of a Marketing Plan the includes wayfinding, and branding of the district;
- 8) Work to enhance a strategy to assist in zoning code enforcement and property maintenance.

OARP Phase II Roadway Infrastructure Implementation Plan

Timeframe/Item	Lead	Support	Cost Estimate*	Potential Funding Sources
<b>Early Action</b>				
1 Modify Right Turn from Arctic to Olden	Mercer County	Ewing Township Redevelopment Agency (ETRA), Ewing Township	Mid	Public
2 Upgrade Prospect and Parkside signals with pedestrian improvements	Mercer County	ETRA, Ewing Township, Private Developers?	Low	Public
3 Provide midblock crossings. Need ordinance for each by County	Mercer County	Ewing Township	Mid	Public
4 Restripe crosswalks at intersections and add more pedestrian signage	Mercer County	ETRA, Ewing Township	Low	Public
5 Convert (Restripe) Olden between Pennington and Parkway into a 3 lane section	Mercer County	NJDOT, Ewing Township	Mid	Public
6 Complete missing sidewalks	Ewing Township	ETRA, Mercer County	Mid	Public/Private
7 Get Plan on the County Transportation Improvement Program	Ewing Township, Mercer County	Mercer County / DVRPC	Low	Staff
8 Obtain Funding for Olden Avenue redesign (engineering and planning)	Ewing Township, Mercer County	NJDOT / DVRPC	High	Public
9 Work with NJ TRANSIT to develop Bus stop and shelter	Ewing Township, Mercer County	NJ TRANSIT	Low	Public
<b>Midterm Action (5 years)</b>				
1 Initiate engineering study for Calhoun Connection	Mercer County	ETRA, DVRPC, Ewing Township, Lawrence Township	Mid	Public/Private
2 Work with County to advance Access Management Plan that supports Olden Avenue Redevelopment Plan.	Mercer County, Ewing Township	ETRA	Low	Public/Private
3 Provide bike lanes on Olden Avenue where not provided by Plan	Mercer County	ETRA, Ewing Township	Low	Public
<b>Long Term Action (Beyond 10 Years)</b>				
1 Advance Intersection Improvements (Roundabouts?)	Mercer County	NJDOT, Ewing Township	High	Public/Private
<b>Ongoing</b>				
1 Streetscape Improvements	ETRA, Private	Mercer County, Ewing Township	Variable	Public/Private
2 Work to Implement Plan	ETRA, Ewing Township, Mercer County, Private			

- Low – (under \$30,000), Mid – (\$30,000-\$100,000), High (\$100,000-\$1,000,000),

## Parks & Open Space

Parks and Open Spaces are critical to the creation of places where people can enjoy a active, healthy lifestyle. To this end, this Plan has created a conceptual framework whereby new development can contribute toward the creation of additional public spaces. The Parks & Open Space Opportunities Map (Map 7) serves as the foundation for such an effort. The map details where opportunities can become integral to strengthening the form of development while enhancing environmental quality and flood protection. As stated in the Land Use Standards, in order to receive consideration of the enhanced standards contained in this Plan, implementation or pro-rata contributions will be made to further the intent of this section.

Some of the concepts in this Plan include;

- Reclaim and restore the floodplain along the Shabakunk Creek
- Enhance and create superior pedestrian connections to Moody Park.
- Initiate discussion with Incarnation Church to improve their open space along North Olden Avenue as an enhanced public amenity.
- Work with the Ewing Environmental Commission to implement the Ewing Lawrence Greenway initiative. Consult the Historic Commission on the restoration of the railroad trestle that crosses the Shabakunk Creek just south of Olden Avenue.
- Explore the potential of creating additional regional flood control infrastructure. Such facilities should be considered as an additional public park space where appropriate.
- Work with potential developers on the expansion of Stout Avenue Park
- Collaborate with the City of Trenton and Mercer County for the improvement and expansion of Trenton's Father Rocco Park into Ewing.
- Explore the intersection of Arctic Parkway & Spruce in Sub-area 4 and the public works facility in Sub-area 3 as potential regional flood-basins/public park facilities
- Transform the former Ewing Golf Range Flood Basin into a multi-functioning basin/public park
- Enhance pedestrian connections to the Farmers Market.



*Father Rocco Field in Trenton*



*subarea 5; Stout Avenue Playground*



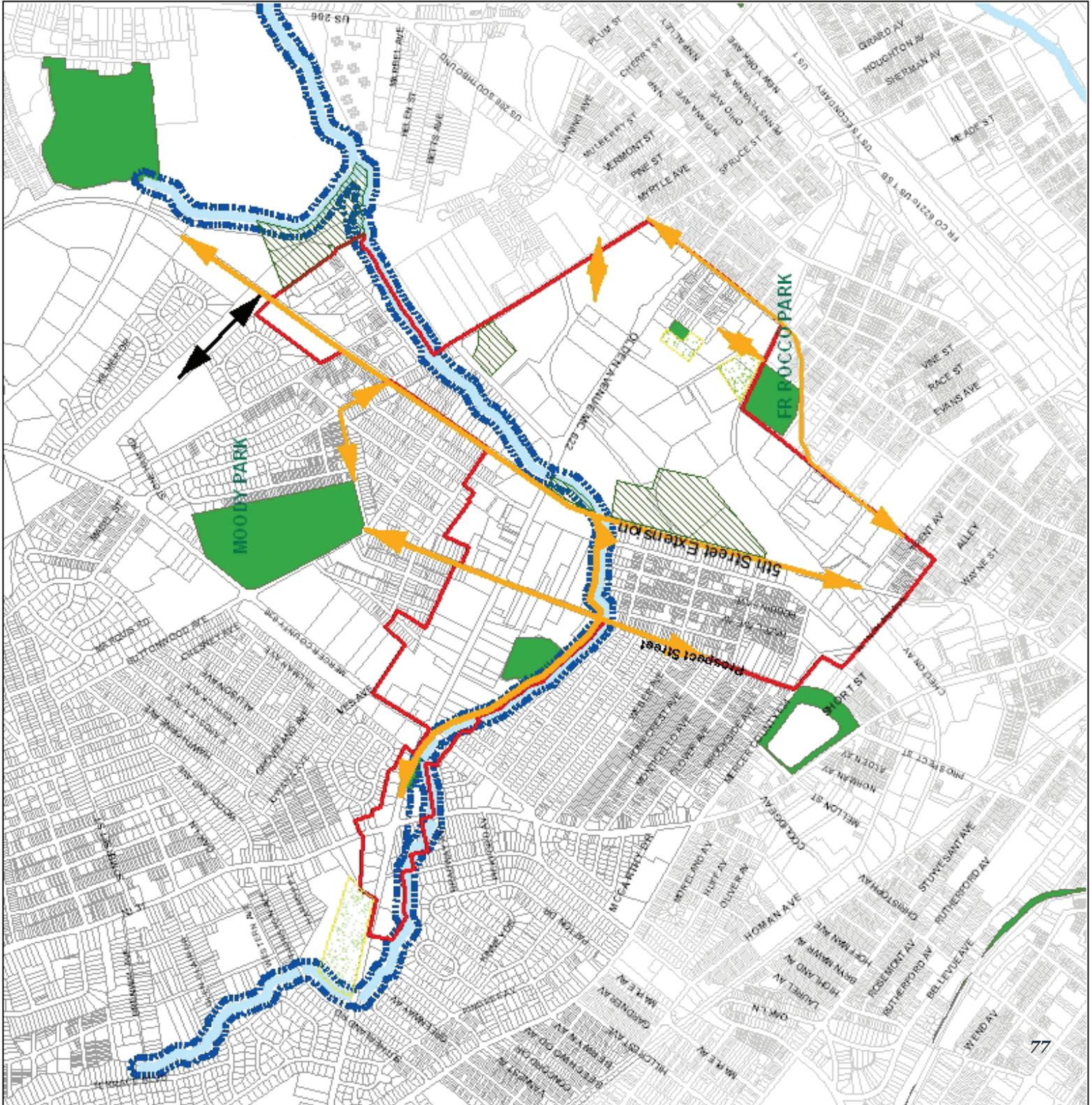
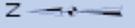
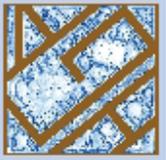
*subarea 2; The former Ewing Golf Range*



*subarea 1; Shabakunk Creek Park*

# PARKS & OPEN SPACE OPPORTUNITIES MAP

- Legend**
-  Shabakunk Creek
  -  Reclamation of stream bank, floodway and floodplain
  -  Strengthen pedestrian and greenway connections
  -  Existing Open Space
  -  Potential Park Space
  -  Environmental restoration, food control, park space
  -  Redevelopment Area



# PURPOSE AND POLICIES

The Redevelopment Plan serves as the principal tool to not merely guide the revitalization of the Olden Avenue Redevelopment Area but promote it as well. While ETRA and the Township Council approve the Redevelopment Plan, the residents, business and property owners of Ewing Township have the opportunity to recommend modifications and improvements to the plan as needed for the plan to remain current and relevant. Successful implementation will require continuous outreach to the residents and commercial property owners in the redevelopment area and judicious use of redevelopment tools and financial incentives by the Ewing Township Redevelopment Agency.

## Relationship to Local Objectives and Municipal Land Development Regulations

A review of the zoning prior to the adoption of this Plan indicates that the current zoning districts within the Redevelopment Area are predominantly B-H (Highway Business), IP-2 (Industrial Park B 2), IP-3 (Industrial Park B 3), with a small area of R-3 Residential zoning in the Stout Avenue neighborhood and a small area of PRO (Professional, Research, Office Zone) at and around the bank site on Pennington Road south of Olden Avenue. While some of these standards are consistent with the intent of this Plan, many of the Land Development Ordinance provisions are contrary to a unified vision. As such, the underlying zoning facilitates many of the issues that this Plan seeks to remedy. This Plan supersedes the "Underlying Zoning".

The Olden Avenue Redevelopment Area shall be redeveloped in accordance with the standards detailed in this Plan. As of the adoption of this Plan, the Township is committing to creating a smooth and predictable process whereby this Plan will supersede use, bulk, and design standard provisions of all remaining Ewing Township Land Use Regulations continue to apply. Additionally, any provision or standard of development that the Redevelopment Plan that is silent on the Ewing Township Land Development Ordinance shall apply.

## Role of Township Boards

After initial consultation with the Ewing Township Redevelopment Agency, all development applications taking advantage of the provisions set forth in this Redevelopment Plan shall be submitted to the Planning Board through the normal site plan and subdivision procedures as identified in N.J.S.A. 40:55D-1, et seq. The Planning Board (but not the Board of Adjustment) may grant minor deviations from the regulations contained within this Redevelopment Plan, where, by reason of exceptional narrowness, shallowness or shape of a specific piece of property, or by reason of exceptional topographic conditions, pre-existing structures or physical features uniquely affecting a specific piece of property, the strict application of any area, yard, bulk or design objective or regulation adopted pursuant to this Redevelopment Plan, would result in peculiar and exceptional practical difficulties to, or exceptional and undue hardship upon the developer of such property. The Planning Board may also grant such relief in any application relating to a specific piece of property, where the purposes and intent of this Redevelopment Plan would be advanced by such deviation and recommended by the Ewing Township Redevelopment Agency. The benefits of any deviation must outweigh any detriments. The Planning Board may grant minor design waivers for signage, streetscape improvements, provided that consistency with adjacent properties and projects are found.

Again, no relief may be granted under the terms of this section unless such deviation or relief can be granted without substantial detriment to the public good and will not substantially impair the intent and purpose of the Redevelopment Plan. An applicant for a deviation from the requirements of this Redevelopment Plan shall provide public notice of such application in accordance with the public notice requirements set forth in N.J.S.A. 40:55D-12(a) and (b).

Notwithstanding the aforementioned, any deviation from the permitted use, or height provisions of the 2009 Olden Avenue Redevelopment Plan that exceeds the Planning Board's ability to grant a 10% increase or 10' feet, whichever is less, can only be made through a formal Plan amendment by the Township Council in accordance

## *From Concept to Approval....*

### **Procedure & Approval Process**

All projects within the Olden Avenue Redevelopment Plan area will follow the following procedure as a supplement to the Township's development review process. The purpose is to ensure consistency and predictability for all applicants and investors as projects are advanced for consideration.

1. *Upon applicant's contact with the Township, the applicant will be referred directly to the Ewing Township Redevelopment Agency (ETRA) for all projects located within the Redevelopment Area. Either through the Township Zoning Official or ETRA, a set of procedures with all submission requirements and contact information will be made available upon first contact with the Township.*
2. *Once referred to ETRA, the Executive Director may hold an initial meeting with the prospective developer and/or property owner to determine:*
  - a. *The level of detail necessary for ETRA administrative review and/or agency recommendation.*
    - i. *Should ETRA professional staff be required for formal review of an application before the agency, conceptual or otherwise, those professionals may be required as necessary to carry the project throughout the entire Planning Board approval process. The Ewing Township Land Development Ordinance pertaining to professional escrow fees shall apply in these cases. In the cases when the Agency's professionals are not deemed necessary, the Planning Board's professional will be engaged.*
  - b. *The most appropriate meeting date for project review, or;*
  - c. *If the project should be referred directly to the Site Review Committee.*
3. *If seeking the benefits of the OARP, ETRA review would then allow for:*
  - a. *ETRA Professional (Planner, Attorney) review, if necessary*
  - b. *The Designation of the Developer*
  - c. *Creation of a Redevelopment Agreement, if necessary*
4. *ETRA's review and recommendation of a project will then be formally sent to the Planning Board by way of letter. ETRA's review will include:*
  - a. *Agreed-upon key site plan considerations and negotiated public improvements*
  - b. *Any deviations from standards necessary for the project that may or may not be supported.*
5. *Any issues that the Site Review Committee or Planning Board finds in need of addressing may require an additional meeting with ETRA to resolve any such issues. In the event of the dissolution of the Site Review Committee, ETRA will deal directly with the Planning Board.*
6. *Project seeks Planning Board approval.*

with the process set forth in the Local Redevelopment and Housing law, N.J.S.A. 40A:12A-1 et seq., and only upon finding that such deviation would be consistent with, and in furtherance of, the goals and objectives of this Plan. The Zoning Board of Adjustment retains no jurisdiction within the Plan's area.

### **Amending the Plan**

The Township reserves the right to require an applicant requesting deviation and Plan amendment to reimburse the Township for such costs of its professional engineers, planners, attorneys and staff time in furtherance of such deviation, for which an escrow fund be established. The escrow payment shall be \$10,000. The Township further reserves the right to require an applicant requesting an amendment to the Plan to prepare a study of the impact of such amendment, which study must be prepared by a professional planner in the State of New Jersey.

### **Redeveloper Designation**

No property in the Olden Avenue Redevelopment Area may be redeveloped until the proposed developer has been designated as the redeveloper and a redevelopment agreement has been executed if applicable; provided, however, that this requirement shall not apply to the following:

- Change in occupancy and/or use that does not require Planning Board approval; or
- Demolition and fit-out of interior improvements only.

In these instances, no redeveloper designation or redevelopment agreement shall be necessary; provided, however, that the applicant shall present a concept plan to ETRA's Executive Director for technical review and approval prior to submission of any application, if necessary, to the Planning Board or to the Township, if necessary, for building permits.

The Township intends to allow all property owners the opportunity to redevelop their property except as may otherwise be required in any of the Standards for

Development contained in any of the Sub-areas. ETRA has application forms detailing the application process, which includes meeting with appropriate Township officials, planning staff and the Site Plan Review Committee to review proposed plans, followed by a presentation to the ETRA for designation.

Under certain conditions ETRA may institute a process of selecting or approving a redeveloper to undertake a redevelopment project through a Redevelopment Agreement will require the following steps:

1. The Redevelopment Agency *may* prepare a Request For Qualifications (RFQ) to include, at a minimum, the following information:
  - a) Description of the redevelopment entity, including type of company or partnership, disclosure of ownership interest, list of references with name, address and phone information, list of any general or limited partners, financial profile of the redeveloper entity, and where applicable, a list of comparable projects successfully completed.
  - b) Description of proposed use for the redevelopment project, including analysis of the site and overall approach to site development regulatory process, use of contractors and subcontractors, etc.
  - c) Anticipated construction schedule, including estimated pre-construction time period to secure permits and approvals once granted final site plan approval by the Township.
2. The Agency will designate a redeveloper entity as the Conditional Redeveloper for a project subject to the successful negotiation and execution of a redevelopment agreement with the Ewing Redevelopment Agency within 12 months of conditional designation. The Agency may grant an extension to the negotiation period of six months or terminate the conditional redeveloper designation.
3. The Redevelopment Agency may, at any time, entertain an unsolicited proposal from a prospective redeveloper or property owner for redevelopment of a redevelopment project. The Agency will have the option

of conferring conditional redeveloper designation to such a redeveloper or putting out an RFQ if the property is publicly owned to solicit interest in the project from other potential redevelopers, subject in either case to the completion of Step 1 above prior to the execution of a redevelopment agreement. Existing property and business owners will be involved in this process as provided in this Plan. Preference on the selection of a redeveloper will be given to an entity that represents all of the property and business owners within a suggested redevelopment parcel and otherwise meets the requirements of the Plan for the selection of a redeveloper. Finally, no redeveloper designation will be made pursuant to an unsolicited proposal received by the Agency until the affected owners of properties within the redevelopment parcel are given the opportunity to present their own proposal within a reasonable time period as established by the Agency.

### **Redevelopment Agreements**

In order to effectuate this Redevelopment Plan, the Local Redevelopment & Housing Law (N.J.S.A. 40A:12A,8-9, permits ETRA the ability to enter into redevelopment agreements. Redevelopment Agreements allow ETRA and a prospective redeveloper the ability to provide each other a degree of expectation during the development process. Whether it be timing of a public improvement or monetary donations in-lieu of construction, the Redevelopment Agreement is a useful tool for all parties involved, public and private. A Redevelopment Agreement is only necessary when seeking the financial benefits that a redevelopment area designation provides.

Although agreements are subject to negotiation, basics of an agreement should include the following considerations;

- All parties to the agreement shall be named and their capacities to enter into the agreement clearly stated. In the case of developer/owners, their equitable or legal interests in the property must be stated.
- Relationship of the Parties. The relationship between

the parties to the agreement shall be stated clearly. Typically, the statement will specify that the relationship is contractual and that the owner/developer is an independent contractor, and not an agent of the local government.

- Property. The property to be subject to the agreement shall be clearly and thoroughly identified. An attachment, preferably with a map, specifically describing the property shall be provided and incorporated into the agreement by reference. Specifically, the agreement shall provide that the property is located in the Township of Ewing, more particularly describing which real property is the subject matter of this Agreement, and that said property consists of meets and bounds, acreage, block and lot and other defining features of the property. All agreements shall contain a covenant running with the land.
- Intent of the Parties. The intent of the parties to be bound by the terms of the agreement should be clearly stated. The agreement shall specifically include a statement that the property owner represents that it has an equitable or a legal interest in the real property and that all other persons holding legal or equitable interests in the real property are to be bound by the agreement. The development agreement will provide for the rights and obligations of the property owner under the agreement and shall run with the land.
- Recitation of Benefits and Burdens. The agreement shall recite the benefits each party expects to gain from entering into the agreement, as well as the burdens each party agrees to bear. Because the agreement will be treated as a contract, the consideration each party is to receive from the other should be stated clearly in order to ensure enforceability. The benefits to the local government and community must be expressed in terms that exhibit the agreement as consistent with the Plan.
- Approval and Permit Requirements. The agreement shall specify all discretionary approvals and permits that will have to be obtained before the development can proceed beyond its various stages. All conditions precedent to the obtaining of the permits and approvals should be

- listed.
- Dedications and Reservations. The agreement should provide, where appropriate, a statement of any land or improvements to be dedicated to the Township or land reservations made by the developer for public purposes, and the specific time period for such dedications and reservations as they relate to the date of entering into the agreement.
- Utility Connections. All water and sewer service, either to be provided by the developer or by the local government, shall be described in detail, together with schedules of construction completion, cost allocation (between or among developers and government and later developers), hookup or connection schedules, and parameters for permitting, including fees for utility provision, service and/or relocation.
- Duration of the Agreement. The agreement shall state a termination date. It should also specify project commencement and completion dates, either for the project on the whole, or for its various phases. The agreement should specify that the termination date can be extended by mutual agreement, and that commencement and completion dates may also be extended.
- Transference. The agreement is not transferable without written consent of the redevelopment agency.
- Periodic Review. The agreement should provide for periodic reviews of the project in order to determine compliance with the terms of the agreement. Unless otherwise negotiated, Ewing Township Construction Office shall be responsible for performing such reviews.
- Remedies and Enforcement. Remedies for breach on the part of either party shall be provided, and the agreement shall provide for enforcement of its provisions.
- Relocation Assistance. If a developer acquires property, the developer may be required to offer relocation assistance.

**PILOT (Payment in lieu of tax) Agreements**

The Local Redevelopment and Housing Law permits ETRA to enter into payment in lieu of tax agreement (PILOT)

to effectuate the implementation of this Plan. Eligible projects within the Redevelopment Area may request a PILOT through a Redevelopment Agreement. A PILOT is not a tax exemption, it is an incentive to improve a property that effectively “freezes” the property’s pre-development tax payment while the improvements are deferred over a time frame until eventually, and incrementally, 100% of the property’s improvements are assessed into the Township’s budget. The Township will never receive less on the property than it did prior to the redevelopment project. The difference is that the PILOT does not have to be apportioned to the county, school district, fire districts, etc., the way it would revenue from property taxes although the Township may wish to provide the schools with funding.

There are two types of PILOT(s), the short-term (5-year) and long-term (up to 30 year). The difference between short-term and long-term abatements is as follows;

**SHORT-TERM ABATEMENT:** This program is intended for development projects in which, after project completion, the increase in assessed value to the improved property does not exceed 30%. The added assessment is phased in over a five-year period.

The short-term abatement is on the improvement portion of the tax assessment. The assessed value of the land on which the improvement is made remains at 100%. The abatement agreement must be approved before the project starts. The type of projects are typically used for rehabilitation projects.

**LONG-TERM ABATEMENT:** For certain redevelopment projects, ETRA may negotiate and submit for approval to the Township Council a long-term payment-in-lieu-of-taxes (pilot) agreement with qualifying developers. Pilot agreements can be up to 30 years. The amount of the pilot is based on a percentage of project income, or a percent of total project costs. Agreements are subject to approval by the Township Council before the project begins. The short-term, five-year abatement cannot be coupled with the long-term abatement program.

In both the long-term and short-term scenarios, PILOTs do not affect a municipality's School-aid formula. Since school-aid is based on a commercial ratable basis, PILOTs effectively take the ratable off-line and out of the calculation. Combined with a fiscal analysis that should be determined during the redevelopment agreement process, PILOTs, when combined with a portion of this revenue stream to the school system will not adversely affect the school budgeting process.

### Property Acquisition

NJ State law requires that the principles of property acquisition be discussed within the Plan. The Local Redevelopment and Housing Law authorizes the use of eminent domain for public acquisition of property for the purpose of redevelopment when such properties are specifically identified in the a redevelopment plan. The Township has made it their policy to restrict the use of eminent domain within the redevelopment area, and this Plan does not propose any property for eminent domain. Since this Plan's original adoption in 1999, no property has been condemned to consolidate property in order to further the intent of the Plan. The intent of this approach was to allow the private marketplace to negotiate their own deals and consolidate property without condemnation proceedings. While this policy is favorable to private property owners within the area, it has been known on several occasions to hinder implementation of the Plan. In some cases, this hindrance delayed projects through unreasonable demands during the negotiation process. In the best cases, these demands lead to project amendments; in the worst, broken deals and lost ratables.

While condemnation is rarely used in redevelopment projects, regardless of whether property is identified or not, it is a useful tool in keeping negotiations fair and reasonable. State law provides that in a redevelopment area, should the power of eminent domain be exercised, the amount to be paid to the owner of property acquired is the greater of fair market value at the time of the taking or at the time of the designation of the redevelopment

area, in this case, the date of the Council Resolution in 1997. In addition, Section 7 of the Local Redevelopment and Housing Law ("LRHL") requires that a redevelopment plan identify "any property within the redevelopment area which is proposed to be acquired in accordance with the redevelopment plan". In fact, more than fair market value is usually paid to a property owner because the redeveloper is gaining increased value through the additional property gained.

The Township continues to carefully consider eminent domain and as such, will require a formal Plan amendment process to accomplish any publicly initiated taking. Using this approach does take more time but adds transparency for public review.

The Township may seek to negotiate contracts of sale of privately held parcels of land within the redevelopment area to enable consolidation of redevelopment sites. Notwithstanding, the Township's ability to bring property owners to the table to discuss future development, any consolidation of property through eminent domain will require a formal Township action through the Plan amendment process outlined in the LRHL N.J.S.A. 40A:12A-7.

If a situation arises that further compels the Agency to request a Plan amendment in order to pursue any potential eminent domain procedures; to protect the public interest, there are clear, detailed steps that must be followed before the Township undertakes the formal Plan amendment process. They are at a minimum:

- The Township and its Redevelopment Agency will ensure that the present owners of property be given every opportunity to participate in the redevelopment program through the reinvestment and redevelopment of their properties in accordance with the land uses, building and design requirements of this Plan.
- Every conceivable effort will be made to reach a negotiated agreement with an individual property owner on a sales price and three reasonable offers for purchase would have to be rejected by that property owner before eminent domain (through a Township Council approved

Plan amendment) would be considered by the Redevelopment Agency.

- In the extreme circumstance that the Agency determines to use eminent domain, the property owner will be compensated based on “fair market value”, which is the higher of the value at the time of taking or at the time of the redevelopment area designation.
- The Township and the Redevelopment Agency will assure that any and all condemnation proceedings comply with state and federal law.

Should a Plan amendment become necessary, the redevelopment agency, shall reserve the right to recommend to Township Council that eminent domain proceedings be commenced with regard to any privately-held parcels within the subject portion of the redevelopment area, on behalf of the selected redeveloper, pending the deposit of cash or a letter of credit with the Agency by the redeveloper equal to the fair market value of such parcel as determined by appraisal. Such money shall be used to purchase the property. The developer shall also be responsible for:

- The difference between that appraisal price (previously deposited with the Agency by the redeveloper) and a court-determined final condemnation award;
- The relocation costs of any business or resident displaced as a result of the condemnation, to the extent required by state or federal law, whichever is applicable (see Relocation Plan below).
- All costs that may arise from the requirements of the Industrial Site Recovery Act (ISRA) of the State of New Jersey, N.J.S.A. 13:1K-6 et. seq.
- The costs of the Township’s professionals, planners, engineers, attorneys in furtherance of such condemnation, for which an escrow fund shall be established.

#### **Specific Property Identified for Acquisition**

No property has been identified for eminent domain.



## RELATIONSHIP TO OTHER PLANS

### **Ewing Township Master Plan**

The master plans which are relevant to this Redevelopment Plan are those of the City of Trenton, (which borders the Redevelopment Area opposite Princeton Avenue, Calhoun Street, Ingham Avenue and Parkway Avenue); and the Township of Lawrence, (which borders the Redevelopment Area opposite Princeton Avenue, Spruce Street and portions of the Shabakunk Creek). The Plan must also be compared to the Mercer County Growth Management Plan and the State Development and Redevelopment Plan.

In 2006, Ewing Township amended and updated its Master Plan including its Conservation Element with Environmental Resource Inventory. These documents are living documents that will continue to mutually evolve. This Plan is substantially consistent with the intent of the Ewing Township Master Plan and actually refines the Master Plan to create livable places where the intent of the plan can be effectively implemented. In-fact this Redevelopment Plan furthers virtually all the Master Plan's goals for Vision, Land Use, and Circulation. Other goals pertaining to Housing, Open Space, and Utilities are also furthered.

After a review of the Land Use and Circulation Elements, this Plan implements many of the recommendations contained in the Future Land Use Plan. Furthermore, the implementation of this Plan as a stand-alone document also works toward these ends as a unified vision is needed to realize the true potential of the Olden Avenue Area.

### **City of Trenton Master Plan**

The portion of the City of Trenton bordering the Redevelopment Area is mixed use and residential neighborhoods, consisting predominantly of row-homes, with some garden style apartment buildings. The row-home neighborhoods are of the same approximate age and character as those in Subarea 5 and the Hammitt Street neighborhood. The rehabilitation of the neighborhoods in Ewing Township can be expected to have only positive effects on the Trenton side. The Trenton Master Plan includes policies to address compatible in-fill development and mixed use zoning which is consistent with the objectives of this Redevelopment Plan for the areas along Princeton Avenue, Calhoun Street and Ingham Avenue.

The industrial land uses within Subarea 6 have historically been in close proximity to the row-house neighborhoods on both sides of Princeton Avenue and Calhoun Street. The eventual redevelopment of Subarea 6 into more productive and active industrial and commercial uses, and the improvement of access and circulation through the area, may increase the level of truck traffic on Princeton Avenue and Calhoun Street, although the bulk of the truck access is expected to use North Olden Avenue as the most direct link to Route 1. It is important to note, however, that these areas are already zoned for industrial uses and that the City's response the Mercer County Cross Acceptance Survey indicated that no significant conflicts with the plans of adjacent municipalities were found.

To the extent that successful redevelopment leads to the incremental improvement of the flooding conditions along the Shabakunk and new detention or other stormwater management facilities are designed to reduce off-site flooding down stream, the implementation of this Redevelopment Plan will have a positive impact on historically flood prone areas in Trenton.

### **Township of Lawrence Master Plan**

The Lawrence Township Master Plan was last updated in 1995 and a new Land Use Ordinance (LUO) was adopted in December of 1997. The Master Plan and LUO both have objectives that urge redevelopment in the southwest section of the Township in the area of the Brunswick Circle (junction of Alternate Route 1, Route 206 and Princeton Pike. This section of Lawrence is very close to the Ewing and Trenton borders and Lawrence may eventually lay out a redevelopment area adjacent to this Redevelopment Area. Most of the zoning in the adjoining sections of Lawrence is either Highway Commercial, or Neighborhood Commercial (NC-1 or NC-2).

### **City of Trenton Urban Enterprise Zone**

A portion of Trenton's Urban Enterprise Zone extends to its borders with Ewing and Lawrence. Depending on the term of Trenton's UEZ designation and the time period at which the City can petition the New Jersey UEZ

Authority for an expansion of the UEZ boundaries, there may be an opportunity for all three municipalities to benefit from a regional UEZ.

### **Mercer County Growth Management Plan**

The Mercer County Growth Management Plan (1986) includes the Olden Avenue Redevelopment Area within the “Urban Growth Area” in Mercer County. The Plan describes an Urban Growth Area as areas which; provide major concentrations of mixed use activities that serve county-wide needs, including high density residential development, single family housing on small lots, County and State government facilities, professional offices and a wide range of cultural facilities are served by public sewer and water, are fully accessible to major transportation facilities, and are the location of major commercial and industrial employment centers.

The Olden Avenue Redevelopment Plan is fully consistent with the type and intensity of development described in an Urban Growth Area. In addition, the Redevelopment Plan is compatible with the following specific policies:

- \* Fifty percent of the demand for new land development in Urban Growth Areas is to be accommodated in redevelopment of existing developed land areas.
- \* In filling or redevelopment of urban areas will be encouraged through legal requirements and through financial incentives at intensities appropriate to the character of the area and to the availability of facilities and infrastructure.
- \* ...nonresidential development is to be of four to six stories in height, and is permitted to cover up to 80% of the land area of the site.

The floodplain of the Shabakunk Creek, including the portion of the stream corridor within the Redevelopment Area, has been identified on the Conservation Areas Map in the 1986 Growth Management Plan. The Management Policies for Conservation Areas includes the following provision:

Incentives are to be established to encourage infill development in existing developed areas that is compatible with the natural and cultural functions of Conservation Areas and that will lessen demands for land development within Conservation Areas.

This Redevelopment Plan allows for an increase in open space requirements to reduce flooding and assumes a 50% open space set-aside for certain redevelopment parcels adjacent to the Shabakunk or abutting wetlands. These provisions are consistent with the relevant Management Policy for Conservation Areas stated above.

The Mercer County Planning Board has prepared a Preliminary Master Plan Update that remains a draft document at the time this Redevelopment Plan was prepared. Information regarding this process, including the draft plan and background documents, is available online at [http://nj.gov/counties/mercer/departments/planning/master\\_plan.html](http://nj.gov/counties/mercer/departments/planning/master_plan.html). It is expected that this Redevelopment Plan will be used by the County in finalizing updates to its 1986 Master Plan.

Based on this review, this Redevelopment Plan is consistent with the policies of the Mercer County Growth Management Plan.

### **State Development and Redevelopment Plan**

The Township has participated in Cross Acceptance and submitted a dissenting report to the State Planning Commission for the purpose of advancing consideration of West Trenton as a designated Center during the second round of Cross Acceptance. The County’s Cross Acceptance Report referred to Ewing as predominantly developed, with future growth likely to come from redevelopment of existing areas rather than development of undeveloped areas.

This Redevelopment Plan is the first undertaken exclusively by the Township and follows the redevelopment strategy outlined in the County Cross Acceptance Report.

Notwithstanding Cross Acceptance, the State Development and Redevelopment Plan (State Plan) locates Olden Avenue in a Planning Area 1 (Metropolitan). The *State Plan Map (Map 8)* indicates the State Plan’s designation and as such designates the area as a Growth Area.

### **Mercer Crossings Report**

The Township has participated in the Urban Land Institute’s Mercer Crossings Report and is an active member of the tri-municipal County steered committee. Since much of the Redevelopment Area is impacted by the recommendations of the Report, it is important to recognize. The Mercer Crossing Report details many initiatives that will be implemented and supported by this Redevelopment Plan.

The ULI panel of experts from across the country spent a week assessing an area that included the eastern portion of the Olden Avenue Redevelopment Area, portions of Lawrence Township and the City of Trenton leading up to Route 1 and made a number of recommendations worthy of consideration. The panel:

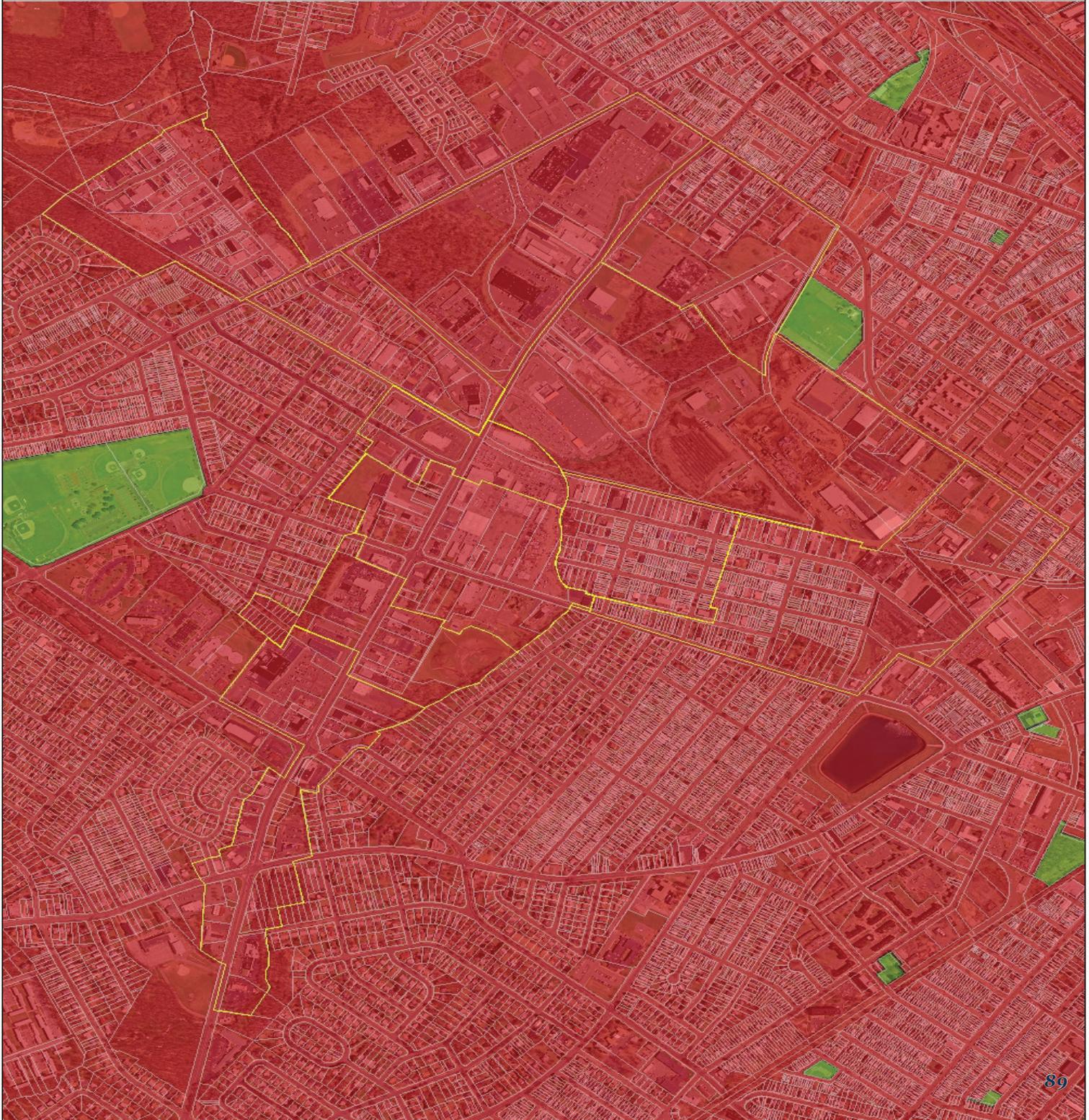
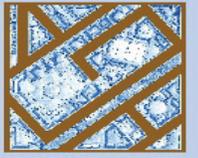
- Identified a portion of Capital Plaza and neighboring areas as “opportunity sites” within Ewing Township.
- Proposed changing the design for Olden Avenue including the creation of a “green median with limited left turn lane to calm and control traffic access/conflict points” and “a wider, tree protected sidewalk to promote pedestrian traffic.” While a “boulevard” approach may be appropriate, careful consideration of the businesses along the Avenue will be considered and options will be explored with the County Transportation Department.
- Advocated the re-establishment of an Olden Avenue bus route. The former bus “Cross-town” route was discontinued in the late 1980’s with the closure of the GM plant and Naval Warfare station.
- Sought to “create a road grid to promote access and ease bottlenecks” between the three municipalities.
- Encouraged the creation and maintenance of a “green infrastructure” that would work to improve aesthetics, create atmosphere and protect the environment.
- Emphasized the need for intergovernmental cooperation and financial resources.
- Recognized that design criteria in both the OARP and the Brunswick Turnpike South Redevelopment Plan (Lawrence Township) need to “provide consistent and solid urban design criteria” (ULI August 2004, p. 29).

ETRA continues to coordinate with Mercer County, Lawrence Township, the City of Trenton and other Ewing Township agencies through County initiatives including land use and transportation studies addressing Spruce Street, Princeton Avenue and the Farmers Market Area. See [http://nj.gov/counties/mercer/departments/planning/mercer\\_crossings.html](http://nj.gov/counties/mercer/departments/planning/mercer_crossings.html) for additional information and updates.

# State Planning Areas

## Legend

- PA 1- METROPOLITAN
- PA 2-SUBURBAN
- PARKS
- WATER
- OARP Boundary



*Eventually, public and private investment will work together with designated open spaces, such as the Ewing Golf Range depicted here, to create a solid environment to do live and do business.*



*APPENDIX*

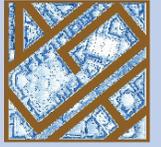


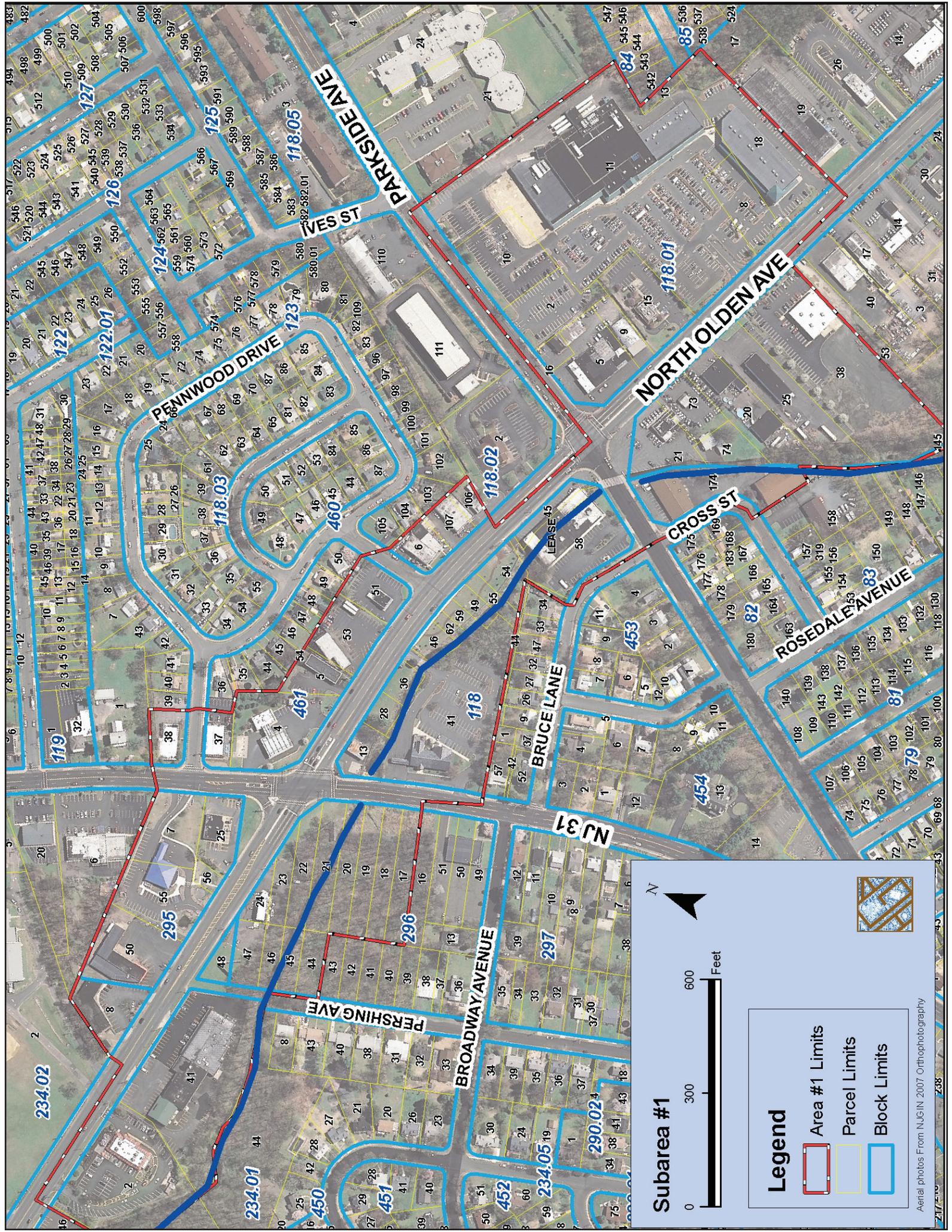
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*.....Parcel Map.....*

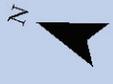
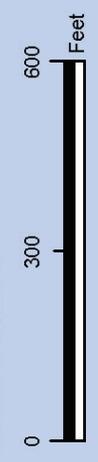


Olden Avenue Redevelopment Plan  
**Redevelopment  
Area**





**Subarea #1**



**Legend**

-  Area #1 Limits
-  Parcel Limits
-  Block Limits





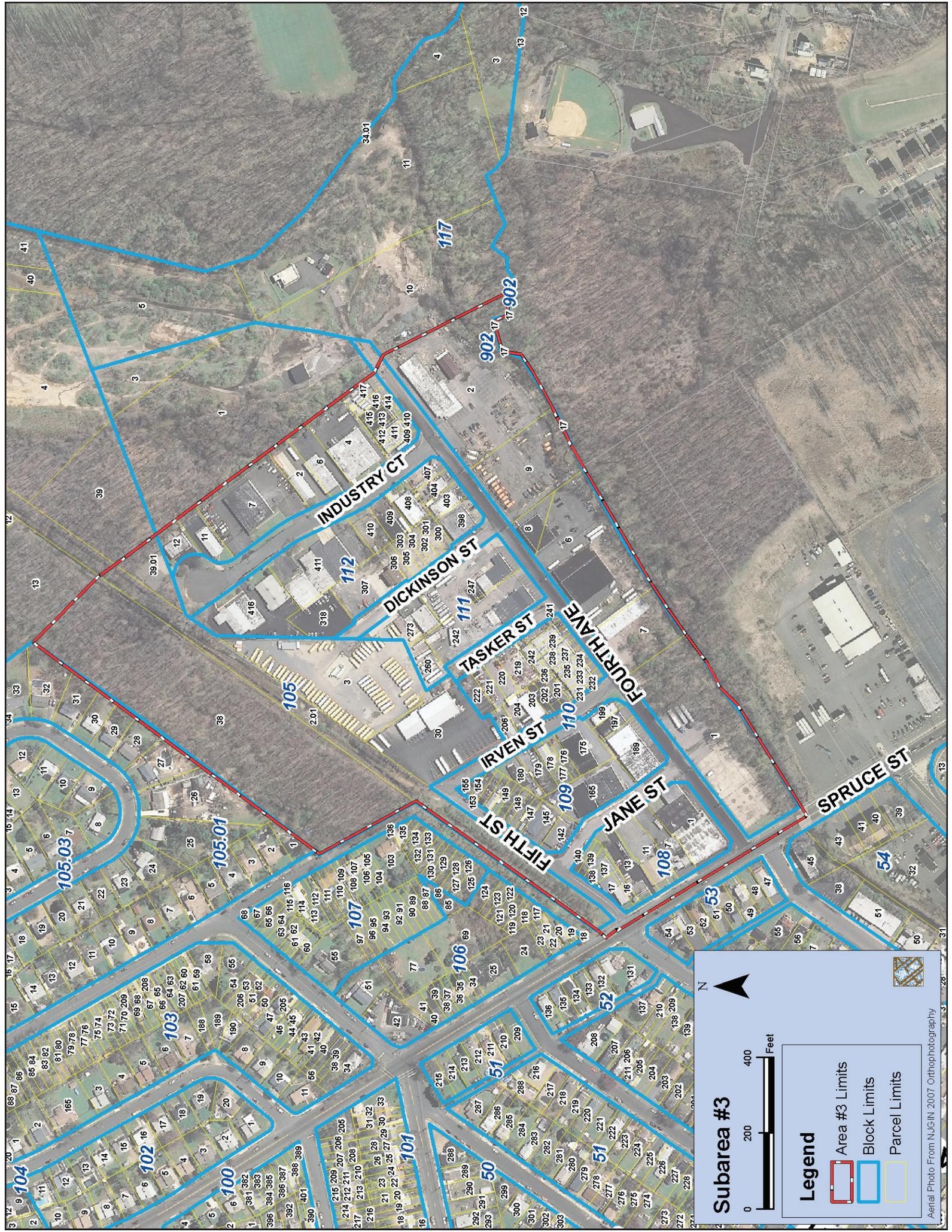
**Subarea #2**

0 300 600 Feet

**Legend**

- Area #2 Limits
- Parcel Limits
- Block Limits

Aerial Photos From NUGIN 2007 Orthophotography



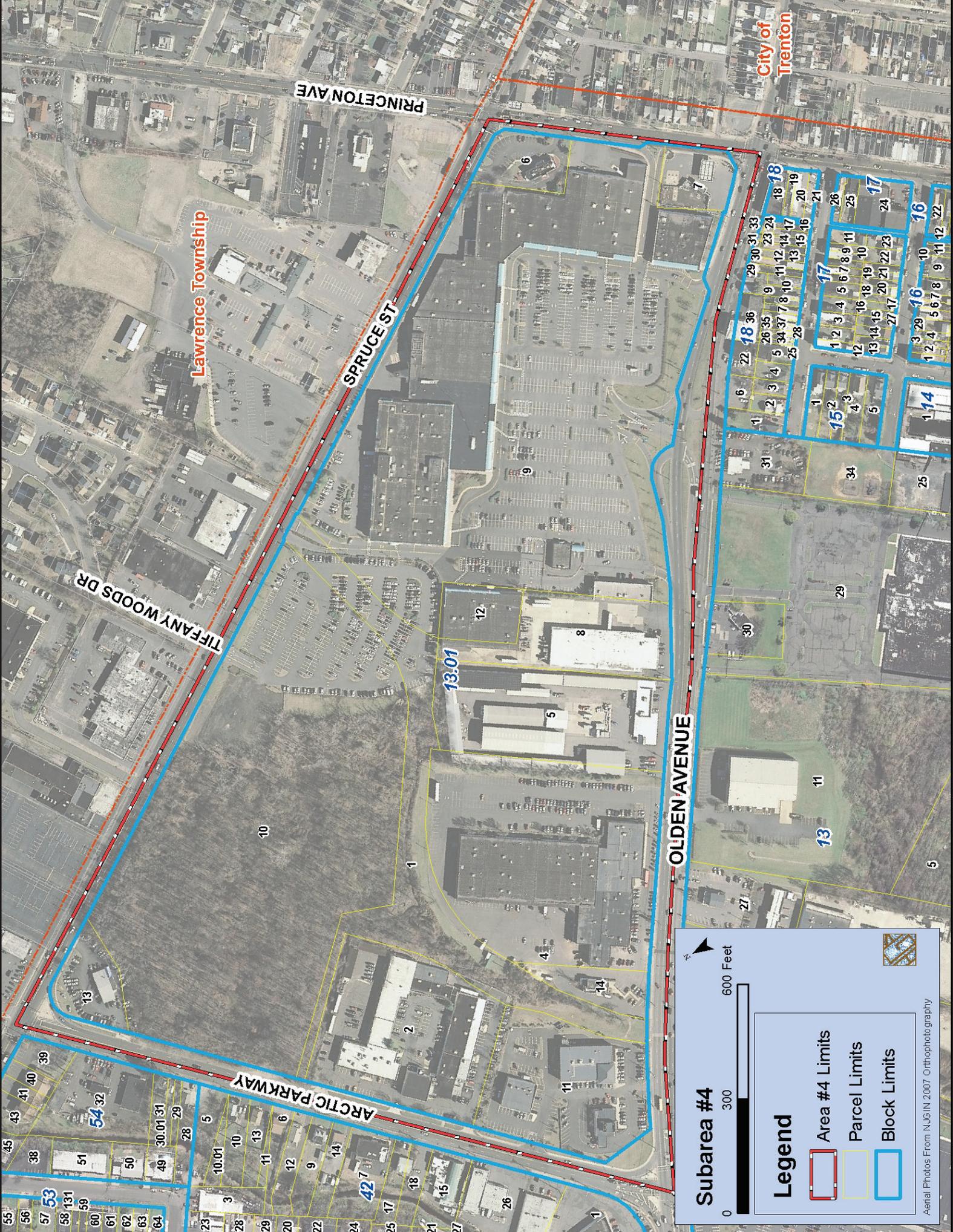
**Subarea #3**

0 200 400 Feet

**Legend**

- Area #3 Limits
- Block Limits
- Parcel Limits

Aerial Photo From N.J.G.I.N 2007 Orthophotography



City of Trenton

Lawrence Township

PRINCETON AVE

SPRUCE ST

TIFFANY WOODS DR

OLDEN AVENUE

ARCTIC PARKWAY

Subarea #4



600 Feet

300

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Legend

Area #4 Limits

Parcel Limits

Block Limits

Aerial Photos From N.J.G.I.N 2007 Orthophotography

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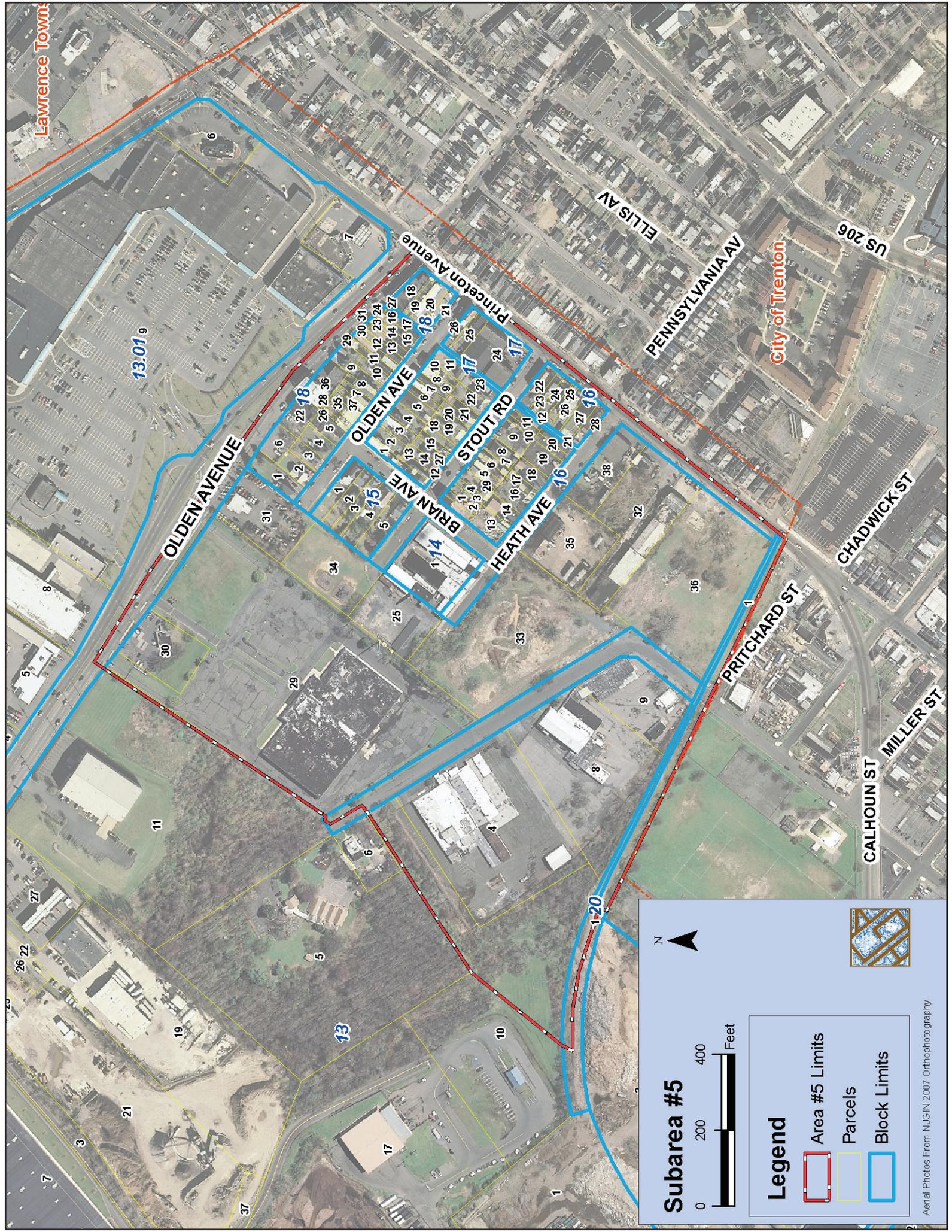
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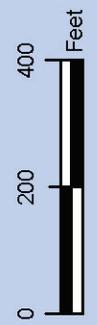
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Lawrence Township

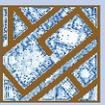
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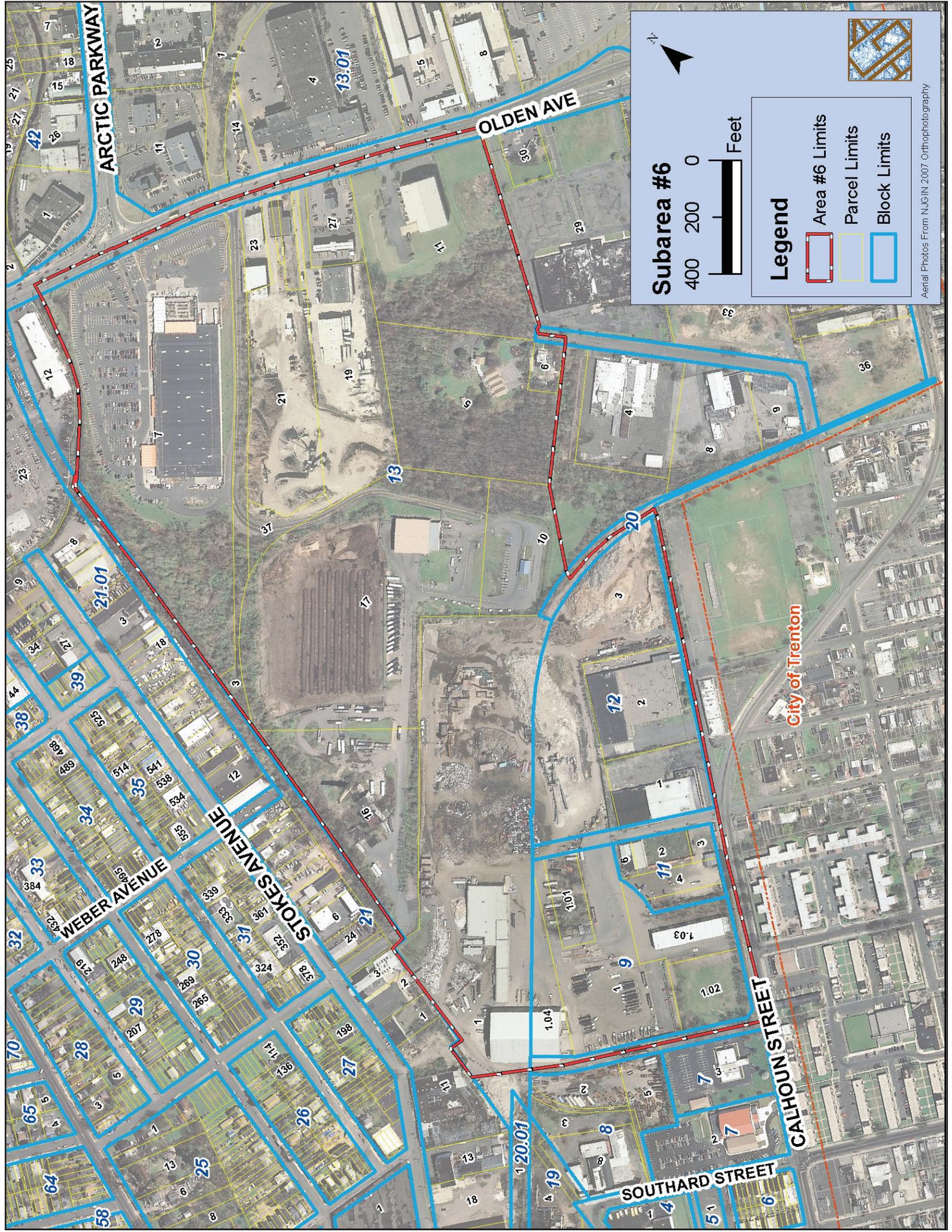
### Subarea #5



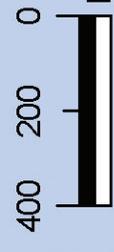
#### Legend

- Area #5 Limits
- Parcels
- Block Limits





**Subarea #6**

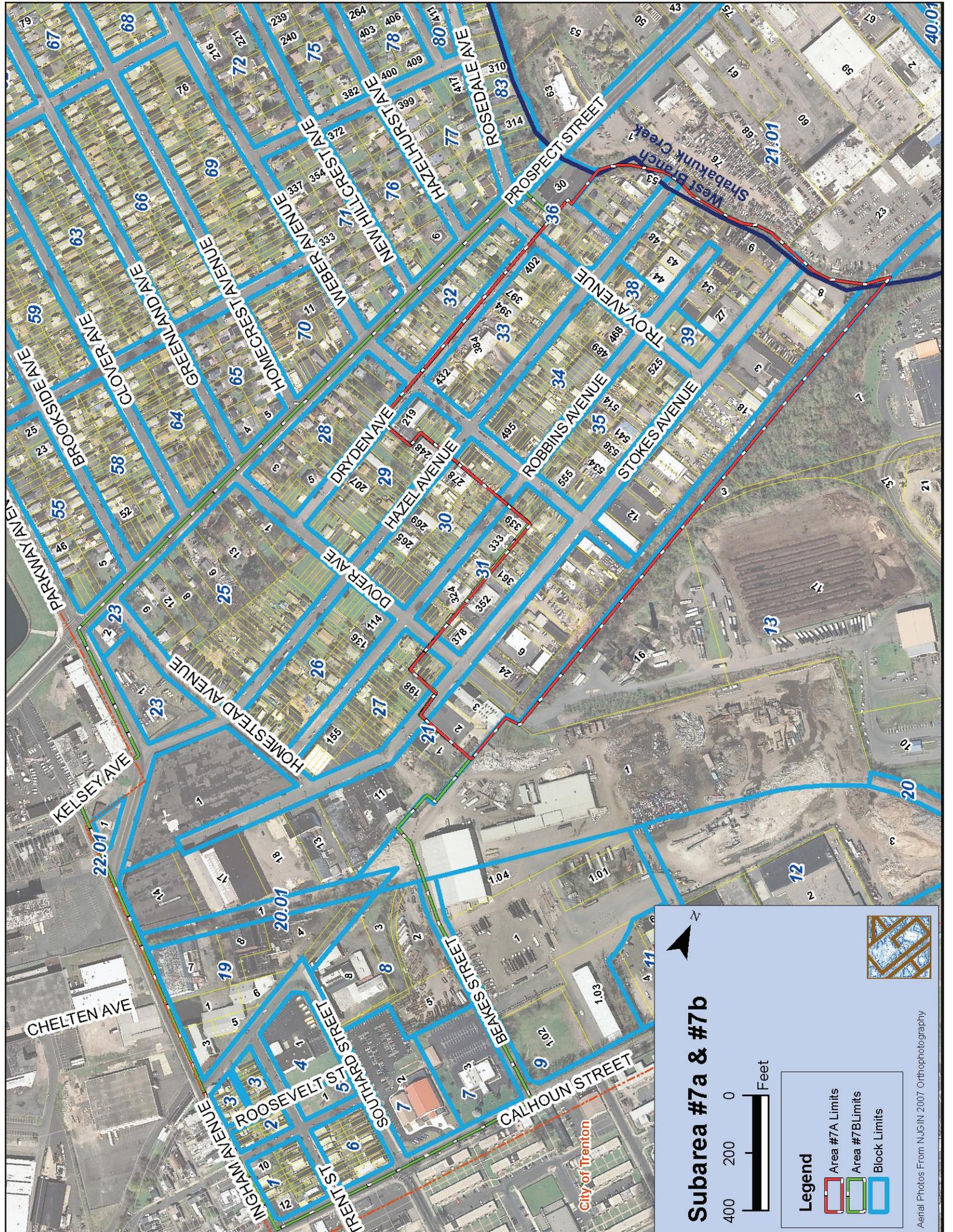


**Legend**

- Area #6 Limits
- Parcel Limits
- Block Limits



City of Trenton



**Subarea #7a & #7b**

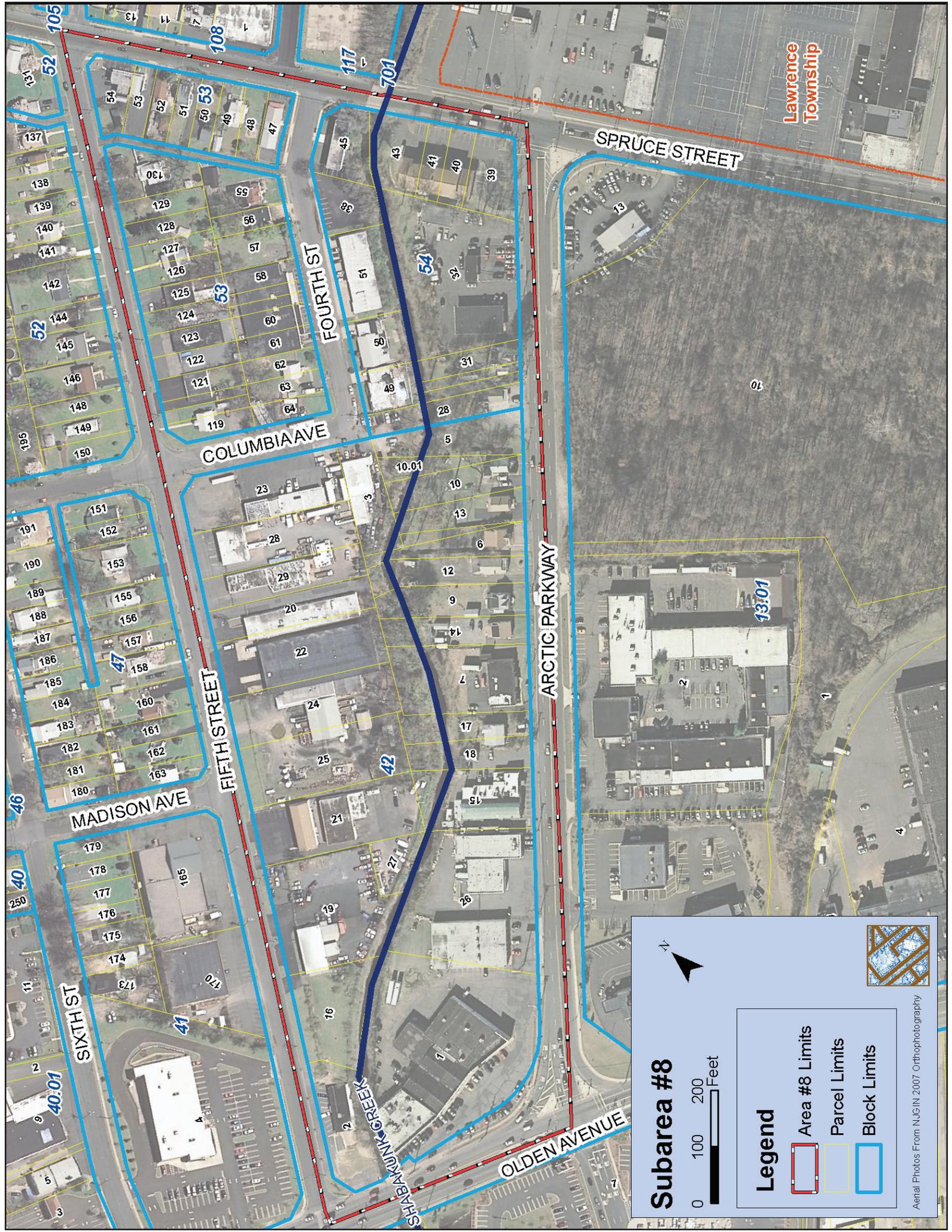
400 200 0 Feet

**Legend**

- Area #7A Limits
- Area #7B Limits
- Block Limits

City of Trenton

Aerial Photos From NUGIN 2007 Orthophotography



Lawrence  
Township

SPRUCE STREET

FOURTH ST

COLUMBIA AVE

FIFTH STREET

MADISON AVE

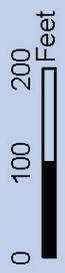
SIXTH ST

ARCTIC PARKWAY

OLDEN AVENUE

SHAWANK CREEK

# Subarea #8



## Legend

-  Area #8 Limits
-  Parcel Limits
-  Block Limits





# APPENDIX **B**

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*..... List of Properties in the Redevelopment Area. ....*

Lot #	Address	Taxable Property Tax Code	Area (sq. ft.)	County	State	APN
6	11 TRENT ST	08638	2	EWING, NJ		11 NEW TRENT ST
6	20 TRENT ST	08638	2	EWING, NJ		23 NEW TRENT STREET
6	21 TRENT ST	08618	2	EWING, NJ		15 TRENT STREET
6	22 TRENT ST	07063	2	PLAINFIELD, NEW JERSEY		1025 WEST 8TH ST.
6	23 1007 SOUTHWARD ST	08638	2	EWING, NJ		1007 SOUTHWARD STREET
6	24 1017 CALHOUN ST	08638	4A	EWING, NJ		1017 CALHOUN ST.
7	2 1509 W STATE ST	08618	2	TRENTON NJ		1509 W STATE ST
7	25 1217 CALHOUN ST	08650	15D	TRENTON, NEW JERSEY		P O BOX 9672
7	3 1225 CALHOUN ST	08618	4A	EWING, NJ		34 MORNINGSIDE DRIVE
8	2 BEAKES ST	08638	1	TRENTON NJ		P O BOX 5626
8	3 1150 SOUTHWARD ST	08638	4A	EWING, NJ		1150 SOUTHWARD STREET
8	4 BEAKES ST	08638	1	TRENTON NJ		P O BOX 5626
8	5 BEAKES ST	08638	1	TRENTON NJ		P O BOX 5626
8	6 1150 SOUTHWARD ST	08638	4A	EWING, NJ		1150 SOUTHWARD STREET
9	1 BEAKES ST	08638	4B	TRENTON NJ		526 NEW YORK AV
9	1.01 KIRKBRIDE AVE	15317	1	MCMURRAY PA		4017 WASHINGTON RD PMB353
9	1.02 BEAKES ST	08638	1	TRENTON NJ		526 NEW YORK AVE
9	1.03 CALHOUN ST	08638	4A	TRENTON, NEW JERSEY		1519 CALHOUN STREET
9	1.04 BEAKES ST	08638	1	TRENTON NJ		P O BOX 5626
11	2 KIRKBRIDE AVE	08638	1	EWING, NJ		1242 PRINCETON AVE
11	3 CALHOUN ST	08610	4A	HAMILTON, NJ		45 WELLER AVE
11	4 1511 CALHOUN ST	08610	1	HAMILTON, NJ		45 WELLER AVE
11	5 1511 CALHOUN ST	08610	1	HAMILTON, NJ		45 WELLER AVE
11	6 KIRKBRIDE AVE	08638	4A	EWING, NJ		1242 PRINCETON AVE
12	1 1605 CALHOUN ST	08638	4A	EWING, NJ		1605 CALHOUN STREET
12	2 1700 CALHOUN ST	08638	4B	TRENTON, NEW JERSEY		1700 CALHOUN STREET
12	3 CALHOUN ST	08610	1	HAMILTON, NJ		45 WELLER AVE
13	1 ALONG RAILROAD	08638	4A	TRENTON NJ		P O BOX 5626
13	3 TRENTON PRINCETON TRACTIO	08611	15C	TRENTON, NEW JERSEY		1589 LAMBERTON ROAD
13	4 PRINCESS DIANA LA	08638	4A	EWING, NJ		25 PRINCESS DIANA LANE
13	5 1 PRINCESS DIANA LA	08638	15B	EWING, NJ		1 PRINCESS DIANA LA
13	6 7 PRINCESS DIANA LA	08638	2	EWING, NJ		7 PRINCESS DIANA LANE
13	7 1621 N OLDEN AVE EXT	30339	4A	ATLANTA GA		2455 PACES FERRY RD #200
13	8 PRINCESS DIANA LA	08638	4B	EWING, NJ		25 PRINCESS DIANA LANE
13	9 PRINCESS DIANA LA	08638	4B	EWING, NJ		25 PRINCESS DIANA LANE
13	10 PRINCESS DIANA LA	08618	4B	EWING, NJ		25 PRINCESS DIANA LANE
13	11 1581 N OLDEN AVE EXT	60045	15C	LAKE FOREST ILL		2 JAKE GARZIO DR
13	12 1641 N OLDEN AVE EXT	94566	4A	PLEASANTON CA		100 GRANGER PKY
13	13 1512 ALADIN WAY	1885	4A	EWING, NJ		1152 PALADIN WAY
13	16 BEAKES ST	08628	15C	EWING, NJ		2 JAKE GARZIO DR
13	17 BEAKES ST	1885	15C	EWING, NJ		2 JAKE GARZIO DR
13	19 1603 N OLDEN AVE EXT	08638	4A	TRENTON NJ		P O BOX 55300
13	21 1607 N OLDEN AVE EXT	08638	4A	TRENTON NJ		PO BOX 55300
13	22 1603 N OLDEN AVE EXT	08638	4A	EWING, NJ		P O BOX 55300
13	23 1608 N OLDEN AVE EXT	08638	4A	TRENTON NJ		P O BOX 5187
13	24 1605 N OLDEN AVE EXT	08638	4A	EWING, NJ		1605 NORTH OLDEN AVE
13	26 1605 N OLDEN AVE EXT	08638	4A	EWING, NEW JERSEY		1605 NORTH OLDEN AVE
13	27 1597 N OLDEN AVE EXT	08638	4A	EWING, NEW JERSEY		1597 NO. OLDEN AVE.
13	29 1561 N OLDEN AVE EXT	08648	4A	LAWRENCEVILLE NJ		1333 BRUNSWICK PK #200
13	30 1571 N OLDEN AVE EXT	08109	1	PENNSAUKEN NJ		3011 ADMIRAL WILSON BLVD
13	31 1551 N OLDEN AVE EXT	08638	1	EWING, NJ		1555 NORTH OLDEN AVENUE
13	32 1321 PRINCETON AVE	22201	4A	ARLINGTON, VA		826 N. JACKSON ST
13	33 PRINCETON AVE	19067	1	MORRISVILLE, PA		583 ROSEMAR DR
13	34 STOUT AVE REAR	08618	15C	EWING, NJ		2 JAKE GARZIO DR
13	35 1441 HEATH AVE	08648	4A	LAWRENCEVILLE NJ		1910 PRINCETON AV
13	36 PRINCETON AVE	19010	1	BRYN MAWR PA		403 WYNTRIE LEA DR
13	37 1603 N OLDEN REAR	08650	15C	TRENTON, NEW JERSEY		P.O. BOX 8068
13	38 1349 PRINCETON AVE	08638	4A	EWING, NJ		1349 PRINCETON AVE
13.01	1 TRENTON PRINCETON TRACTIO	07060	1	N PLAINFIELD NJ		893 ROUTE 22 WEST
13.01	2 25 ARCTIC PARKWAY	08608	4A	TRENTON NEW JERSEY		P O BOX 2771
13.01	4 1600 N OLDEN AVE EXT	07666	4A	TEANECK NJ		153 FORT LEE RD
13.01	5 1580 N OLDEN AVE EXT	08638	4A	EWING, NJ		1580 N OLDEN AV
13.01	6 1565 PRINCETON AVE	19428	4A	CONSHOHOCKEN, PA		P O BOX 992
13.01	7 1513 PRINCETON AVE	07095	4A	WOODBRIDGE NJ		1 HESS PLAZA
13.01	8 1570 N OLDEN AVE EXT	08638	4A	EWING, NJ		1570 N OLDEN AVE

TAXABLE PROPERTY TAX CODES
1 VACANT LAND
2 RESIDENTIAL PROPERTY (1 - 4 FAMILY)
3A FARM (HOUSE)
3B FARM (QUALIFIED)
4A COMMERCIAL
4B INDUSTRIAL
4C APARTMENT
5A RAILROAD CLASS I
5B RAILROAD CLASS II
15A PUBLIC SCHOOL PROPERTY
15B OTHER SCHOOL PROPERTY
15C PUBLIC PROPERTY
15D CHURCH & CHARITABLE PROPERTY
15E CEMETERIES & GRAVEYARDS
15F OTHER EXEMPT

BLOCK	LOT	QUAL	Parcel Address	Owner Mailing Address	City, State	ZipCode	Property Class	Acreeage
1	1		22 TRENT ST	22 TRENT ST	EWING NJ	08638	2	.04
1	3		18 TRENT ST	18 NEW TRENT ST	EWING NJ	08638	2	.04
1	4		16 TRENT ST	16 NEW TRENT STREET	EWING, NEW JERSEY	08638	2	.04
1	5		14 TRENT ST	14 NEW TRENT ST	EWING, NEW JERSEY	08638	2	.04
1	6		10 TRENT ST	10 TRENT STREET	EWING, NJ	08618	2	.04
1	7		8 TRENT ST	10 TRENT STREET	TRENTON, N.J.	08638	1	.04
1	8		4 TRENT ST	4 NEW TRENT STREET	EWING, NEW JERSEY	08638	2	.08
1	9		2 TRENT ST	146 PHILLIP'S AVE	TRENTON NJ	08638	4A	.04
1	10		186 INGHAM AVE	123 HAZEL AVE	EWING, NJ	08638	4A	.17
1	11		210 INGHAM AVE	210 INGHAM AVE	EWING, NJ	08638	2	.04
1	12		901 CALHOUN ST	901 CALHOUN STREET	EWING, NJ	08638	4A	.00
1	13		20 TRENT ST	20 TRENT ST	EWING, NJ	08638	2	.04
1	14		12 TRENT ST	12 TRENT ST	EWING, NJ	08638	2	.04
1	15		216 INGHAM AVE	216 INGHAM AVE	EWING, NJ	08638	2	.04
1	16		214 INGHAM AVE	214 INGHAM AVE	EWING, NJ	08638	2	.04
1	17		212 INGHAM AVE	212 INGHAM AVENUE	EWING, NJ	08638	2	.04
2	1		207 TRENT ST	2 JAKE GARZIO DR	EWING, NJ	08628	15C	.03
2	2		201 TRENT ST	1413 WEST STATE STREET	TRENTON, NEW JERSEY	08618	2	.07
2	3		194 INGHAM AVE	194 W. INGHAM AVENUE	EWING, NJ	08638	2	.03
2	4		192 INGHAM AVE	192 INGHAM AV	EWING NJ	08638	2	.03
2	5		190 INGHAM AVE	190 INGHAM AV	EWING NJ	08638	2	.04
2	6		188 INGHAM AVE	20 LAUREL AVENUE	TRENTON, NJ	08618	2	.04
2	7		201 TRENT ST	1413 WEST STATE STREET	TRENTON, NEW JERSEY	08618	2	.07
2	8		205 TRENT ST	205 TRENT STREET	EWING, NJ	08638	2	.03
2	9		TRENT ST	2 JAKE GARZIO DR	EWING, NJ	08618	15C	.02
3	1		524 INGHAM AVE	524 INGHAM AVENUE	EWING, NJ	08638	2	.03
3	2		522 INGHAM AVE	218 SPRING ST	TRENTON NJ	08618	2	.03
3	3		520 INGHAM AVE	520 INGHAM AVE	EWING, NEW JERSEY	08638	2	.03
3	4		243 TRENT ST	243 TRENT ST	EWING, NJ	08638	2	.11
3	5		530 INGHAM AVE	190 HOLLOWBROOK RD	EWING, NJ	08638	4A	.00
3	6		243 TRENT ST	243 TRENT ST	EWING, NJ	08638	2	.11
3	7		225 TRENT ST	225 TRENT ST	EWING, NJ	08638	2	.05
3	8		227 TRENT ST	227 TRENT ST	EWING, NJ	08638	2	.02
3	9		229 TRENT ST	229 NEW TRENT STREET	EWING, NJ	08638	2	.02
3	10		231 TRENT ST	231 TRENT ST	EWING NJ	08638	2	.05
3	11		237 TRENT ST	5 THORNLEIGH PLACE	WILLINGBORO, NEW JERSEY	08046	2	.05
3	12		239 TRENT ST	524 INGHAM AV	EWING NJ	08638	2	.02
3	13		241 TRENT ST	243 TRENT STREET	EWING, NJ	08638	2	.02
3	14		243 TRENT ST	243 TRENT ST	EWING, NJ	08638	2	.11
3	15		528 INGHAM AVE	481 BRUNSWICK AVE	TRENTON, NEW JERSEY	08638	2	.03
3	16		526 INGHAM AVE	526 W INGHAM AV	EWING, NJ	08638	2	.03
3	17		518 INGHAM AVE	518 INGHAM AVENUE	EWING, NEW JERSEY	08638	2	.03
3	18		516 INGHAM AVE	516 INGHAM AVE	EWING, NEW JERSEY	08638	2	.03
4	1		1198 SOUTHARD ST	1198 SOUTHARD ST	EWING NJ	08638	4A	1.25
5	1		37 ROOSEVLET AVE	2 JAKE GARZIO DR	EWING, NJ	08628	15C	.00
6	1		1023 SOUTHARD ST	111 HUFF AVENUE	TRENTON, NEW JERSEY	08618	2	.04
6	2		1021 SOUTHARD ST	P O BOX 5439	TRENTON NJ	08638	2	.04
6	3		1019 SOUTHARD ST	111 HUFF AVENUE	TRENTON, NEW JERSEY	08618	2	.04
6	4		1017 SOUTHARD ST	111 HUFF AVE	TRENTON NJ	08618	2	.04
6	5		1015 SOUTHARD ST	1015 SOUTHARD STREET	EWING, NJ	08638	2	.04
6	6		1011 SOUTHARD ST	19 ARROWWOOD DRIVE	HAMILTON SQUARE NJ	08690	2	.04
6	7		1005 SOUTHARD ST	1005 SOUTHARD STREET	EWING, NJ	08618	2	.04
6	8		1003 SOUTHARD ST	289 HOMECREST AV	EWING, NJ	08638	2	.02
6	9		1001 SOUTHARD ST	186 BUTTWOOD DRIVE	EWING, NJ	08638	2	.00
6	10		21 TRENT ST	21 TRENT ST	EWING, NJ	08638	2	.04
6	11		17 TRENT ST	5373 W ALABAMA SUITE 600	HOUSTON TX	77056	2	.04
6	12		13 TRENT ST	13 NEW TRENT STREET	EWING, NEW JERSEY	08618	2	.04
6	13		5 TRENT ST	5 TRENT ST	EWING, NJ	08618	2	.04
6	14		3 TRENT ST	950 E PACES FERRY RD S190	ATLANTA GA	30326	2	.03
6	15		1009 SOUTHARD ST	1 SPRINGFIELD AV 1ST FLR	SUMMIT NJ	07901	2	.04
6	16		19 TRENT ST	19 TRENT STREET	EWING, NEW JERSEY	08638	2	.04
6	17		1013 SOUTHARD ST	1013 SOUTHARD STREET	EWING, NJ	08618	2	.04
6	18		9 TRENT ST	9 NEW TRENT ST	EWING, NJ	08638	2	.04

TAXABLE PROPERTY TAX CODES
1 VACANT LAND
2 RESIDENTIAL PROPERTY (1 -4 FAMILY)
3A FARM (HOUSE)
3B FARM (QUALIFIED)
4A COMMERCIAL
4B INDUSTRIAL
4C APARTMENT
5A RAILROAD CLASS I
5B RAILROAD CLASS II
15A PUBLIC SCHOOL PROPERTY
15B OTHER SCHOOL PROPERTY
15C PUBLIC PROPERTY
15D CHURCH & CHARITABLE PROPERTY
15E CEMETERIES & GRAVEYARDS
15F OTHER EXEMPT

To be verified by Ewing Township Tax Office

17	1465 PRINCETON AVE	1632 PRINCETON AVENUE	EWING, NJ	08638	2	.10
17	1469 PRINCETON AVE	106-02 ROCKAWAY BLVD	OZONE PARK, NEW YORK	11417	4A	.03
17	44 STOUT AVE	623 RAINBOW BLVD	LADY LAKE FL	08638	2	.10
18	1472 N OLDEN AVE	1472 N OLDEN AVE	EWING, NJ	08638	2	.20
18	1468 N OLDEN AVE	1468 N OLDEN AVE	EWING, NEW JERSEY	08638	2	.11
18	1462 N OLDEN AVE	29 KILMER DRIVE	EWING, NJ	08638	2	.09
18	1456 N OLDEN AVE	1545 NORTH OLDEN AVE	EWING, NJ	08638	4A	.28
18	1456 N OLDEN AVE	1545 NORTH OLDEN AVE	EWING, NJ	08638	4A	.28
18	1456 N OLDEN AVE	1545 NORTH OLDEN AVE	EWING, NJ	08638	4A	.28
18	1442 N OLDEN AVE	1442 N OLDEN AVE	EWING NJ	08638	2	.03
18	1440 N OLDEN AVE	1440 NO. OLDEN AVE.	EWING, NJ	08638	2	.04
18	1438 N OLDEN AVE	1438 N OLDEN AVE	EWING, NJ	08638	2	.07
18	1434 N OLDEN AVE	1434 N OLDEN AVE	EWING, NJ	08638	2	.06
18	1432 N OLDEN AVE	76 LAWRENCEVILLE-PENN. RD	LAWRENCEVILLE N.J.	08648	2	.06
18	1430 N OLDEN AVE	1430 NORTH OLDEN AVENUE	EWING, NJ	08638	2	.06
18	1428 N OLDEN AVE	34 WARDMAN AVE	EWING, NJ	08638	2	.06
18	1426 N OLDEN AVE	1426 N OLDEN AV	EWING NJ	08638	2	.06
18	1424 N OLDEN AVE	1424 N OLDEN AV	EWING NJ	08638	2	.06
18	1422 N OLDEN AVE	1422 NO. OLDEN AVENUE	EWING, NJ	08638	2	.06
18	1420 N OLDEN AVE	1509 PRINCETON AV	EWING NJ	08638	2	.06
18	1509 PRINCETON AVE	1509 PRINCETON AV	EWING NJ	08638	2	.06
18	1507 PRINCETON AVE	1509 PRINCETON AVE	EWING NJ	08638	4A	.11
18	1503 PRINCETON AVE	1509 PRINCETON AVE	EWING, NJ	08618	4A	.05
18	1503 PRINCETON AVE	1509 PRINCETON AV	EWING NJ	08638	4A	.17
18	1496 N OLDEN AVE	1509 PRINCETON AV	EWING NJ	08638	4A	.17
18	1424 N OLDEN AVE	1545 NORTH OLDEN AVE	EWING, NJ	08638	4A	.28
18	1424 N OLDEN AVE	1424 N OLDEN AV	EWING, NJ	08638	2	.06
18	1422 N OLDEN AVE	1422 NO. OLDEN AVENUE	EWING, NJ	08638	2	.06
18	1454 N OLDEN AVE	6 JOHNSON CORNER RD	E WINDSOR NJ	08520	2	.02
18	1462 N OLDEN AVE	2 GWENDOLYN DR	EWING NJ	08638	2	.02
18	1420 N OLDEN AVE	1509 PRINCETON AV	EWING NJ	08638	2	.06
18	1450 N OLDEN AVE	103 LAWRENCE-PENN. RD.	LAWRENCE, N.J.	08648	2	.02
18	1432 N OLDEN AVE	76 LAWRENCEVILLE-PENN. RD	LAWRENCEVILLE N.J.	08648	2	.06
18	1428 N OLDEN AVE	34 WARDMAN AVE	EWING, NJ	08638	2	.06
18	1426 N OLDEN AVE	1426 N OLDEN AV	EWING NJ	08638	2	.06
18	1426 N OLDEN AVE	1426 N OLDEN AV	EWING NJ	08638	2	.06
18	1448 N OLDEN AVE	103 LAWRENCE-PENN. RD.	LAWRENCE, N.J.	08648	2	.03
18	1446 N OLDEN AVE	5 MERVINE PL	HAMILTON TWP NJ	08609	4A	.20
18	1531 N OLDEN AVE EXT	1444 N OLDEN AVENUE	EWING, NJ	08638	2	.04
19	1444 N OLDEN AVE	550 INGHAM AVE	EWING, NJ	08638	1	.44
19	SOUTHARD ST	2 JAKE GARZIO DR	EWING NJ	08628	15C	.19
19	550 W INGHAM AVE	550 W INGHAM AVE	EWING, NJ	08638	4A	1.80
19	1200 SOUTHARD ST	2 JAKE GARZIO DR	EWING NJ	08628	15C	2.10
19	550 W INGHAM AVE	550 W INGHAM AVE	EWING, NJ	08638	4A	1.80
19	550 W INGHAM AVE	550 W INGHAM AVE	EWING, NJ	08638	4A	1.80
19	558 INGHAM AV	2 JAKE GARZIO DR	EWING NJ	08628	15C	1.01
19	1200 SOUTHARD ST	2 JAKE GARZIO DR	EWING NJ	08628	15C	2.10
20	CALHOUN ST	45 WELLS AVE	HAMILTON, NJ	08610	1	1:17
20.01	INGHAM AVE	82 STOKES AV	EWING NJ	08618	1	1.02
21	128 STOKES AVE	1201 BRUNS WICK AVE	TRENTON, NEW JERSEY	08638	4B	.46
21	140 STOKES AVE	140 STOKES AVE	EWING, NJ	08638	4B	1:15
21	140 STOKES AVE	140 STOKES AVE	EWING, NJ	08638	4B	1:15
21	140 STOKES AVE	140 STOKES AVE	EWING, NJ	08638	4B	1:15
21	204 STOKES AVE	204 STOKES AVE	FAIRLESS HILLS, PA.	19030	4A	.46
21	208 STOKES AVE	718 HOOD BLVD.	EWING, NJ	08638	4A	.46
21	220 STOKES AVE	995 BEAR TAVERN ROAD	EWING, NJ	08628	4A	.46
21	224 STOKES AVE	995 BEAR TAVERN ROAD	EWING, NJ	08628	4A	.46
21	240 STOKES AVE	1 MASTERS WAY	EWING, NJ	08628	4A	.46
21	240 STOKES AVE	1 MASTERS WAY	PENNINGTON NJ	08534	4A	.44
21	120 STOKES AVE	108 STOKES AVE	PENNINGTON NJ	08534	4A	.44
21	120 STOKES AVE	120 STOKES AVE	EWING, NJ	08638	4B	2.11
21	94 STOKES AVE	4020 NOTTINGHAM WAY	HAMILTON NJ	08690	4A	.80
21	200 KELSEY AVE	3511 HIGHWAY 9 NORTH	HOWELL NJ	07331	4B	4.53
21	1643 N OLDEN AVE EXT	P.O. BOX 247	HOPEWELL, NEW JERSEY	08525	1	3.02

TAXABLE PROPERTY TAX CODES	
1	VACANT LAND
2	RESIDENTIAL PROPERTY (1 - 4 FAMILY)
3A	FARM (HOUSE)
3B	FARM (QUALIFIED)
4A	COMMERCIAL
4C	APARTMENT
5A	RAILROAD CLASS I
5B	RAILROAD CLASS II
15A	PUBLIC SCHOOL PROPERTY
15B	OTHER SCHOOL PROPERTY
15C	PUBLIC PROPERTY
15D	CHURCH & CHARITABLE PROPERTY
15E	CEMETERIES & GRAVEYARDS
15F	OTHER EXEMPT

13.01	9	1510 N OLDEN AVE EXT	PLAINFIELD NJ	07061	4A	21.18
13.01	10	SPRUCE ST	PLAINFIELD, N.J.	07061	1	15.93
13.01	11	1620 N OLDEN AVE EXT	MT LAUREL NJ	08054	4A	3.03
13.01	12	1510 N OLDEN AVE EXT	PLAINFIELD NJ	07061	4A	21.18
13.01	13	1960 SPRUCE ST	EWING, NJ	08638	4A	.82
13.01	14	1600 N OLDEN AVE EXT	EWING, NJ	08638	4A	.62
14	1	1500 HEATH AVE	EWING, NJ	08638	4B	1.57
15	1	119 BRIAN AVE	EWING NJ	08638	2	.17
15	2	121 BRIAN AVE	EWING, NEW JERSEY	08638	2	.13
15	3	123 BRIAN AVE	EWING, NJ	08638	2	.13
15	4	125 BRIAN AVE	EWING, NJ	08638	2	.20
15	5	129 BRIAN AVE	EWING, NEW JERSEY	08638	2	.23
16	1	STOUT & BRIAN AVE	EWING, NJ	08638	4B	.18
16	2	STOUT & BRIAN AVE	EWING, NJ	08638	4B	.18
16	3	STOUT & BRIAN AVE	EWING, NJ	08638	4B	.18
16	4	STOUT & BRIAN AVE	EWING, NJ	08638	4B	.18
16	5	41 STOUT AVE	EWING, NJ	08638	2	.05
16	6	39 STOUT AVE	EWING NJ	08638	2	.04
16	7	37 STOUT AVE	EWING, NJ	08638	2	.04
16	8	33 STOUT AVE	EWING, NJ	08638	1	.09
16	9	27 STOUT AVE	EWING, NJ	08638	2	.13
16	10	25 STOUT AVE	EWING NJ	08638	2	.03
16	11	23 STOUT AVE	EWING, NEW JERSEY	08638	2	.03
16	12	21 STOUT AVE	EWING, NJ	08638	2	.03
16	13	HEATH & BRIAN	EWING, NJ	08638	1	.13
16	14	1442 HEATH AVE	EWING, NJ	08638	4A	.09
16	16	1435 HEATH AVE	EWING, NJ	08638	2	.13
16	17	1434 HEATH AVE	TRENTON, NJ.	08638	2	.04
16	18	1432 HEATH AVE	EWING, NJ	08638	2	.09
16	19	1424 HEATH AVE	EWING, NJ	08638	4B	.22
16	20	1424-28 HEATH AVE	EWING, NJ	08638	4B	.22
16	21	1424-28 HEATH AVE	EWING, NJ	08638	4B	.22
16	22	1419 PRINCETON AVE	EGGHARBOR, NEW JERSEY	08234	2	.11
16	23	1415 PRINCETON AVE	EWING, NJ	08638	2	.05
16	24	1413 PRINCETON AVE	EWING, NEW JERSEY	08638	2	.08
16	25	1411 PRINCETON AVE	EWING, NJ	08638	2	.05
16	26	1409 PRINCETON AVE	EWING, NJ	08638	2	.08
16	27	1403 PRINCETON AVE	EWING, NEW JERSEY	08618	2	.10
16	28	1401 PRINCETON AVE	EWING NJ	08618	2	.06
16	29	43 STOUT AVE	EWING, NJ	08638	2	.08
17	1	1447 N OLDEN AVE	PLUMSTEAD NJ	08527	2	.04
17	2	1445 N OLDEN AVE	EWING, NJ	08638	2	.09
17	3	1443 N OLDEN AVE	EWING, NEW JERSEY	08638	2	.09
17	4	1441 N OLDEN AVE	EWING, NJ	08638	2	.09
17	5	1433 N OLDEN AVE	EWING, NJ	08638	2	.09
17	6	1431 N OLDEN AVE	EWING, NJ	08638	2	.09
17	7	1429 N OLDEN AVE	LAWRENCE, N.J.	08648	2	.04
17	8	1427 N OLDEN AVE	EWING, NEW JERSEY	08638	2	.04
17	9	1425 N OLDEN AVE	EWING, NJ	08638	2	.04
17	10	1423 N OLDEN AVE	EWING, NJ	08638	2	.13
17	11	50 STOUT AVE	EWING, NJ	08638	2	.13
17	12	48 STOUT AVE	EWING, NJ	08638	2	.04
17	13	46 STOUT AVE	EWING, NJ	08638	2	.06
17	14	44 STOUT AVE	EWING, NJ	08638	2	.05
17	15	623 RAINBOW BLVD	LADY LAKE FL	32159	2	.10
17	16	36 STOUT AVE	EWING, NJ	08638	1	.13
17	17	36 STOUT AVE	EWING, NJ	08638	2	.13
17	18	36 STOUT AVE	EWING, NJ	08638	2	.13
17	19	34 STOUT AVE	EWING, NJ	08638	2	.04
17	20	32 STOUT AVE	EWING, NJ	08638	2	.04
17	21	30 STOUT AVE	EWING, NJ	08638	2	.04
17	22	26 STOUT AVE	EWING, NJ	08638	2	.09
17	23	24 STOUT AVE	EWING, NEW JERSEY	08638	2	.09
17	24	1459 PRINCETON AVE	EWING, NJ	08638	4A	.40

TAXABLE PROPERTY TAX CODES	
1	VACANT LAND
2	RESIDENTIAL PROPERTY (1 -4 FAMILY)
3A	FARM (HOUSE)
3B	FARM (QUALIFIED)
4A	COMMERCIAL
4B	INDUSTRIAL
4C	APARTMENT
5A	RAILROAD CLASS I
5B	RAILROAD CLASS II
15A	PUBLIC SCHOOL PROPERTY
15B	OTHER SCHOOL PROPERTY
15C	PUBLIC PROPERTY
15D	CHURCH & CHARITABLE PROPERTY
15E	CEMETERIES & GRAVEYARDS
15F	OTHER EXEMPT

Olden Avenue Redevelopment Plan

Properties within the OARP Area

21.01	68	1460 PROSPECT ST	MORRISVILLE PA	19067	4A	1.77	5B RAILROAD CLASS II 15A PUBLIC SCHOOL PROPERTY 15B OTHER SCHOOL PROPERTY 15C PUBLIC PROPERTY 15D CHURCH & CHARTABLE PROPERTY 15F OTHER EXEMPT
21.01	75	1476 PROSPECT ST	EWING, NJ	08638	4A	.22	
21.01	76	1843 ARENA DRIVE	TRENTON, NEW JERSEY	08610	4A	.00	
21.01	77	316 STOKES AVE	PRINCETON JUNC NJ	08636	1	.23	
21.01	78	340 STOKES AVE	HOPEWELL, NEW JERSEY	08625	1	.46	
22.01	1	660 INGHAM AVE	EWING, NJ	08638	4A	.40	
23	1	50 PARKWAY AVE	HOWELL NJ	07331	1	.00	
23	2	1000 PROSPECT ST	EWING, NJ	08638	4A	.31	
25	1	1060 PROSPECT ST	NEWARK NJ	07102	2	.00	
25	2	27 DOVER AVE	EWING, NJ	08638	2	.19	
25	3	27 DOVER AVE	EWING, NJ	08638	2	.19	
25	4	1068 PROSPECT ST	EWING, NJ	08638	2	.39	
25	6	1042 PROSPECT ST	EWING, NJ	08638	2	.66	
25	7	1032 PROSPECT ST	EWING NJ	08638	2	.36	
25	8	1026 PROSPECT ST	EWING, NJ	08628	15C	.43	
25	9	1018 PROSPECT ST	TRENTON, NJ	08638	2	.38	
25	10	18 HOMESTEAD AVE	EWING, NEW JERSEY	08638	2	.15	
25	11	22 HOMESTEAD AVE	EWING, NEW JERSEY	08638	2	.17	
25	12	1022 PROSPECT ST	EWING, NJ	08638	2	.00	
25	13	1050 PROSPECT ST	EWING, NJ	08638	2	.00	
25	14	18 HOMESTEAD AVE	EWING, NEW JERSEY	08638	2	.15	
25	15	22 HOMESTEAD AVE	EWING, NEW JERSEY	08638	2	.17	
25	25	26 HOMESTEAD AVE	EWING, NEW JERSEY	08638	2	.23	
25	52	26 HOMESTEAD AVE	EWING, NEW JERSEY	08638	2	.23	
25	53	26 HOMESTEAD AVE	EWING, NEW JERSEY	08638	2	.23	
25	54	26 HOMESTEAD AVE	EWING, NEW JERSEY	08638	2	.23	
25	55	26 HOMESTEAD AVE	EWING, NEW JERSEY	08638	2	.23	
25	56	26 HOMESTEAD AVE	EWING, NEW JERSEY	08638	2	.23	
25	57	26 HOMESTEAD AVE	EWING, NEW JERSEY	08638	2	.23	
25	58	26 HOMESTEAD AVE	EWING, NEW JERSEY	08638	2	.23	
25	59	32 HOMESTEAD AVE	EWING, NJ	08638	2	.06	
25	60	34 HOMESTEAD AVE	EWING NJ	08638	2	.06	
25	61	36 HOMESTEAD AVE	EWING, NJ	08638	2	.05	
25	62	26 HOMESTEAD AVE	EWING, NEW JERSEY	08638	2	.23	
25	63	26 HOMESTEAD AVE	EWING, NEW JERSEY	08638	2	.23	
25	64	26 HOMESTEAD AVE	EWING, NEW JERSEY	08638	2	.23	
25	65	111 HAZEL AVE	EWING, NEW JERSEY	08638	2	.00	
25	66	111 HAZEL AVE	EWING, NEW JERSEY	08638	2	.00	
25	67	111 HAZEL AVE	EWING, NEW JERSEY	08638	2	.00	
25	68	111 HAZEL AVE	EWING, NEW JERSEY	08638	2	.00	
25	69	117 HAZEL AVE	EWING, NEW JERSEY	08618	2	.28	
25	70	117 HAZEL AVE	EWING, NEW JERSEY	08618	2	.28	
25	71	117 HAZEL AVE	EWING, NEW JERSEY	08618	2	.28	
25	72	123 HAZEL AVE	EWING, NJ	08638	2	.16	
25	73	123 HAZEL AVE	EWING, NJ	08638	2	.16	
25	74	127 HAZEL AVE	EWING NJ	08638	2	.24	
25	75	127 HAZEL AVE	EWING NJ	08638	2	.24	
25	76	127 HAZEL AVE	EWING NJ	08638	2	.24	
25	77	131 HAZEL AVENUE	EWING, NJ	08638	2	.15	
25	78	131 HAZEL AVENUE	EWING, NJ	08638	2	.15	
25	79	137 ROBBINS AVENUE	EWING, NJ	08638	2	.14	
25	80	135 HAZEL AVE	EWING, NJ	08638	2	.14	
25	81	137 HAZEL AVE	EWING, NJ	08638	2	.14	
25	82	137 HAZEL AVE	EWING, NJ	08638	1	.13	
25	83	33 DOVER AVE	EWING, NJ	08638	1	.19	
25	84	33 DOVER AVE	EWING, NJ	08638	1	.19	
25	85	33 DOVER AVE	EWING, NJ	08638	1	.19	
25	86	37 DOVER AVE	EWING, NJ	08638	1	.13	
25	87	37 DOVER AVE	EWING, NJ	08638	1	.13	
25	88	37 DOVER AVE	EWING, NJ	08638	1	.13	
25	89	109 HAZEL AVE	EWING, NJ	08638	2	.09	
26	89	HAZEL AVE	TRENTON NJ	08618	1	.04	
26	90	HAZEL AVE	TRENTON NJ	08618	1	.04	
26	91	110 HAZEL AVE	EWING, NEW JERSEY	08638	2	.18	
26	92	110 HAZEL AVE	EWING, NEW JERSEY	08638	2	.18	
26	93	110 HAZEL AVE	EWING, NEW JERSEY	08638	2	.18	

To be verified by Ewing Township Tax Office

21	17	82 STOKES AVE	EWING, NJ	08638	4B	1.31
21	18	88 STOKES AVE	EWING NJ	08628	15C	1.11
21	20	216 STOKES AVE	EWING, NJ	08638	1	.23
21	21	232 STOKES AVE	TITUSVILLE, NEW JERSEY	08560	4B	.23
21	22	228 STOKES AVE	EWING, NJ	08628	1	.23
21	24	204 STOKES AVE	718 HOOD BLVD.	19030	4A	.46
21	25	244 STOKES AVE	2 JAKE GARZIO DR	08628	4A	.24
21	26	244 STOKES AVE	2 JAKE GARZIO DR	08628	4A	.24
21	01	1400 PROSPECT ST	TRENTON, NEW JERSEY	08610	1	1.29
21	01	1661 N OLDEN AVE EXT	CHICAGO IL	60603	4A	.89
21	01	404 STOKES AVE	MORRISVILLE PA	19067	4A	.73
21	01	424 STOKES AVE	HOPEWELL, NEW JERSEY	08525	4A	.92
21	01	424 STOKES AVE	HOPEWELL, NEW JERSEY	08525	4A	.92
21	01	424 STOKES AVE	HOPEWELL, NEW JERSEY	08525	4A	.92
21	01	424 STOKES AVE	HOPEWELL, NEW JERSEY	08525	4A	.92
21	01	432 STOKES AVE	HOPEWELL, NEW JERSEY	08525	4A	.42
21	01	433 STOKES AVE	HOPEWELL, NEW JERSEY	08525	4A	1.12
21	01	433 STOKES AVE	HOPEWELL, NEW JERSEY	08525	4A	1.12
21	01	425 STOKES AVE	HOPEWELL, NEW JERSEY	08638	2	.11
21	01	304 STOKES AVE	TRENTON, NJ	08638	4A	.46
21	01	324 STOKES AVE	PRINCETON JUNC NJ	08550	4A	.46
21	01	324 STOKES AVE	PRINCETON JUNC NJ	08550	4A	.46
21	01	332 STOKES AVE	EWING, NJ	08638	4A	.23
21	01	340 STOKES AVE	HOPEWELL, NEW JERSEY	08525	1	.46
21	01	396 STOKES AVE	HOPEWELL, NEW JERSEY	08525	1	.23
21	01	400 STOKES AVE	EWING, NJ	08638	4A	.18
21	01	1498 PROSPECT ST	PRINCETON JUNC NJ	08638	4A	.48
21	01	1681 N OLDEN AVE EXT	TRENTON, NJ	08619	4A	1.01
21	01	1653 N OLDEN AVE EXT	EWING, NEW JERSEY	08638	4A	4.88
21	01	409 STOKES AVE	EWING, NJ	08638	4A	.34
21	01	421 STOKES AVE	TRENTON NJ	08618	2	.11
21	01	408 ROBBINS AVE	TRENTON NJ	08610	4A	.11
21	01	412 ROBBINS AVE	TRENTON NJ	08610	1	.11
21	01	416 ROBBINS AVE	PRINCETON NJ	08540	2	.11
21	01	424 ROBBINS AVE	HOPEWELL, NJ	08525	1	.22
21	01	424 ROBBINS AVE	HOPEWELL, NJ	08525	1	.22
21	01	433 STOKES AVE	HOPEWELL, NEW JERSEY	08525	4A	1.12
21	01	433 STOKES AVE	HOPEWELL, NEW JERSEY	08525	4A	1.12
21	01	425 ROBBINS AVE	PENNINGTON NJ	08534	1	.11
21	01	421 ROBBINS AVE	EWING, NJ	08638	4A	.11
21	01	415 ROBBINS AVE	EWING, NJ	08628	4A	.14
21	01	409 ROBBINS AVE	HOPEWELL, NJ	08525	4A	.19
21	01	408 HAZEL AVE	PENNINGTON NJ	08534	2	.11
21	01	412 HAZEL AVE	TRENTON NJ	08638	2	.11
21	01	416 HAZEL AVE	EWING NJ	08618	2	.17
21	01	424 HAZEL AVE	EWING, NEW JERSEY	08638	2	.17
21	01	424 HAZEL AVE	EWING, NEW JERSEY	08638	2	.17
21	01	426 HAZEL AVE	EWING, NJ	08638	2	.11
21	01	430 HAZEL AVE	EWING, NJ	08638	1	.10
21	01	429 HAZEL AVE	TITUSVILLE, NJ	08560	4A	.17
21	01	425 HAZEL AVE	TITUSVILLE NJ	08560	4A	.20
21	01	421 HAZEL AVE	EWING, NJ	08638	4A	.22
21	01	417 HAZEL AVE	EWING, NJ	08638	2	.22
21	01	413 HAZEL AVE	EWING, NJ	08638	2	.22
21	01	1490 PROSPECT ST	IRVING TX	75015	4A	4.12
21	01	1474 PROSPECT ST	EWING NJ	08638	4A	1.81
21	01	1478 PROSPECT ST	EWING NJ	08638	4A	.27
21	01	1470 PROSPECT ST	TRENTON, N.J.	08648	4A	.22
21	01	336 STOKES AVE	EWING, NJ	08638	1	.23
21	01	316 STOKES AVE	PRINCETON JUNC NJ	08636	1	.23
21	01	300 STOKES AVE	TRENTON NJ	08611	4A	.23
21	01	312 STOKES AVE	TRENTON, NJ	08638	4A	.23
21	01	328 STOKES AVE	EWING, NJ	08638	1	.23
21	01	1681 N OLDEN AVE EXT	TRENTON, NJ	08619	4A	1.01

TAXABLE PROPERTY TAX CODES	
1	VACANT LAND
2	RESIDENTIAL PROPERTY (1 -4 FAMILY)
3A	FARM (HOUSE)
3B	FARM (QUALIFIED)
4A	COMMERCIAL
4B	INDUSTRIAL
4C	APARTMENT
5A	RAILROAD CLASS I

To be verified by Ewing Township Tax Office

Olden Avenue Redevelopment Plan

Properties within the OARP Area

27	171	132 ROBBINS AVE	EWING, NJ	08638	2	.18
27	172	132 ROBBINS AVE	EWING, NJ	08638	2	.18
27	173	132 ROBBINS AVE	EWING, NJ	08638	2	.18
27	174	132 ROBBINS AVE	EWING, NJ	08638	2	.18
27	175	65 DOVER AVE	EWING, NJ	08638	2	.22
27	176	65 DOVER AVE	EWING, NJ	08638	2	.22
27	177	65 DOVER AVE	EWING, NJ	08638	2	.22
27	178	65 DOVER AVE	EWING, NJ	08638	2	.22
27	179	65 DOVER AVE	EWING, NJ	08638	2	.22
27	180	73 DOVER AVE	EWING, NJ	08638	2	.22
27	181	73 DOVER AVE	EWING, NJ	08638	2	.22
27	182	73 DOVER AVE	EWING, NJ	08638	2	.22
27	183	73 DOVER AVE	EWING, NJ	08638	2	.22
27	184	73 DOVER AVE	EWING, NJ	08638	2	.22
27	185	STOKES AVE	TRENTON, NJ	08638	1	.34
27	188	118 ROBBINS AVE	PENNINGTON NJ	08534	1	.18
27	189	118 ROBBINS AVE	PENNINGTON NJ	08534	1	.18
27	190	121 STOKES AVE	EWING, NJ	08638	2	.09
27	191	121 STOKES AVE	EWING, NJ	08638	2	.09
27	192	123 STOKES AVE	EWING, NJ	08638	2	.09
27	193	123 STOKES AVE	EWING, NJ	08638	15D	.09
27	194	127 STOKES AVE	EWING, NJ	08638	15D	.09
27	195	127 STOKES AVE	EWING, NJ	08638	2	.09
27	196	129 STOKES AVE	EWING, NJ	08638	2	.09
27	197	129 STOKES AVE	EWING, NJ	08638	1	.09
27	198	131 STOKES AVE	EWING, NJ	08638	1	.09
28	1	1100 PROSPECT ST	PALM BAY FLORIDA	32907	2	.18
28	2	1100 PROSPECT ST	TITUSVILLE, NEW JERSEY	08560	2	.06
28	3	1108 PROSPECT ST	TITUSVILLE, NEW JERSEY	08560	2	.06
28	5	211 DRYDEN AVE	TITUSVILLE, NEW JERSEY	08560	2	.24
28	6	1110 PROSPECT ST	TRENTON, NEW JERSEY	08619	2	.16
28	8	1116 PROSPECT ST	EWING, NJ	08638	2	.11
28	9	1116 PROSPECT ST	EWING, NJ	08638	2	.12
28	10	1120 PROSPECT ST	EWING, NJ	08638	2	.12
28	11	1120 PROSPECT ST	EWING NJ	08618	2	.18
28	12	1124 PROSPECT ST	EWING NJ	08618	2	.18
28	13	DRYDEN AVE	TRENTON NEW JERSEY	08638	2	.11
28	14	1132 PROSPECT ST	EWING, NJ	08638	1	.08
28	15	1136 PROSPECT ST	EWING, NJ	08638	2	.16
28	16	1144 PROSPECT ST	GALLOWAY TWP NJ	08205	2	.10
28	17	1144 PROSPECT ST	EWING, NJ	08638	2	.42
28	18	1148 PROSPECT ST	EWING, NJ	08638	2	.42
28	19	1148 PROSPECT ST	EWING NJ	08638	2	.42
28	38	18 DOVER AVE	EWING, NJ	08638	4A	.18
28	39	18 DOVER AVE	EWING, NJ	08638	4A	.18
28	40	18 DOVER AVE	EWING, NJ	08638	4A	.18
28	41	18 DOVER AVE	EWING, NJ	08638	4A	.18
28	45	215 DRYDEN AVE	EWING, NJ	08638	2	.05
28	46	221 DRYDEN AVE	LAWRENCEVILLE, N.J.	08648	1	.21
28	47	221 DRYDEN AVE	LAWRENCEVILLE, N.J.	08648	1	.21
28	48	221 DRYDEN AVE	LAWRENCEVILLE, N.J.	08648	1	.21
28	49	221 DRYDEN AVE	LAWRENCEVILLE, N.J.	08648	1	.21
28	50	DRYDEN AVE	EWING, NEW JERSEY	08638	1	.11
28	51	DRYDEN AVE	EWING, NEW JERSEY	08638	1	.11
28	52	1128 PROSPECT ST	EWING, NJ	08638	2	.09
28	53	221 DRYDEN AVE	LAWRENCEVILLE, N.J.	08648	1	.21
29	202	210 DRYDEN AVE	LAMBERTVILLE, N.J.	08628	4A	.22
29	203	210 DRYDEN AVE	LAMBERTVILLE, N.J.	08628	4A	.22
29	204	210 DRYDEN AVE	LAMBERTVILLE, N.J.	08628	4A	.22
29	205	210 DRYDEN AVE	LAMBERTVILLE, N.J.	08628	4A	.22
29	206	210 DRYDEN AVE	LAMBERTVILLE, N.J.	08628	4A	.22
29	207	218 DRYDEN AVE	LAMBERTVILLE, N.J.	08628	4A	.22
29	210	220 DRYDEN AVE	P O BOX 517	19020	4A	.13
29	211	220 DRYDEN AVE	TRENTON NJ	08638	2	.09
29	211	220 DRYDEN AVE	TRENTON NJ	08638	2	.09

TAXABLE PROPERTY TAX CODES	
1	VACANT LAND
2	RESIDENTIAL PROPERTY (1 - 4 FAMILY)
3A	FARM (HOUSE)
3B	FARM (QUALIFIED)
4A	COMMERCIAL
4B	INDUSTRIAL
4C	APARTMENT
5A	RAILROAD CLASS I
5B	RAILROAD CLASS II
15A	PUBLIC SCHOOL PROPERTY
15B	OTHER SCHOOL PROPERTY
15C	PUBLIC PROPERTY
15D	CHURCH & CHARITABLE PROPERTY
15E	CEMETERIES & GRAVEYARDS
15F	OTHER EXEMPT

Parcel ID	Address	Municipality	Assessor's Parcel ID	Area (Acres)	Property Class
26	94	110 HAZEL AVE	EWING, NEW JERSEY	2	08638
26	95	118 HAZEL AVE	EWING, NEW JERSEY	2	08638
26	96	118 HAZEL AVE	EWING, NEW JERSEY	2	08638
26	97	118 HAZEL AVE	EWING, NEW JERSEY	2	08638
26	98	124 HAZEL AVE	EWING, NJ	2	08638
26	99	124 HAZEL AVE	EWING, NJ	2	08638
26	101	126 HAZEL AVE	EWING, NJ	1	08638
26	102	126 HAZEL AVE	EWING, NJ	1	08638
26	103	130 HAZEL AVE	DETROIT, MICHIGAN	2	48205
26	104	130 HAZEL AVE	DETROIT, MICHIGAN	2	48205
26	105	132 HAZEL AVE	EWING, NEW JERSEY	1	08638
26	106	132 HAZEL AVE	EWING, NEW JERSEY	1	08638
26	107	136 HAZEL AVE	EWING, NEW JERSEY	1	08638
26	108	136 HAZEL AVE	EWING, NEW JERSEY	1	08638
26	109	45 DOVER AVE	EWING, NJ	1	08638
26	110	45 DOVER AVE	EWING, NJ	1	08638
26	111	45 DOVER AVE	EWING, NJ	1	08638
26	112	49 DOVER AVE	EWING, NJ	1	08638
26	113	49 DOVER AVE	EWING, NJ	1	08638
26	114	53 DOVER AVE	PISCATAWAY NJ	4A	08655
26	119	113 ROBBINS AVE	TRENTON, NEW JERSEY	1	08618
26	120	113 ROBBINS AVE	TRENTON, NEW JERSEY	1	08618
26	121	113 ROBBINS AVE	TRENTON, NEW JERSEY	1	08618
26	122	113 ROBBINS AVE	TRENTON, NEW JERSEY	1	08618
26	123	117 ROBBINS AVE	EWING, NJ	15C	08628
26	124	117 ROBBINS AVE	EWING, NJ	15C	08628
26	125	119 ROBBINS AVE	EWING, NJ	2	08638
26	126	119 ROBBINS AVE	EWING, NJ	2	08638
26	127	119 ROBBINS AVE	EWING, NJ	2	08638
26	128	125 ROBBINS AVE	EWING, NJ	2	08638
26	129	125 ROBBINS AVE	EWING, NJ	2	08638
26	130	125 ROBBINS AVE	EWING, NJ	2	08638
26	131	131 ROBBINS AVE	EWING, NJ	2	08638
26	132	131 ROBBINS AVE	EWING, NJ	2	08638
26	133	131 ROBBINS AVE	EWING, NJ	2	08638
26	134	133 ROBBINS AVE	EWING, NJ	2	08638
26	135	133 ROBBINS AVE	EWING, NJ	2	08638
26	136	137 ROBBINS AVE	EWING, NEW JERSEY	2	08638
26	139	40 HOMESTEAD AVE	EWING, NJ	2	08638
26	140	40 HOMESTEAD AVE	EWING, NJ	2	08638
26	141	40 HOMESTEAD AVE	EWING, NJ	2	08638
26	142	40 HOMESTEAD AVE	EWING, NJ	2	08638
26	143	44 HOMESTEAD AVE	EWING, NEW JERSEY	2	08638
26	144	44 HOMESTEAD AVE	EWING, NEW JERSEY	2	08638
26	145	48 HOMESTEAD AVE	EWING, NJ	2	08638
26	146	48 HOMESTEAD AVE	EWING, NJ	2	08638
26	147	52 HOMESTEAD AVE	EWING, NEW JERSEY	2	08638
26	148	52 HOMESTEAD AVE	EWING, NEW JERSEY	2	08638
27	150	101 STOKES AVE	EWING, NJ	4A	08638
27	151	101 STOKES AVE	EWING, NJ	4A	08638
27	152	101 STOKES AVE	EWING, NJ	4A	08638
27	153	101 STOKES AVE	EWING, NJ	4A	08638
27	154	101 STOKES AVE	EWING, NJ	4A	08638
27	155	108 ROBBINS AVE	EWING, NEW JERSEY	2	08638
27	161	118 ROBBINS AVE	PENNINGTON NJ	1	08534
27	162	118 ROBBINS AVE	PENNINGTON NJ	1	08534
27	163	120 ROBBINS AVE	EWING, NJ	4C	08638
27	164	120 ROBBINS AVE	EWING, NJ	4C	08638
27	165	124 ROBBINS AVE	EWING, NJ	2	08638
27	166	124 ROBBINS AVE	EWING, NJ	2	08638
27	167	124 ROBBINS AVE	EWING, NJ	2	08638
27	168	124 ROBBINS AVE	EWING, NJ	2	08638
27	169	9 NORTH PEMBERTON RD	PEMBERTON, N.J.	1	08068
27	170	9 NORTH PEMBERTON RD	PEMBERTON, N.J.	1	08068

TAXABLE PROPERTY TAX CODES
1 VACANT LAND
2 RESIDENTIAL PROPERTY (1 -4 FAMILY)
3A FARM (HOUSE)
3B FARM (QUALIFIED)
4A COMMERCIAL
4B INDUSTRIAL
4C APARTMENT
5A RAILROAD CLASS I
5B RAILROAD CLASS II
15A PUBLIC SCHOOL PROPERTY
15B OTHER SCHOOL PROPERTY
15C PUBLIC PROPERTY
15D CHURCH & CHARITABLE PROPERTY
15E CEMETERIES & GRAVEYARDS
15F OTHER EXEMPT



Parcel ID	Address	Municipality	Assessed Value	Area (sq ft)	Property Class	Notes
9 212	226 DRYDEN AVE	EWING, NJ	08638	2	09	
9 214	DRYDEN AVE	EWING, NJ	08638	15D	.18	
9 216	230 DRYDEN AVE	EWING, NJ	08638	2	.13	
9 217	230 DRYDEN AVE	EWING, NJ	08638	2	.13	
9 218	230 DRYDEN AVE	EWING, NJ	08638	2	.13	
9 219	236 DRYDEN AVE	EWING, NJ	08638	15D	.00	
9 226	33 WEBER AVE	EWING, NEW JERSEY	08638	2	.13	
9 227	33 WEBER AVE	EWING, NEW JERSEY	08638	2	.13	
9 228	33 WEBER AVE	EWING, NEW JERSEY	08638	2	.13	
9 229	39 WEBER AVE	TRENTON, NJ	08638	2	.13	
9 230	39 WEBER AVE	TRENTON, NJ	08638	2	.13	
9 231	39 WEBER AVE	TRENTON, NJ	08638	2	.13	
9 232	209 HAZEL AVE	EWING, NEW JERSEY	08638	2	.22	
9 233	209 HAZEL AVE	EWING, NEW JERSEY	08638	2	.22	
9 234	209 HAZEL AVE	EWING, NEW JERSEY	08638	2	.22	
9 235	209 HAZEL AVE	EWING, NEW JERSEY	08638	2	.22	
9 236	209 HAZEL AVE	EWING, NEW JERSEY	08638	2	.22	
9 237	215 HAZEL AVE	EWING, NJ	08638	2	.09	
9 238	215 HAZEL AVE	EWING, NJ	08638	2	.09	
9 239	HAZEL AVE	EWING, NEW JERSEY	08638	1	.09	
9 240	HAZEL AVE	EWING, NEW JERSEY	08638	1	.09	
9 241	225 HAZEL AVE	EWING, NJ	08618	2	.13	
9 242	225 HAZEL AVE	EWING, NJ	08618	2	.13	
9 243	225 HAZEL AVE	EWING, NJ	08618	2	.13	
9 246	231 HAZEL AVE	TRENTON, NJ	08638	2	.09	
9 247	231 HAZEL AVE	TRENTON, NJ	08638	2	.09	
9 248	237 HAZEL AVE	TRENTON, NJ	08638	2	.09	
9 252	DOVER AVE	PENNINGTON NJ	08534	4A	.18	
9 253	DOVER AVE	EWING, NJ	08638	1	.13	
9 254	DOVER AVE	EWING, NJ	08638	1	.13	
9 255	DOVER AVE	EWING, NJ	08638	2	.18	
9 256	30 DOVER AVE	EWING, NJ	08638	2	.18	
9 257	30 DOVER AVE	EWING, NJ	08638	2	.18	
9 258	30 DOVER AVE	EWING, NJ	08638	2	.18	
9 259	38 DOVER AVE	EWING, NJ	08638	2	.13	
9 260	38 DOVER AVE	EWING, NJ	08638	2	.13	
9 261	38 DOVER AVE	EWING, NJ	08638	2	.13	
0 262	206 HAZEL AVE	EWING, NJ	08638	1	.09	
0 263	206 HAZEL AVE	EWING, NJ	08638	1	.09	
0 265	212 HAZEL AVE	EWING, NJ	81623	4A	.18	
0 269	220 HAZEL AVE	EWING, NJ	08638	4A	.18	
0 272	224 HAZEL AVE	EWING, NJ	08638	2	.13	
0 273	224 HAZEL AVE	EWING, NJ	08638	2	.13	
0 274	224 HAZEL AVE	EWING, NJ	08638	2	.13	
0 275	226 HAZEL AVE	EWING, NJ	08638	1	.13	
0 276	226 HAZEL AVE	EWING, NJ	08638	1	.13	
0 277	228 HAZEL AVE	EWING, NJ	08638	1	.13	
0 278	228 HAZEL AVE	TRENTON, NJ	08618	4A	.18	
0 282	45 WEBER AVE	EWING, NJ	08638	4A	.45	
0 283	45 WEBER AVE	EWING, NJ	08638	4A	.45	
0 284	45 WEBER AVE	EWING, NJ	08638	4A	.45	
0 285	45 WEBER AVE	EWING, NJ	08638	4A	.45	
0 286	45 WEBER AVE	EWING, NJ	08638	4A	.45	
0 287	45 WEBER AVE	EWING, NJ	08638	4A	.45	
0 292	207 ROBBINS AVE	PISCATAWAY NJ	08855	1	.09	
0 293	207 ROBBINS AVE	PISCATAWAY NJ	08855	1	.09	
0 294	213 ROBBINS AVE	EWING, NEW JERSEY	08638	2	.09	
0 295	213 ROBBINS AVE	EWING, NEW JERSEY	08638	2	.09	
0 296	215 ROBBINS AVE	TRENTON, NEW JERSEY	08607	2	.09	
0 297	P.O. BOX 1461	TRENTON, NEW JERSEY	08638	2	.27	
0 298	225 ROBBINS AVE	EWING, NJ	08638	2	.27	
0 299	225 ROBBINS AVE	EWING, NJ	08638	2	.27	
0 300	225 ROBBINS AVE	EWING, NJ	08638	2	.27	
0 301	225 ROBBINS AVE	EWING, NJ	08638	2	.27	

TAXABLE PROPERTY TAX CODES
1 VACANT LAND
2 RESIDENTIAL PROPERTY (1 - 4 FAMILY)
3A FARM (HOUSE)
3B FARM (QUALIFIED)
4A COMMERCIAL
4B INDUSTRIAL
4C APARTMENT
5A RAILROAD CLASS I
5B RAILROAD CLASS II
15A PUBLIC SCHOOL PROPERTY
15B OTHER SCHOOL PROPERTY
15C PUBLIC PROPERTY
15D CHURCH & CHARITABLE PROPERTY
15E CEMETERIES & GRAVEYARDS
15F OTHER EXEMPT

Olden Avenue Redevelopment Plan

Properties within the OARP Area

34	451	320 HAZEL AVE	EWING, NJ	08618	2	.09	5A RAILROAD CLASS I
34	452	324 HAZEL AVE	EWING, NJ	08628	1	.09	5B RAILROAD CLASS II
34	453	324 HAZEL AVE	EWING, NJ	08628	1	.09	15A PUBLIC SCHOOL PROPERTY
34	454	328 HAZEL AVE	EWING, NJ	08628	2	.09	15B OTHER SCHOOL PROPERTY
34	455	328 HAZEL AVE	EWING, NJ	08628	2	.09	15C PUBLIC PROPERTY
34	456	332 HAZEL AVE	EWING, NJ	08638	2	.09	15D CHURCH & CHARITABLE PROPERTY
34	457	332 HAZEL AVE	EWING, NJ	08638	2	.09	15E CEMETERIES & GRAVEYARDS
34	458	338 HAZEL AVE	TRENTON, NEW JERSEY	08628	1	.18	15F OTHER EXEMPT
34	459	5 HOLDER AVENUE	TRENTON, NEW JERSEY	08628	1	.18	
34	460	338 HAZEL AVE	TRENTON, NEW JERSEY	08628	1	.18	
34	461	338 HAZEL AVE	TRENTON, NEW JERSEY	08628	1	.18	
34	462	45 TROY AVE	EWING, NEW JERSEY	08628	2	.18	
34	463	45 TROY AVE	EWING, NEW JERSEY	08628	2	.18	
34	464	45 TROY AVE	EWING, NEW JERSEY	08628	2	.18	
34	465	45 TROY AVE	EWING, NEW JERSEY	08628	2	.18	
34	466	53 TROY AVE	EWING, NEW JERSEY	08628	2	.09	
34	467	53 TROY AVE	EWING, NEW JERSEY	08628	2	.09	
34	468	343 ROBBINS AVE	EWING, NJ	08628	1	.18	
34	472	307 ROBBINS AVE	EWING, NJ	08638	4A	.13	
34	473	307 ROBBINS AVE	EWING, NJ	08638	4A	.13	
34	474	307 ROBBINS AVE	EWING, NJ	08638	4A	.13	
34	475	313 ROBBINS AVE	HAMILTON NJ	08610	4A	.13	
34	476	313 ROBBINS AVE	HAMILTON NJ	08610	4A	.13	
34	477	313 ROBBINS AVE	HAMILTON NJ	08610	4A	.13	
34	478	ROBBINS AVE	EWING, NJ	08638	1	.13	
34	479	ROBBINS AVE	EWING, NJ	08638	1	.13	
34	480	ROBBINS AVE	EWING, NJ	08638	1	.13	
34	481	327 ROBBINS AVE	EWING, NJ	08638	2	.09	
34	482	327 ROBBINS AVE	EWING, NJ	08638	2	.09	
34	483	331 ROBBINS AVE	EWING, NJ	08638	2	.09	
34	484	331 ROBBINS AVE	LAWRENCEVILLE NJ	08648	1	.27	
34	485	331 ROBBINS AVE	LAWRENCEVILLE NJ	08648	1	.27	
34	486	331 ROBBINS AVE	LAWRENCEVILLE NJ	08648	1	.27	
34	487	331 ROBBINS AVE	LAWRENCEVILLE NJ	08648	1	.27	
34	488	331 ROBBINS AVE	LAWRENCEVILLE NJ	08648	1	.27	
34	489	335 ROBBINS AVE	EWING, NJ	08638	4A	.13	
34	492	46 WEBER AVE	EWING, NJ	08638	15D	.11	
34	493	46 WEBER AVE	EWING, NJ	08638	15D	.11	
34	494	46 WEBER AVE	EWING, NJ	08638	15D	.11	
34	495	50 WEBER AVE	EWING, NJ	08638	4A	.13	
34	498	58 WEBER AVE	YARDLEY PA	19067	4A	.13	
34	499	58 WEBER AVE	TRENTON, N.J.	08619	4A	.18	
34	500	58 WEBER AVE	TRENTON, N.J.	08619	4A	.18	
34	501	58 WEBER AVE	TRENTON, N.J.	08619	4A	.18	
35	502	306 ROBBINS AVE	EWING NJ	08638	2	.11	
35	503	306 ROBBINS AVE	EWING NJ	08638	2	.11	
35	504	306 ROBBINS AVE	EWING NJ	08638	2	.11	
35	505	312 ROBBINS AVE	HOPEWELL, NEW JERSEY	08525	1	.11	
35	506	312 ROBBINS AVE	HOPEWELL, NEW JERSEY	08525	1	.11	
35	507	320 ROBBINS AVE	TRENTON, N.J.	08607	2	.18	
35	508	320 ROBBINS AVE	TRENTON, N.J.	08607	2	.18	
35	509	320 ROBBINS AVE	TRENTON, N.J.	08607	2	.18	
35	510	320 ROBBINS AVE	TRENTON, N.J.	08607	2	.18	
35	511	322 ROBBINS AVE	TRENTON, N.J.	08607	2	.18	
35	514	328 ROBBINS AVE	WEST WINDSOR NJ	08550	4A	.09	
35	516	332 ROBBINS AVE	EWING, NJ	08628	1	.13	
35	517	332 ROBBINS AVE	LAWRENCEVILLE NJ	08648	4A	.18	
35	518	332 ROBBINS AVE	LAWRENCEVILLE NJ	08648	4A	.18	
35	519	332 ROBBINS AVE	LAWRENCEVILLE NJ	08648	4A	.18	
35	520	338 ROBBINS AVE	LAWRENCEVILLE NJ	08648	4A	.18	
35	521	338 ROBBINS AVE	EWING, NJ	08628	15C	.09	
35	522	65 TROY AVE	EWING, NJ	08628	15C	.09	
35	523	65 TROY AVE	EWING, NJ	08638	1	.13	
35	524	65 TROY AVE	EWING, NJ	08638	1	.13	

To be verified by Ewing Township Tax Office

32	14	PROSPECT ST	1642 PENNINGTON ROAD	EWING, N.J.	08618	1	00
32	15	1214 PROSPECT ST	1214 PROSPECT STREET	EWING, NJ	08638	2	.25
32	16	1214 PROSPECT ST	1214 PROSPECT STREET	EWING, NJ	08638	2	.25
32	17	1214 PROSPECT ST	1214 PROSPECT STREET	EWING, NJ	08638	2	.25
32	18	1230 PROSPECT ST	1230 PROSPECT STREET	EWING, NJ	08638	2	.07
32	19	1234 PROSPECT ST	1234 PROSPECT STREET	EWING, NJ	08638	2	.06
32	20	1234 PROSPECT ST	1234 PROSPECT ST	EWING, NJ	08638	2	.12
32	21	1236 PROSPECT ST	1236 PROSPECT ST	EWING, NJ	08638	2	.13
32	22	1236 PROSPECT ST	1236 PROSPECT ST	EWING, NJ	08638	2	.12
32	23	1244 PROSPECT ST	1244 PROSPECT STREET	EWING, NJ	08638	2	.13
32	24	1244 PROSPECT ST	1244 PROSPECT STREET	EWING, NJ	08638	2	.06
32	25	1244 PROSPECT ST	1244 PROSPECT STREET	EWING, NEW JERSEY	08638	2	.12
32	26	1244 PROSPECT ST	1244 PROSPECT STREET	EWING, NEW JERSEY	08638	2	.12
33	362	26 WEBER AVE	4 WOODFERN AVE	EWING, NJ	08628	4A	.25
33	382	26 WEBER AVE	4 WOODFERN AVE	EWING, NJ	08628	4A	.25
33	384	310 DRYDEN AVE	4 WOODFERN AVE	EWING, NEW JERSEY	08638	4A	.55
33	394	326 DRYDEN AVE	6410 RADCLIFFE STREET	BRISTOL, PA	19007	4A	.13
33	397	330 DRYDEN AVE	6410 RADCLIFFE ST	BRISTOL, PA	19007	4A	.13
33	400	338 DRYDEN AVE	338 DRYDEN AVE	EWING, NJ	08638	2	.09
33	401	338 DRYDEN AVE	338 DRYDEN AVE	EWING, NJ	08638	2	.09
33	402	25 TROY AVE	25 TROY AVENUE	EWING, NEW JERSEY	08638	2	.13
33	405	31 TROY AVE	31 TROY AVENUE	EWING, NJ	08638	2	.04
33	406	31 TROY AVE	31 TROY AVENUE	EWING, NJ	08638	2	.04
33	407	31 TROY AVE	31 TROY AVENUE	EWING, NJ	08638	2	.04
33	408	31 TROY AVE	31 TROY AVENUE	EWING, NJ	08638	2	.04
33	409	37 TROY AVE	16 MIDDLE ROSE STREET	TRENTON, N.J.	08618	2	.13
33	410	37 TROY AVE	16 MIDDLE ROSE STREET	TRENTON, N.J.	08618	2	.13
33	411	37 TROY AVE	16 MIDDLE ROSE STREET	TRENTON, N.J.	08618	2	.13
33	412	307 HAZEL AVE	86 CARLTON AVE	EWING, NJ	08618	2	.09
33	413	307 HAZEL AVE	86 CARLTON AVE	EWING, NJ	08618	2	.09
33	414	311 HAZEL AVE	311 HAZEL AVENUE	EWING, NJ	08638	2	.09
33	415	311 HAZEL AVE	311 HAZEL AVENUE	EWING, NJ	08638	2	.09
33	420	321 HAZEL AVE	325 HAZEL AVE	EWING, NJ	08638	1	.09
33	421	321 HAZEL AVE	325 HAZEL AVE	EWING, NJ	08638	1	.09
33	422	325 HAZEL AVE	325 HAZEL AVE	EWING, NJ	08638	2	.27
33	423	325 HAZEL AVE	325 HAZEL AVE	EWING, NJ	08638	2	.27
33	424	325 HAZEL AVE	325 HAZEL AVE	EWING, NJ	08638	2	.27
33	425	325 HAZEL AVE	325 HAZEL AVE	EWING, NJ	08638	2	.27
33	426	325 HAZEL AVE	325 HAZEL AVE	EWING, NJ	08638	2	.27
33	427	325 HAZEL AVE	325 HAZEL AVE	EWING, NJ	08638	2	.27
33	428	335 HAZEL AVE	39 STOKLEY AVENUE	TRENTON NJ	08611	2	.11
33	429	335 HAZEL AVE	39 STOKLEY AVENUE	TRENTON NJ	08611	2	.11
33	430	335 HAZEL AVE	39 STOKLEY AVENUE	TRENTON NJ	08611	2	.11
33	431	335 HAZEL AVE	39 STOKLEY AVENUE	TRENTON NJ	08611	2	.11
33	432	26 WEBER AVE	4 WOODFERN AVE	EWING, NJ	08628	4A	.25
33	436	32 WEBER AVE	P. O. BOX 1441	TRENTON NJ	08607	2	.09
33	437	32 WEBER AVE	P. O. BOX 1441	TRENTON NJ	08607	2	.09
33	438	38 WEBER AVE	6 KAREN CT	PRINCETON NJ	08540	4A	.18
33	439	38 WEBER AVE	6 KAREN CT	PRINCETON NJ	08540	4A	.18
33	440	38 WEBER AVE	6 KAREN CT	PRINCETON NJ	08540	4A	.18
33	441	38 WEBER AVE	6 KAREN CT	PRINCETON NJ	08540	4A	.18
33	442	31 TROY AVE	31 TROY AVENUE	EWING, NJ	08638	2	.04
33	443	31 TROY AVE	31 TROY AVENUE	EWING, NJ	08638	2	.04
33	444	31 TROY AVE	31 TROY AVENUE	EWING, NJ	08638	2	.04
33	445	31 TROY AVE	31 TROY AVENUE	EWING, NJ	08638	2	.04
34	442	46 WEBER AVE	46 WEBER AVE	EWING, NJ	08638	2	.04
34	443	46 WEBER AVE	46 WEBER AVE	EWING, NJ	08638	15D	.11
34	444	310 HAZEL AVE	112 CLIFTON AVE #9	EWING, NJ	08701	2	.13
34	445	310 HAZEL AVE	112 CLIFTON AVE #9	LAKWOOD, NJ	08701	2	.13
34	446	310 HAZEL AVE	112 CLIFTON AVE #9	LAKWOOD, NJ	08701	2	.13
34	447	314 HAZEL AVE	314 HAZEL AVENUE	EWING, NJ	08638	2	.13
34	448	314 HAZEL AVE	314 HAZEL AVENUE	EWING, NJ	08638	2	.13
34	449	314 HAZEL AVE	314 HAZEL AVENUE	EWING, NJ	08638	2	.13
34	450	320 HAZEL AVE	1075 PENNINGTON RD	EWING, NJ	08618	2	.09

**TAXABLE PROPERTY TAX CODES**  
 1 VACANT LAND  
 2 RESIDENTIAL PROPERTY (1 - 4 FAMILY)  
 3A FARM (HOUSE)  
 3B FARM (QUALIFIED)  
 4A COMMERCIAL  
 4B INDUSTRIAL  
 4C APARTMENT

Olden Avenue Redevelopment Plan

Properties within the OARP Area

Parcel ID	Address	Location	APN	Area	Code	Taxable Property Tax Codes
40.01 6	1688 N OLDEN AVE EXT	EWING NJ	08638	4A	22	1 VACANT LAND
40.01 7	1565 SIXTH ST	EWING, NEW JERSEY	08638	4B	35	2 RESIDENTIAL PROPERTY (1 - 4 FAMILY)
40.01 8	1688 N OLDEN AVE EXT	LAWRENCEVILLE, N. J.	08648	4A	08	3A FARM (HOUSE)
40.01 9	1571 SIXTH ST	LAWRENCEVILLE NJ	08648	4B	75	3B FARM (QUALIFIED)
40.01 10	1566 SIXTH ST	EWING, NEW JERSEY	08638	4B	35	4A COMMERCIAL
40.01 11	1581 SIXTH ST	EWING, NJ	08638	15D	1.46	4B INDUSTRIAL
40.01 14	1688 N OLDEN AVE EXT	EWING NJ	08638	4A	22	4C APARTMENT
40.01 15	1688 N OLDEN AVE EXT	EWING NJ	08638	4A	22	5A RAILROAD CLASS I
40.01 16	1672 N OLDEN AVE EXT	LAWRENCEVILLE, N.J.	08648	1	45	15A PUBLIC SCHOOL PROPERTY
40.01 17	1672 N OLDEN AVE EXT	LAWRENCEVILLE, N.J.	08648	1	45	15B OTHER SCHOOL PROPERTY
40.01 18	1676 N OLDEN AVE EXT	LAWRENCEVILLE, N.J.	08648	4A	00	15C PUBLIC PROPERTY
40.01 19	1688 N OLDEN AVE EXT	EWING NJ	08638	4A	22	15D CHURCH & CHARITABLE PROPERTY
40.01 20	1532 PROSPECT ST	EWING NJ	08618	1	70	15E CEMETERIES & GRAVEYARDS
40.01 21	1542 PROSPECT ST	EWING, NJ	08638	2	67	15F OTHER EXEMPT
40.01 22	1682 N OLDEN AVE EXT	PLAINSBORO, NJ	08536	4A	00	
40.01 23	1542 PROSPECT ST	EWING, NJ	08638	2	67	
40.01 33	1680 N OLDEN AVE EXT	TRENTON, NEW JERSEY	08618	4A	1.07	
40.01 35	1688 N OLDEN AVE EXT	EWING NJ	08638	4A	22	
40.01 36	1688 N OLDEN AVE EXT	EWING NJ	08638	4A	22	
41 4	1650 N OLDEN AVE EXT	CENTER VALLEY, PA	18034	4A	2.67	
41 165	1597 FIFTH ST	TITUSVILLE NJ	08560	4A	39	
41 170	1595 FIFTH ST	NEW HOPE, PA	18938	4A	68	
41 173	1576 SIXTH ST	EWING, NEW JERSEY	08628	2	09	
41 174	1578 SIXTH ST	EWING, NJ	08618	2	09	
41 175	1580 SIXTH ST	EWING, NJ	08638	2	12	
41 176	1584 SIXTH ST	EWING, NEW JERSEY	08638	2	21	
41 177	1584 SIXTH ST	EWING, NEW JERSEY	08638	2	21	
41 178	1594 SIXTH ST	EWING, NJ	08638	2	22	
41 179	1594 SIXTH ST	EWING, NJ	08638	2	22	
42 1	1632 N OLDEN AVE EXT	YARDLEY, PA	19067	4A	2.88	
42 2	1640 N OLDEN AVE EXT	TRENTON NJ	08618	4A	1.21	
42 3	2100 COLUMBIA AVE	EWING, NJ	08638	4A	27	
42 5	50 ARCTIC PARKWAY	PENNINGTON, NJ	08534	4A	.73	
42 6	19 ARCTIC PARKWAY	EWING, NJ	08638	4A	.38	
42 7	32 ARCTIC PARKWAY	TRENTON, NEW JERSEY	08618	4A	66	
42 9	38 ARCTIC PARKWAY	EWING, NJ	08638	2	00	
42 10	48 ARCTIC PARKWAY	EWING, NEW JERSEY	08638	2	.33	
42 10.01	48 ARCTIC PARKWAY	EWING, NEW JERSEY	08638	2	.33	
42 11	19 ARCTIC PARKWAY	EWING, NJ	08638	4A	.38	
42 12	40 ARCTIC PARKWAY	EWING, NJ	08638	4A	.33	
42 13	46 ARCTIC PARKWAY	P.O. BOX 65067	08638	4A	.27	
42 14	36 ARCTIC PARKWAY	LAWRENCEVILLE NJ	08648	4A	.33	
42 15	22 ARCTIC PARKWAY	EWING, NJ	08638	4A	.28	
42 16	1640 N OLDEN AVE EXT	EWING, NJ	08638	4B	.73	
42 17	28 ARCTIC PARKWAY	TRENTON NJ	08618	4A	1.21	
42 18	22 ARCTIC PARKWAY	EWING, NJ	08638	2	26	
42 19	1580 FIFTH ST	EWING, NJ	08638	4B	.73	
42 20	1686 FIFTH ST	EWING, NJ	08638	4A	.46	
42 21	1590 FIFTH ST	EWING, NJ	08638	4A	.40	
42 22	1684 FIFTH ST	EWING, NJ	08638	4A	.43	
42 23	1688 FIFTH ST	GARDEN CITY NY	11530	4A	.67	
42 24	1600 FIFTH ST	LAWRENCEVILLE NJ	08648	4A	.67	
42 25	1600 FIFTH ST	EWING, NJ	08638	4A	1.34	
42 26	20 ARCTIC PARKWAY	EWING, NJ	08638	4A	1.34	
42 27	1590 FIFTH ST	TRENTON NJ	08618	4A	1.42	
42 28	1680 FIFTH ST	EWING, NJ	08638	4A	.43	
42 29	1688 FIFTH ST	PENNINGTON NJ	08534	4A	.53	
53 47	2100 SPRUCE ST	PENNINGTON NJ	08534	4A	.40	
53 48	2114 SPRUCE ST	SUNRISE FL	33322	4A	.11	
53 49	2114 SPRUCE ST	EWING NJ	08638	2	.22	
53 50	2118 SPRUCE ST	EWING NJ	08638	2	.22	
53 51	2118 SPRUCE ST	EWING, NJ	08638	2	.22	
53 52	2120 SPRUCE ST	EWING, NJ	08628	2	.11	
53 53	2124 SPRUCE ST	EWING, NJ	08638	4A	.11	
53 54	2128 SPRUCE ST	EWING NJ	08638	4A	.09	

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35	525	69 TROY AVE	EWING, NJ	08638	1	.13
35	528	341 STOKES AVE	TRENTON, NEW JERSEY	08638	2	.18
35	529	167 HOMECREST AVE.	TRENTON, NEW JERSEY	08638	2	.18
35	530	341 STOKES AVE	TRENTON, NEW JERSEY	08638	2	.18
35	531	167 HOMECREST AVE.	TRENTON, NEW JERSEY	08638	2	.18
35	534	313 STOKES AVE	EWING, NJ	08638	4A	.22
35	538	STOKES AVE	EWING, NJ	08628	1	.13
35	541	321 STOKES AVE	EWING, NJ	08628	1	.13
35	543	329 STOKES AVE	EWING, NJ	08628	4A	.09
35	544	329 STOKES AVE	EWING, NJ	08628	1	.13
35	545	329 STOKES AVE	EWING, NJ	08628	1	.13
35	546	331 STOKES AVE	EWING, NJ	08628	1	.13
35	548	335 STOKES AVE	CREAM RIDGE NJ	08514	4A	.09
35	549	335 STOKES AVE	CREAM RIDGE NJ	08514	2	.09
35	550	339 STOKES AVE	CREAM RIDGE NJ	08514	2	.09
35	551	339 STOKES AVE	TRENTON, NEW JERSEY	08618	2	.18
35	552	339 STOKES AVE	TRENTON, NEW JERSEY	08618	2	.18
35	553	64 WEBER AVE	EWING, NJ	08638	2	.13
35	553	64 WEBER AVE	EWING, NJ	08638	2	.13
35	554	64 WEBER AVE	EWING, NJ	08638	2	.13
35	555	70 WEBER AVE	EWING, NJ	08638	4A	.18
35	559	76 WEBER AVE	EWING, NJ	08638	4A	.18
35	560	76 WEBER AVE	EWING, NJ	08638	1	.13
35	561	76 WEBER AVE	EWING, NJ	08638	1	.13
35	562	312 ROBBINS AVE	EWING, NJ	08638	1	.13
36	27	1304 PROSPECT ST	HOPEWELL, NEW JERSEY	08525	1	.11
36	28	1304 PROSPECT ST	EWING, NJ	08638	2	.22
36	29	1304 PROSPECT ST	EWING, NJ	08638	2	.22
36	30	1304 PROSPECT ST	EWING, NJ	08638	2	.22
36	31	409 HAZEL AVE	EWING, NJ	08638	2	.22
36	31	409 HAZEL AVE	EWING, NJ	08638	2	.22
36	593	24 TROY AVE	EWING, NJ	08638	2	.22
36	594	24 TROY AVE	EWING, NJ	08611	2	.23
36	595	24 TROY AVE	TRENTON NJ	08611	2	.23
36	596	24 TROY AVE	TRENTON NJ	08611	2	.23
36	598	30 TROY ST	TRENTON NJ	08611	2	.23
36	598	30 TROY ST	EWING, NJ	08638	2	.22
36	599	30 TROY ST	EWING, NJ	08638	2	.22
36	600	30 TROY ST	EWING, NJ	08638	2	.22
36	601	30 TROY ST	EWING, NJ	08638	2	.22
36	602	30 TROY ST	EWING, NJ	08638	2	.22
38	631	44 TROY AVE	PENNINGTON NJ	08534	1	.09
38	632	44 TROY AVE	PENNINGTON NJ	08534	1	.09
38	633	48 TROY AVE	EWING, NEW JERSEY	08638	2	.09
38	634	48 TROY AVE	EWING, NEW JERSEY	08638	2	.09
38	635	409 ROBBINS AVE	EWING, NEW JERSEY	08638	2	.09
38	637	56 TROY AVE	HOPEWELL, NJ	08525	4A	.19
38	638	56 TROY AVE	YARDVILLE, NEW JERSEY	08620	4A	.18
38	639	56 TROY AVE	YARDVILLE, NEW JERSEY	08620	4A	.18
38	640	56 TROY AVE	YARDVILLE, NEW JERSEY	08620	4A	.18
39	679	64 TROY AVE	YARDVILLE, NEW JERSEY	08620	4A	.18
39	680	64 TROY AVE	YARDVILLE, NEW JERSEY	08620	4A	.18
39	681	64 TROY AVE	YARDVILLE, NEW JERSEY	08620	4A	.18
39	682	68 TROY AVE	EWING, NJ	08638	1	.13
39	683	68 TROY AVE	EWING, NJ	08638	1	.13
39	684	74 TROY AVE	EWING, NJ	08638	1	.13
39	685	74 TROY AVE	EWING, NJ	08638	1	.13
39	686	74 TROY AVE	EWING, NJ	08638	1	.13
39	687	74 TROY AVE	EWING, NJ	08638	1	.13
39	688	78 TROY AVE	EWING, NJ	08638	1	.09
40	248	1599 SIXTH ST	EWING, NJ	08638	1	.09
40	249	1599 SIXTH ST	EWING, NJ	08638	2	.17
40	250	1585 SIXTH ST	EWING, NJ	08638	2	.17
40.01	1	1564 PROSPECT ST	LAWRENCEVILLE, NEW JERSEY	08648	2	.11
40.01	2	SIXTH ST REAR	EWING, NJ	08638	4A	.93
40.01	3	1660 N OLDEN AVE EXT	EWING, NJ	08638	4A	3.29
40.01	5	1665 SIXTH ST	VINELAND, NJ	08360	4A	.91
			EWING, NEW JERSEY	08638	4B	.35

TAXABLE PROPERTY TAX CODES	
1	VACANT LAND
2	RESIDENTIAL PROPERTY (1 - 4 FAMILY)
3A	FARM (HOUSE)
3B	FARM (QUALIFIED)
4A	COMMERCIAL
4B	INDUSTRIAL
4C	APARTMENT
5A	RAILROAD CLASS I
5B	RAILROAD CLASS II
15A	PUBLIC SCHOOL PROPERTY
15B	OTHER SCHOOL PROPERTY
15C	PUBLIC CHURCH & CHARITABLE PROPERTY
15D	CHURCH & CHARITABLE PROPERTY
15E	CEMETERIES & GRAVEYARDS
15F	OTHER EXEMPT

Olden Avenue Redevelopment Plan

Properties within the OARP Area

108	16	2199 SPRUCE ST	180 BULL RUN RD	EWING NJ	08638	4A	.17
108	17	2199 SPRUCE ST	180 BULL RUN RD	EWING NJ	08638	4A	.17
108	108	2199 SPRUCE ST	180 BULL RUN RD	EWING NJ	08638	4A	.17
108	138	2199 SPRUCE ST	180 BULL RUN RD	EWING NJ	08638	4A	.17
108	139	2199 SPRUCE ST	180 BULL RUN RD	EWING NJ	08638	4A	.17
108	140	2199 SPRUCE ST	180 BULL RUN RD	EWING NJ	08638	4A	.17
109	142	141 FIFTH ST	59 TAYLOR TERR	HOPEWELL NJ	08525	4A	.13
109	145	145 FIFTH ST	145 FIFTH STREET	EWING NJ	08638	4A	.13
109	147	147 FIFTH ST	145 FIFTH ST	EWING NJ	08638	1	.14
109	150	153 FIFTH ST	153 5TH STREET	EWING, NEW JERSEY	08638	2	.35
109	151	153 FIFTH ST	153 5TH STREET	EWING, NEW JERSEY	08638	2	.35
109	152	153 FIFTH ST	153 5TH STREET	EWING, NEW JERSEY	08638	2	.35
109	153	153 FIFTH ST	153 5TH STREET	EWING, NEW JERSEY	08638	2	.35
109	154	153 FIFTH ST	153 5TH STREET	EWING, NEW JERSEY	08638	2	.35
109	155	153 FIFTH ST	153 5TH STREET	EWING, NEW JERSEY	08638	2	.35
109	165	4 JANE ST	1008 WHITEHEAD ROAD EXT.	EWING, NJ	08638	4A	.42
109	175	1 IRVEN ST	1 IRVEN STREET	EWING, NJ	08638	4A	.23
109	176	IRVEN ST	487 WHITEHEAD RD	TRENTON, NEW JERSEY	08619	2	.17
109	177	IRVEN ST	487 WHITEHEAD RD	TRENTON, NEW JERSEY	08619	2	.17
109	178	IRVEN ST	487 WHITEHEAD RD	TRENTON, NEW JERSEY	08619	2	.17
109	179	182 IRVEN ST	182 IRVEN ST	EWING NJ	08638	2	.34
109	180	182 IRVEN ST	182 IRVEN ST	EWING NJ	08638	2	.34
109	189	310 FOURTH ST	310 FOURTH STREET	EWING, NJ	08638	2	.38
109	197	318 FOURTH ST	4 ANDREE PL	MERCERVILLE NJ	08619	4A	.10
109	199	320 FOURTH ST	1201 BRUNSWICK AVE	TRENTON NJ	08638	4A	.09
110	201	IRVEN ST	6 FLOWER HILL LANE	PENNINGTON NJ	08534	1	.14
110	202	IRVEN ST	6 FLOWER HILL LANE	PENNINGTON NJ	08534	1	.14
110	203	IRVEN ST	6 FLOWER HILL LANE	PENNINGTON NJ	08534	1	.14
110	204	IRVEN ST	6 FLOWER HILL LANE	EWING, NJ	08638	4A	.00
110	206	209 IRVEN ST	204 IRVEN ST	EWING, NJ	08638	2	.00
110	219	219 TASKER ST	204 IRVEN STREET	EWING, NJ	08638	1	.13
110	220	TASKER ST	204 IRVEN STREET	EWING, NJ	08638	1	.09
110	221	TASKER ST	204 IRVEN STREET	EWING, NJ	08638	1	.09
110	222	225 TASKER ST	2191 SPRUCE STREET	TRENTON, NEW JERSEY	08638	1	.10
110	231	322 FOURTH ST	6 FLOWER HILL LANE	PENNINGTON N. J.	08534	4A	.00
110	232	322 FOURTH ST	6 FLOWER HILL LANE	PENNINGTON N. J.	08534	4A	.00
110	233	322 FOURTH ST	6 FLOWER HILL LANE	PENNINGTON N. J.	08534	4A	.00
110	234	322 FOURTH ST	6 FLOWER HILL LANE	PENNINGTON N. J.	08534	4A	.00
110	235	322 FOURTH ST	6 FLOWER HILL LANE	PENNINGTON N. J.	08534	4A	.00
110	236	322 FOURTH ST	6 FLOWER HILL LANE	PENNINGTON N. J.	08534	4A	.00
110	237	322 FOURTH ST	6 FLOWER HILL LANE	PENNINGTON N. J.	08534	4A	.00
110	238	322 FOURTH ST	6 FLOWER HILL LANE	PENNINGTON N. J.	08534	4A	.00
110	239	322 FOURTH ST	6 FLOWER HILL LANE	PENNINGTON N. J.	08534	4A	.00
110	240	322 FOURTH ST	6 FLOWER HILL LANE	PENNINGTON N. J.	08534	4A	.00
110	241	322 FOURTH ST	6 FLOWER HILL LANE	PENNINGTON N. J.	08534	4A	.00
110	242	322 FOURTH ST	6 FLOWER HILL LANE	PENNINGTON N. J.	08534	4A	.00
111	242	334 FOURTH ST	340 FOURTH STREET	EWING, NEW JERSEY	08638	4A	.74
111	247	340 FOURTH ST	340 FOURTH STREET	EWING, NEW JERSEY	08638	4A	.00
111	260	TASKER ST	13 HARMONY WY	NEWTOWN PA	18940	1	.22
111	273	314 DICKENSON ST	13 HARMONY WY	NEWTOWN PA	18940	4A	.46
112	300	DICKENSON ST	P.O. BOX 5456	TRENTON, NEW JERSEY	08638	1	.09
112	301	DICKENSON ST	P.O. BOX 5456	TRENTON, NEW JERSEY	08638	1	.09
112	302	DICKENSON ST	P.O. BOX 5456	TRENTON, NEW JERSEY	08638	1	.09
112	303	DICKENSON ST	P.O. BOX 5456	TRENTON, NEW JERSEY	08638	1	.09
112	304	DICKENSON ST	P.O. BOX 5456	TRENTON, NEW JERSEY	08638	1	.09
112	305	DICKENSON ST	P.O. BOX 5456	TRENTON, NEW JERSEY	08638	1	.09
112	306	DICKENSON ST	P.O. BOX 5456	TRENTON, NEW JERSEY	08638	1	.09
112	307	309 DICKENSON ST	P.O. BOX 5456	TRENTON, N.J.	08638	4A	.18
112	318	DICKENSON ST	21-23 INDUSTRY COURT	EWING, NEW JERSEY	08638	1	.18
112	398	342 FOURTH ST	342 FOURTH STREET	EWING, NJ	08638	4A	.18
112	403	344 FOURTH ST	1201 BRUNSWICK AVE	EWING, NJ	08638	4A	.11
112	404	350 FOURTH ST	350 FOURTH STREET	EWING, NJ	08638	4A	.09
112	407	352 FOURTH ST	350 FOURTH STREET	EWING, NEW JERSEY	08618	1	.09
112	408	3 INDUSTRY CT	3 INDUSTRY CT	EWING, NJ	08638	4A	.15

1	VACANT LAND
2	RESIDENTIAL PROPERTY (1 - 4 FAMILY)
3A	FARM (HOUSE)
3B	FARM (QUALIFIED)
4A	COMMERCIAL
4B	INDUSTRIAL
4C	APARTMENT
5A	RAILROAD CLASS I
5B	RAILROAD CLASS II
15A	PUBLIC SCHOOL PROPERTY
15B	OTHER SCHOOL PROPERTY
15C	PUBLIC PROPERTY
15D	CHURCH & CHARITABLE PROPERTY
15E	CEMETERIES & GRAVEYARDS
15F	OTHER EXEMPT

To be verified by Ewing Township Tax Office



Olden Avenue Redevelopment Plan

Properties within the OARP Area

118.04	17	1739 N OLDEN AVE EXT	25 CRESTMONT AVE	EWING, NJ	08618	4A	.34	4C APARTMENT
118.04	19	1479 PROSPECT ST	9 WOOLSEY COURT	PENNINGTON, NJ	08534	2	.92	5A RAILROAD CLASS I
118.04	20	1761 N OLDEN AVE EXT	1761 N OLDEN AVE	TRENTON, N.J.	08638	4A	.95	5B RAILROAD CLASS II
118.04	21	1769 N OLDEN AVE EXT	1769 N OLDEN AVE	EWING, N.J.	08638	4A	.00	15A PUBLIC SCHOOL PROPERTY
118.04	24	1723 N OLDEN AVE EXT	1723 N OLDEN AVE	EWING, N.J.	08638	4A	.91	15B OTHER SCHOOL PROPERTY
118.04	25	1757 N OLDEN AVE EXT	1757 NORTH OLDEN	TRENTON, NEW JERSEY	08638	4A	1.04	15C PUBLIC PROPERTY
118.04	30	1729 N OLDEN AVE EXT	P O BOX 672346 C/O ITM SER	HOUSTON TX	77267	4A	.00	15D CHURCH & CHARITABLE PROPERTY
118.04	38	1751 N OLDEN AVE EXT	P O BOX 2198	MEMPHIS TN	38101	4A	2.86	15F OTHER EXEMPT
118.04	39	1721 N OLDEN AVE EXT	1710 N OLDEN AVE	EWING, NJ	08638	4A	.92	
118.04	40	1743 N OLDEN AVE EXT	22 MONTAQUE AVE	EWING, NJ	08628	4A	.82	
118.04	43	1475 PROSPECT ST	1016 HARVARD DR	YARDLEY PA	19067	4A	1.52	
118.04	50	PROSPECT ST	80 PARK PLAZA DEPT T-6B	NEWARK, NEW JERSEY	07101	4A	.68	
118.04	53	1471 PROSPECT ST	34 SCOTCH RD	EWING, NJ	08628	4A	5.93	
118.04	53.01	1471 PROSPECT ST REAR	2 JAKE GARZIO DR	EWING NJ	08628	15C	6.45	
118.04	63	1463 PROSPECT ST	1463 PROSPECT STREET	EWING, NJ	08638	4A	.00	
118.04	73	1765 N OLDEN AVE EXT	1769 NO. OLDEN AVE	EWING, NEW JERSEY	08638	4A	.65	
234.01	2	1885 N OLDEN AVE EXT	2556 SOUTH BROAD STREET	TRENTON, NEW JERSEY	08610	4A	2.60	
234.01	41	1875 N OLDEN AVE EXT	261 OLD YORK RD S 700-40	JENKINTOWN, PA	19046	4A	2.00	
234.02	8	1870 N OLDEN AVE EXT	10 KNOLL TAVERN ROAD	FLEMINGTON, NEW JERSEY	08822	4A	.96	
295	7	1852 N OLDEN AVE EXT	P O BOX 1671	ORLANDO FL	32802	4A	1.40	
295	25	1517 PENNINGTON RD	P.O. BOX 711	DALLAS, TEXAS	75221	4A	.38	
295	50	1860 N OLDEN AVE EXT	400 W MAIN ST PO BOX 2001	BOUND BROOK NJ	08805	4A	.69	
295	55	1852 N OLDEN AVE EXT	P O BOX 1671	ORLANDO FL	32802	4A	1.40	
295	56	1852 N OLDEN AVE EXT	P O BOX 1671	ORLANDO FL	32802	4A	1.40	
296	16	1481 PENNINGTON RD	905 NORTH KINGS HWY	CHERRY HILL NJ	08034	1	5.27	
296	17	1481 PENNINGTON RD	905 NORTH KINGS HWY	CHERRY HILL NJ	08034	1	5.27	
296	18	1481 PENNINGTON RD	905 NORTH KINGS HWY	CHERRY HILL NJ	08034	1	5.27	
296	19	1481 PENNINGTON RD	905 NORTH KINGS HWY	CHERRY HILL NJ	08034	1	5.27	
296	20	1481 PENNINGTON RD	905 NORTH KINGS HWY	CHERRY HILL NJ	08034	1	5.27	
296	21	1481 PENNINGTON RD	905 NORTH KINGS HWY	CHERRY HILL NJ	08034	1	5.27	
296	22	1481 PENNINGTON RD	905 NORTH KINGS HWY	CHERRY HILL NJ	08034	1	5.27	
296	23	1481 PENNINGTON RD	905 NORTH KINGS HWY	CHERRY HILL NJ	08034	1	5.27	
296	24	1855 N OLDEN AVE EXT	317 HUDSON STREET	TRENTON, NJ	08611	4A	.00	
296	44	1481 PENNINGTON RD	905 NORTH KINGS HWY	CHERRY HILL NJ	08034	1	5.27	
296	45	1481 PENNINGTON RD	905 NORTH KINGS HWY	CHERRY HILL NJ	08034	1	5.27	
296	46	1481 PENNINGTON RD	905 NORTH KINGS HWY	CHERRY HILL NJ	08034	1	5.27	
296	47	1481 PENNINGTON RD	905 NORTH KINGS HWY	CHERRY HILL NJ	08034	1	5.27	
296	49	1479 PENNINGTON RD	1479 PENNINGTON ROAD	EWING, NEW JERSEY	08618	15D	.96	
296	50	1479 PENNINGTON RD	1479 PENNINGTON ROAD	EWING, NEW JERSEY	08618	15D	.96	
296	51	1479 PENNINGTON RD	1479 PENNINGTON ROAD	EWING, NEW JERSEY	08618	15D	.96	
461	4	1500 PENNINGTON RD	P.O. BOX 53	HOUSTON, TEXAS	77001	4A	1.05	
461	5	1832 N OLDEN AVE EXT	200 HOLLEDER PKY TAX DEPT	ROCHESTER, NY	14615	4A	.20	
461	37	1510 PENNINGTON RD	191 WERTSVILLE RD.	RINGOES NJ	08551	4A	.23	
461	51	1822 N OLDEN AVE EXT	RR #1 BOX 1153	BEACH LAKE, PA	18405	4A	.41	
461	53	1830 N OLDEN AVE EXT	1830 N OLDEN AVE	EWING, NJ	08638	4A	.56	
701	44	1120 SPRUCE ST	34 MONTAQUE AVE	EWING, NJ	08628	4A	4.34	

112	409	5 INDUSTRY CT	32 S MAIN ST	STOCKTON NJ	08559	4A	.15
112	410	7 INDUSTRY CT	48 BERNARD DR	MORRISVILLE, PA	19067	1	.16
112	411	9 INDUSTRY CT	6410 RADCLIFFE STREET	BRISTOL, PA	19007	4A	.90
112	416	21 INDUSTRY CT	21 INDUSTRY COURT	EWING, NJ	08638	4A	.88
113	2	10 INDUSTRY CT	959 RIDGE RD	MONMOUTH JCT, NEW JERSEY	08652	4A	.00
113	4	4 INDUSTRY CT	4 INDUSTRY COURT	EWING, NEW JERSEY	08638	4A	.57
113	6	8 INDUSTRY CT	8 INDUSTRY COURT	EWING, NJ	08638	4A	.28
113	7	18 INDUSTRY CT	18 INDUSTRY COURT	TRENTON, N.J.	08638	4A	.93
113	11	20 INDUSTRY CT	20 INDUSTRY COURT	EWING, NJ	08638	4A	.00
113	12	22 INDUSTRY CT	1723 N OLDEN AV	EWING NJ	08638	4A	.50
113	409	354 FOURTH ST	1618 PRINCETON AVENUE	TRENTON, NEW JERSEY	08638	4A	.24
113	413	358 FOURTH ST	110 SIXTH AV	NEPTUNE NJ	07753	4A	.24
113	414	358 FOURTH ST	110 SIXTH AV	NEPTUNE NJ	07753	4A	.24
113	415	358 FOURTH ST	110 SIXTH AV	NEPTUNE NJ	07753	4A	.24
113	416	358 FOURTH ST	110 SIXTH AV	NEPTUNE NJ	07753	4A	.24
113	417	358 FOURTH ST	110 SIXTH AV	NEPTUNE NJ	07753	4A	.24
117	1	1995 SPRUCE ST	5765 VALLEY STREAM DR	DOYLESTOWN PA	18901	1	2.20
117	2	410 FOURTH ST	2 JAKE GARZIO DR	EWING, NEW JERSEY	08628	15C	9.50
117	6	FOURTH ST	218 FOURTH STREET	EWING, NJ	08638	4A	1.67
117	7	210 FOURTH ST	210 FOURTH ST	EWING NJ	08638	4A	.72
117	8	FOURTH ST	218 FOURTH STREET	EWING, NJ	08638	4A	1.67
117	9	FOURTH ST	2 JAKE GARZIO DR	EWING, NJ	08628	15C	1.13
118	13	1841 N OLDEN AVE EXT	655 ROUTE 524	ALLENTOWN NJ	08501	4A	.00
118	28	1835 N OLDEN AVE EXT	301 SOUTH TRYON ST. M-9	CHARLOTTE, NC	28288	1	.17
118	36	1831 N OLDEN AVE EXT	301 SOUTH TRYON ST. M-9	CHARLOTTE, NC	28288	1	.17
118	41	1480 PENNINGTON RD	301 SOUTH TRYON ST. M-9	CHARLOTTE, NC	28288	4A	2.42
118	44	BRUCE LA	301 SOUTH TRYON ST. M-9	CHARLOTTE, NC	28288	4A	1.11
118	45	1801 N OLDEN AVE EXT	90 WOODBRIDGE CTR DR #150	WOODBRIDGE NJ	07095	4A	.00
118	46	1833 N OLDEN AVE EXT	2 JAKE GARZIO DR	EWING, NJ	08628	15C	.21
118	49	1823 N OLDEN AVE EXT	56 DOUGLAS STREET	LAMBERTVILLE, NJ	08530	4A	.14
118	54	1817 N OLDEN AVE EXT	2 JAKE GARZIO DRIVE	EWING, NJ	08628	15C	.32
118	55	1821 N OLDEN AVE EXT	2 JAKE GARZIO DRIVE	EWING, NJ	08628	4A	.18
118	58	1801 N OLDEN AVE EXT	90 WOODBRIDGE CTR DR #150	WOODBRIDGE NJ	07095	4A	.00
118	59	1827 N OLDEN AVE EXT	2 JAKE GARZIO DRIVE	EWING, NJ	08628	15C	.09
118	62	1829 N OLDEN AVE EXT	2 JAKE GARZIO DR	EWING, NJ	08628	15C	.09
118.01	2	1750 N OLDEN AVE EXT	194 NASSAU ST ATT SANDS J	PRINCETON NJ	08542	4A	.00
118.01	3	1557 PROSPECT ST	1557 PROSPECT STREET	EWING, NJ	08618	2	.00
118.01	5	1768 N OLDEN AVE EXT	680 WHITEHEAD ROAD	TRENTON NJ	08648	4A	.00
118.01	6	1559 PROSPECT ST	1559 PROSPECT STREET	EWING, NJ	08638	2	.17
118.01	7	1700 N OLDEN AVE EXT	P.O. BOX 9104 #3398	CLEARWATER, FLORIDA	33758	4A	1.27
118.01	8	1750 N OLDEN AVE EXT	194 NASSAU ST ATT SANDS J	PRINCETON NJ	08542	4A	.00
118.01	9	1764 N OLDEN AVE EXT	76 MIRY BROOK ROAD	TRENTON, NEW JERSEY	08690	4A	.26
118.01	10	1434 PARKSIDE AVE	1558 NORTH OLDEN AVE	EWING, NJ	08638	4A	.91
118.01	11	1750 N OLDEN AVE EXT	194 NASSAU ST ATT SANDS J	PRINCETON NJ	08542	4A	.00
118.01	13	THIRTEENTH ST	513 EAGLES CHASE DRIVE	LAWRENCEVILLE, N.J.	08648	1	.06
118.01	14	1710 N OLDEN AVE EXT	1710 NORTH OLDEN AVE	EWING, NJ	08638	4A	5.21
118.01	15	1760 N OLDEN AVE EXT	14 BALLIGOMINGO RD	WEST CONSHOCKEN PA	19428	4A	.71
118.01	16	1410 PARKSIDE AVE	680 WHITEHEAD RD	LAWRENCEVILLE NJ	08648	4A	.14
118.01	17	1750 N OLDEN AVE EXT	194 NASSAU ST ATT SANDS J	PRINCETON NJ	08542	4A	.00
118.01	18	1750 N OLDEN AVE EXT	194 NASSAU ST ATT SANDS J	PRINCETON NJ	08542	4A	.00
118.01	19	1734 N OLDEN AVE EXT	630 MILLCROSS RD.	LANCASTER, PA.	17601	4A	1.37
118.01	23	1555 PROSPECT ST	1555 PROSPECT STREET	EWING, NJ	08638	2	.16
118.01	26	1730 N OLDEN AVE EXT	608 CORPORATE DR W	LANGHORNE PA	19047	4A	1.73
118.02	6	1816 N OLDEN AVE EXT	1816 N OLDEN AVE	EWING NJ	08638	4A	.27
118.02	38	1520 PENNINGTON RD	ONE WOODHAVEN MALL S 202	BENSALEM PA	19020	4A	.26
118.02	106	1812 N OLDEN AVE EXT	1812 NORTH OLDEN AVE.	TRENTON, NJ.	08618	4A	.12
118.02	107	1814 N OLDEN AVE EXT	1200 BUSTLETON PIKE, SUIT	FEASTERVILLE, PA.	19053	4A	.26
118.04	3	1739 N OLDEN AVE EXT	25 CRESTMONT AVE	EWING, NJ	08618	4A	.34
118.04	4	1483 PROSPECT ST	1483 PROSPECT STREET	EWING, NJ	08638	2	.00
118.04	5	1701 N OLDEN AVE EXT	P O BOX 52085	PHOENIX AZ	85072	4A	.00
118.04	11	1715 N OLDEN AVE EXT	9 WOOLSEY COURT	PENNINGTON, NEW JERSEY	08534	4A	.38
118.04	12	1479 PROSPECT ST	9 WOOLSEY COURT	PENNINGTON, NJ	08534	2	.92
118.04	14	1735 N OLDEN AVE EXT	1735 N OLDEN AVE	EWING, NJ	08638	4A	1.37
118.04	16	1711 N OLDEN AVE EXT	117 GLEN VALLEY RD	YARDLEY PA	19067	4A	.19

TAXABLE PROPERTY TAX CODES

- 1 VACANT LAND
- 2 RESIDENTIAL PROPERTY (1 - 4 FAMILY)
- 3A FARM (HOUSE)
- 3B FARM (QUALIFIED)
- 4A COMMERCIAL
- 4B INDUSTRIAL

# APPENDIX



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*..... Floor Area Ratio Scale Table. ....*

**EWING FAR SLIDING SCALE TABLE**

	LOT SIZE (sq. ft.)	START POINT	MULTIPLIER	BASE FAR (sq. ft.)	GROSS FLOOR AREA PERMITTED (GFAP)	FORMULA	SAMPLE TEST VAULES
	2000	0	0.2	600	1000		
	3000	0	0.2	600	1200		
	3500	0	0.2	600	1300		
	4000	0	0.2	600	1400		
<b>BRACKET</b>	4500	0	0.2	600	1500	<b>GFAP = ((LOT SIZE - Start Point) X .2) + 600</b>	<b>1600</b>
<b>1</b>	5000	0	0.2	600	1600		
	5500	0	0.2	600	1700		
	6000	0	0.2	600	1800		
	6500	0	0.2	600	1900		
	7000	0	0.2	600	2000		
	7499	0	0.2	600	2100		
	7500	7500	0.18	2100	2100	<b>GFAP = ((LOT SIZE - Start Point) X .18) + 2100</b>	
	8000	7500	0.18	2100	2190		
<b>BRACKET</b>	8500	7500	0.18	2100	2280	<b>GFAP = (LOT SIZE - 7500) X .18 + 2100</b>	<b>2280</b>
<b>2</b>	9000	7500	0.18	2100	2370		
	9500	7500	0.18	2100	2460		
	9999	7500	0.18	2100	2550		
	10000	10000	0.045714	2550	2550	<b>GFAP = ((LOT SIZE - Start Point) X .045714) + 2550</b>	
	10500	10000	0.045714	2550	2573		
	11000	10000	0.045714	2550	2596	<b>GFAP = (LOT SIZE - 10000) X .045714 + 2550</b>	<b>2687</b>
<b>BRACKET</b>	11500	10000	0.045714	2550	2619		
<b>3</b>	12000	10000	0.045714	2550	2641		
	12500	10000	0.045714	2550	2664		
	13000	10000	0.045714	2550	2687		
	13500	10000	0.045714	2550	2710		
	14000	10000	0.045714	2550	2733		
	14500	10000	0.045714	2550	2756		
	15000	15000	0.075	2925	2925	<b>GFAP = ((LOT SIZE - Start Point) X .075) + 2925</b>	
	15500	15000	0.075	2925	2963		
	16000	15000	0.075	2925	3000	<b>GFAP = (LOT SIZE - 15000) X .075 + 2925</b>	<b>3075</b>
	16500	15000	0.075	2925	3038		
	17000	15000	0.075	2925	3075		
<b>BRACKET</b>	17500	15000	0.075	2925	3113		
<b>4</b>	18000	15000	0.075	2925	3150		
	18500	15000	0.075	2925	3188		
	18749	15000	0.075	2925	3206		
	18750	15000	0.075	2925	3206		
	19000	15000	0.075	2925	3225		
	20000	15000	0.075	2925	3300		
	21000	21000	0.02827	3350	3350	<b>GFAP = ((LOT SIZE - Start Point) X .02827) + 3350</b>	
	22000	21000	0.02827	3350	3378		
	23000	21000	0.02827	3350	3407	<b>GFAP = (LOT SIZE - 21000) X .02827 + 3350</b>	<b>3689</b>
	24000	21000	0.02827	3350	3435		
	25000	21000	0.02827	3350	3463		
	26000	21000	0.02827	3350	3491		
	27000	21000	0.02827	3350	3520		
	28000	21000	0.02827	3350	3548		
	29000	21000	0.02827	3350	3576		
<b>BRACKET</b>	30000	21000	0.02827	3350	3604		
<b>5</b>	31000	21000	0.02827	3350	3633		
	32000	21000	0.02827	3350	3661		
	33000	21000	0.02827	3350	3689		
	34000	21000	0.02827	3350	3718		
	35000	21000	0.02827	3350	3746		
	36000	21000	0.02827	3350	3774		
	37000	21000	0.02827	3350	3802		
	38000	21000	0.02827	3350	3831		
	39000	21000	0.02827	3350	3859		
	40000	21000	0.02827	3350	3887		
	41000	21000	0.02827	3350	3915		
	42000	21000	0.02827	3350	3944		
	43000	21000	0.02827	3350	3972		
	44000	21000	0.02827	3350	4000		
	87000	21000	0.02827	3350	5216		

APPENDIX



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*..... Report to Council on Accident Data, Traffic Calming. ....*



**MEMORANDUM**

TO: Ewing Township Council

FROM: Chuck Latini, PP, AICP

CC: Jack Ball, Mayor  
Robert Kull, ETRA Chairperson  
Tyrone Garret, ETRA Executive Director

RE: Request for additional information concerning the Olden Avenue  
Redevelopment Plan

Date: March 2, 2009

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The following is at the request of Township Council in its effort to obtain information in support of the introduction of the Olden Avenue Redevelopment Plan. As you will see, there is a glut of support for the Plan itself as well as some of the Plan's concepts that we will continue to develop once the Plan is adopted.

Given the extraordinary amount of data available, we focused on several documents and provided a summary of each, as well as links to the full documents themselves. Notwithstanding information from outside sources, the Township's own Master Plan provides us with a first look at the issues encountered along Olden Avenue. To support the proposed revisions, we have supplemented the findings of the Ewing Township Master Plan with outside sources to show Council how other communities across the nation have created solutions to the prevalent issues that plague the Olden Avenue Redevelopment Area.

Just about all of the negative qualities that Olden Avenue exhibits; the vast quantities of surface parking, traffic congestion and safety issues, as well as pedestrian hostility, has led planners and engineers to conclude that land development and transportation are inextricably linked. A few of the available documents published by entities such as NJDOT and Maryland DOT depict that recommended solutions are in-fact consistent with the approach the Olden Avenue Redevelopment Plan seeks to implement.

It is important to highlight that this new Redevelopment Plan deals primarily with the land development side of a two-prong issue that involves both land use and transportation issues. While roads are in-fact addressed within the Plan, the Plan merely sets the stage for the important road design work to come. However, and most importantly, the Township needs to be proactive in addressing the stagnant conditions that are present along the Olden Avenue corridor for its future's sake.

Chairman Kull has long referred to this planning effort as a two-phase process. This phase, the Plan before you, is the "Do no harm" phase while the second phase is the "Do good" phase. Council should be mindful that phase 2 is not before you at this time. The "do no harm" phase is setting land use standards in place to allow us to capture additional opportunities without jeopardizing the long term viability of the Area. The Plan allows existing businesses to grow and prosper under a unified vision, but will not allow short-sighted, single-interest development plans to become an additional impediment to the

long-term economic viability of the commercial district. Phase 2, the "do good" phase will be the finalization of the Olden Avenue redesign. Design considerations will likely include streetscape standards, and traffic calming solutions so that public infrastructure can be upgraded to bolster private economic investment. Again, the Plan's recommendations on roadway improvements and streetscape are conceptual and require additional coordination with the County of Mercer, Police Department and business community to finalize.

The Plan as presented today offers the foundation on which new development will be presented to the Township and negotiated. The details on road design, while based in solutions implemented in areas similar in nature to Olden Avenue from throughout the Country are not finalized in this Plan. The Plan merely solidifies and protects the land use opportunities (while recognizing the roadway design issues) that are available to the Township and its business community.

**Accident Data Analysis**

Based on the information found in the Master Plan, as well as additional data provided by the Township's Police Department, accident reports along Olden Avenue are as follows.

The Master Plan indicated that a total of 229 accidents in Ewing Township for 2002 and 2003. Of these, a total of 142 (62%) accidents occurred along Olden Avenue between Pennington Road and Princeton Avenue with the highest frequency occurring in the area of Artic Parkway (39, 17%), Prospect Street (32, 14%), Parkside Avenue (28, 12%) and Pennington Road (24, 11%),

Nine (9) or 17% occurred at the intersection of Olden Avenue with Parkside Avenue. At the intersection of Olden Avenue and Parkside Avenue, six (6) of the accidents involved through vehicles along Olden Avenue hitting through northbound vehicles. This could indicate red light running or signal visibility questions that need to be addressed. A total of 22 accidents occurred at various intersections along Olden Avenue.

One fatality occurred in 2003 along Olden Avenue in the area of 5<sup>th</sup> Street. Of the accidents with injuries, 78 (70%) occurred along Olden Avenue.

To supplement the Master Plan, the Police Department furnished the following data on Olden Avenue for Council's review.

<b>Intersection</b>	<b>2007</b>	<b>2008</b>	<b>2009 (to-date)</b>
Olden & Pennington	44	35	2
Olden & Parkside	21	35	2
Olden & Prospect	31	30	6
Olden & Arctic	32	28	2
Olden & Princeton	14	15	0
	142	143	12 (so far)

**Accident Summary**

The data indicates the issues experienced along Olden Avenue and need to calm traffic. While lowering speed limits is part of the solution, driver behavior must also be curbed. Some of the solutions involve what the planning and transportation world refer to as "Road Diets". We have included the definition of the road diet to follow. In essence

the combination between physical and aesthetic improvements can work toward making Olden Avenue a better experience for the consumer.

While the Police and traffic bureau are not able to track speeding violations by road without a time consuming effort, it has been characterized in an unfavorable light, and is noticeable to all users of the road. We need to continue to work toward finalizing solutions to calm traffic while positively influencing driver behavior and creating a better place to do business.

In addition to the traffic analysis, Master Plan offers some additional insightful dialogue. *“Ewing Township in conjunction with the County, the DVRPC, the State and in some instances adjacent municipalities needs to ensure that items on the strategic improvement list move expeditiously and in coordination with the development patterns that come before the Township and region. The improvements are consistent with the goals of supporting Smart Growth by focusing growth in infill areas where infrastructure currently exists and will encourage the use and growth of public transit options and alternatives. Furthermore, alternatives are to maximize accessibility potential for pedestrian, bicycle and transit users and reduce reliance on auto-dependent travel.”*

*A series of Strategic Improvements for enhancing traffic flow to meet future development needs includes:*

- *North Olden Avenue improvements to facilitate redevelopment activities and slow down traffic including streetscapes and expanded bus service to reduce congestion*
- *Extension of Calhoun Street to Olden Avenue*
- *Enhanced bicycle, greenway and pedestrian systems”*

### **Roadway Design and Land use Integration.**

The following is information regarding the validity of the CONCEPTS of reducing curb-cuts, and coordinating streetscape and sign design standards with “Road Diets”. These reports offer communities guidance on all the available techniques to assist area like Olden Avenue solve its land development and transportation problems in order to create better shopping experiences.

- New Jersey Department of Transportation; “*Mobility and Community Form: A Guide to Linking Transportation and Land Use in the Municipal Master Plan*”  
Recall, a redevelopment plan is a detailed master plan for a specific area. The NJ DOT has studied the critical link between land use and transportation for several years now. The report that is highlighted here explores many creative ways to achieve success at both the land use and transportation components of a community. Lane widths, crosswalks, signage, and landscaping treatments have been found to be part of the equation as there is clearly no one-size-fits-all approach. While we have appended some of the relevant sections from the report, the full report can be found at <http://www.state.nj.us/transportation/community/mobility/guide.shtm>
- Maryland Department of Transportation; “*When Main Street is a State Highway: Blending Function, Beauty and Identity*”  
The title of this suggests “Main Street”, the publication address different road types and commercial districts. Furthermore, County routes and State

Highways are many times synonymous. The report talks about the various components that go into integrating roadway design with community objectives. The full report can be found at

<http://www.sha.state.md.us/businessWithSHA/projects/ohd/Mainstreet/mainstreet.asp>

- Duchess County Planning and Development: "Greenway Guide; Commercial Strip Redevelopment"

This publication from Poughkeepsie, New York hits all the salient points we seek to address through the Redevelopment Plan. From land use and architecture to auto dependency, the report presents similar solutions as to what the Olden Avenue Redevelopment Plan addresses. The full report can be found at

<http://www.co.dutchess.ny.us/CountyGov/Departments/Planning/ELPgreenwayguide.htm>

- Sierra Business Council; "Building Vibrant Sierra Communities: A Commercial and Mixed Use Handbook"

This report identifies and characterizes commercial districts of similar characteristics as Olden Avenue. The report indicates that land use practice and roadway design are not exclusive but require coordination to meet local objectives. The full report can be found at

<http://www.sbcouncil.org/Publications/Mixed-Use-Handbook>

- City of Vandalia, Ohio; "Transportation Plan"

The City's planning effort involves coordination with transportation and roadway design. Access management (aka curb-cuts) is a part of this plan. Recommendation for their "Urban Corridors" of which Olden can be classified, solutions presented are entirely consistent with the Redevelopment Plan. The full report can be found on-line at

<http://www.ci.vandalia.oh.us/uploads/Comp%20Plan/TRANSPORTATION.pdf>

- Road Diets Support Local Economic Development

Atlantic Boulevard in Del Ray Beach Florida, and another couplet of main street streets in West Palm Beach County went on road diets. In each case the businesses did much better once the roads were made more attractive and speeding was reduced. The Atlantic Beach treatment was so successful that it is being extended another 10 blocks. Ferndale, Michigan, also saw great success with such solutions. Prior to the transition businesses struggled. Following the conversion there has been a major return of shoppers. The treatment is being extended. The full report can be found at

<http://www.walkablestreets.com/diet.htm>

- Grandview Avenue Speed Reductions (Smart Growth Network, 2002)

Grandview Avenue in University Place, Washington is a busy two-lane suburban road where traffic averaged 44 miles per hour despite a 35-mph posted speed limit, until the roadway was redesigned with narrower traffic lanes, bike lanes, landscaping and sidewalks. After the project, average traffic speeds have declined to 35-mph, and the road is much more attractive for pedestrians and cyclists. The full report can be found at

<http://www.smartgrowth.org/pdf/gettosg.pdf>

APPENDIX



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*..... Master plan Consistency Report. ....*



## MEMORANDUM

**TO:** Ewing Township Planning Board

**FROM:** Chuck Latini, PP, AICP

**CC:** Ewing Township Redevelopment Agency (ETRA)

**RE:** Consistency Report comparing the Olden Avenue Redevelopment Plan to the Township's Master Plan

**Date:** April 5, 2009

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We have reviewed the Township's Master Plan and have provided the Agency with a Master Plan consistency review report typical of what we see in other communities that our firm does work in. In this memo you find that I took excerpts directly from the Master Plan itself. As you will see, the language in the Master Plan is clear and provides a firm foundation for the Olden Avenue Redevelopment Plan. In my opinion, the Plan is not only consistent with the Township's Master Plan, but an implementation mechanism to it.

### Summary

The Township Council, at their meeting on March 2, 2009, introduced the Olden Avenue Redevelopment Plan (OARP) as an ordinance on first reading, and forwarded it as such to the Township Planning Board for public hearing and their review and comment. Both the Municipal Land Use Law (NJSA 40:55D 26 a) and the Local Housing and Redevelopment Law (LHRL 40A:12A-7 (d.) & (e.)) require that prior to the adoption of any land use regulation, that a governing body refers said development regulation to the Planning Board. Specifically, the LHRL in regards to a redevelopment plan and the referral from the governing body requires the following:

- d. *All provisions of the redevelopment plan shall be either substantially consistent with the municipal master plan or designed to effectuate the master plan; by the municipal governing body may adopt a redevelopment plan which is inconsistent with or not designed to effectuate the master plan by affirmative voter of a majority of its full authorized membership with the reasons for so acting set forth in the redevelopment plan.*
- e. *Upon receipt of the transmittal, the Planning Board shall within 45 days after referral, transmit a report to the government body containing its recommendations concerning the redevelopment plan. The report shall include an identification of any provisions in the proposed redevelopment plan, which are inconsistent with the master plan and recommendations concerning these inconsistencies and any other matters as the Board deems appropriate.*

The OARP is presented as the enacting ordinances/ zone plan for the area identified as the Olden Avenue Redevelopment Area, which was designated as an Area in Need of Redevelopment in June of 1997. It is as such because today, too many conflicting provisions allow for the coordinated revitalization of the Redevelopment Area.

To address the aforementioned statute and supply the Agency with information to necessary to understand where the OARP stands in relation to the Master Plan, we have reviewed both the proposed OARP and the Master Plan together. As you are aware, I will be in attendance to present the OARP and review the plans details with the Planning Board. I can also be available for discussion with the Board's professionals as deemed necessary.

The following report represents our review pursuant to statute, of the proposed ordinance and the identification of any potential inconsistencies with the Master Plan. My comments are addressed in each section in italics below.

### **Olden Avenue Redevelopment Plan**

#### *Redevelopment Area Zone Boundary*

The Olden Avenue Redevelopment Plan is the proposed zoning ordinance for the area that was designated as an Area in Need of Redevelopment and located in the easternmost portion of the Township. The Area has been subject to much study and as the Master Plan points out, the Urban Land Institute (ULI) also set forth forth recommendations that were included as well. The OARP indicates that it considered and was the result of the review of the Master Plan, input received from public meetings, input from the businesses, Mayor's meeting, as well as the Agency's (ETRA) experience in implementing the prior version of the Redevelopment Plan dated 1999.

#### *Plan Overview*

In short, the Plan proposes to break down the area into subareas, each with a specific focus as conditions change from one end of the area to other. The OARP, while an ordinance, is also a comprehensive plan for the area. The Plan provides a great deal of detail represented in graphic illustrations, mapping and text, used to help to clarify what future development should look like and how the Plan's principles can be implemented.

The Plan breaks the OARP into eight (8) zone districts, with each of these zones creating enhanced development and design standards to attract new investment but varying in: permitted uses, intensity of development, building height and floor area. Additionally, there are also both mandated and bonus requirements in each of the zones. These include but are not limited to:

- Affordable housing or contributions to affordable housing;
- Off-tract improvements to the infrastructure within the area for sanitary, stormwater, and roads;
- Contributions in land or design toward Park, open space or environmental buffering from natural resources, and;
- Provisions for cultural and community spaces throughout the area.

The plan offers inducements or bonuses in terms of greater height, or additional floor area for integrated design that work toward creating a sustainable economic climate complete with public spaces and better access. It treats economic development holistically whereby a strong tax base is sought with less focus on individual ratables.

In its consideration of designing the pedestrian environment, the Plan has examined the vehicular movement through this heavily trafficked area. The road improvements as well as stormwater & sanitary systems and other utilities are addressed as off tract improvement contributions that will be required as part of developer agreements.

Additional design work will be necessary to finalize the concepts of the Plan such as curb-cut reductions and streetscape improvements to Olden Avenue itself

Overall, the Plan envisions utilizing design to create a dramatic gateway for the community, a dynamic pedestrian friendly business district, and an improved circulation/traffic pattern while adding to the quality of life in surrounding neighborhoods.

The Plan also introduces the concept of mixed-use development in a more measurable way. This addition is not an inconsistency with the Master Plan, but upon review of the text of the Master Plan, we feel is in fact consistent with known planning practice.

### **2006 Ewing Township Master Plan**

The Master Plan is a comprehensive policy document that is designed to articulate the Township's vision for the future. Within the Ewing Township Master Plan are various elements and maps as well as goals, objectives, and recommendations listed throughout the Plan. While most of the Master Plan is applicable to the entire Township, there are also specific recommendations for the Olden Avenue Redevelopment Area. Upon review of the Master Plan, it is easily discernable how it is applicable to the OARP. The following summary reflects the Master Plan sections that address the area identified as the Olden Avenue Redevelopment Area.

#### **-Goals and Objectives Element**

The MLUL requires that a Master Plan include Goal and Objectives. In Ewing's Master Plan, there are goals and objectives identified, they are as follows:

#### Community Vision

Goal: Enhance and create a sense of place that encourages economic vitality and community activity through well-designed land development that is consistent with established and planned land use patterns and preserves the community's suburban and urban landscapes alike, which make Ewing Township a unique and desirable place to both live and work.

#### Objectives:

- Encourage the preservation of existing neighborhoods through innovative community-based programs that target all socio-economic demographics as well as protect existing stable communities.
- Reduce auto-dependency through innovative design practices that encourage and allow for pedestrian activity where appropriate.
- Consistent with recommendations in the State Development and Redevelopment Plan for Metropolitan and Suburban Planning Areas, capitalize on opportunities for redevelopment.
- Encourage the continued redevelopment of business corridors and centers including Olden Avenue, as well as key intersections such as Parkway and Olden Avenues, Pennington and Ewingville Roads, Pennington and Somerset Roads, and Pennington Road and Parkway Avenue.

**COMMENT:** *The OARP addresses these issues throughout the entire document. This goal and its associated objectives are the basis of the OARP. From the vision statement to the regulations,*

the Plan enhances this goal and its objectives throughout. In addition, the OARP not only meets the intent of the State Plan but in-fact implements it.

### Land Use

Goal: Preserve residential neighborhoods.

#### Objectives:

- Provide effective buffering of adjacent nonresidential properties.
- Where development is proposed in adjacent municipalities that will impact residential communities in Ewing Township, all reasonable measures should be taken to participate in the public hearing process and minimize deleterious and negative impacts to Ewing Township residents and the general quality of life in Ewing Township.
- Minimize the deleterious impacts of collegiate rental units on owner occupied single-family residential neighborhoods while fostering the need for rental housing opportunities for people with limited means for home-ownership.
- Encourage infill housing that is consistent with the scale and character of existing neighborhoods at a block level.

**COMMENT:** This goal and its associated objectives are addressed in various places within the OARP. With exception to Area #4, all other subareas about residential land uses. Some examples of how the OARP implements this goal and its objectives are as follows;

Area 2- This area seeks to create additional housing opportunities that includes college related activity. Adding such housing for students will also help relieve the impacts on single-family neighborhoods.

Area 5- The development standards and conceptual road network works to open-up and create an "eyes on the street" development approach that works to stabilize the neighborhoods and provide additional housing opportunities while ensuring proper and adequate buffers are in place from surrounding industrial uses.

Area 6- The development standards in this subarea ensure substantial buffering from residential land uses.

Area 8- The intent of this zone is to ensure proper transitioning and buffering between residential and non-residential land uses.

Goal: Increase available housing options for pre-retirement, retirement and elderly residents.

#### Objectives:

- Encourage mixed-use, pedestrian driven activity located within close proximity to goods and services.
- Through zoning and appropriate design guidelines, create opportunity for various types of active adult housing.

**COMMENT:** This goal and its associated objectives are addressed in various places within the OARP as the pedestrian has become an important component in strengthening the area. While the Plan addresses pedestrian activity throughout the area, Area #1 is one specific area where the primary objective is connect neighborhoods to goods and services. Area #2 is where a mixed-use approach is sought thorough enhanced development and design standards. Additionally, Area #4 and Area #5 seek to connect the pedestrian to the goods and services both existing and permitted.

Goal: Maintain and rehabilitate older neighborhoods.

Objectives:

- Continue to use available resources such as the Neighborhood Preservation Program, Rehabilitation Area designations and other tools to encourage rehabilitation of older neighborhoods.

**COMMENT:** *This goal and its associated objective are addressed in several places within the OARP. There are several areas within the Plan area that have existing neighborhoods located within them. The redevelopment area designation, and the Township-wide 5-year rehabilitation tax abatement, combined with the vision of the OARP will work to strengthen the following neighborhoods within the Plan.*

*Area #5- the existing neighborhood is being addressed through new development that will spur reinvestment in the neighborhood and redevelopment of industrial property.*

*Area #7b- the Plan seeks to stabilize the neighborhood with enhanced development and design provisions that allow for the transformation of underutilized industrial facilities adjacent to residential land uses. Implementation of the Plan will lead to the potential of exploring funding options like the HMFA Neighborhood Tax Credit program and Neighborhood Preservation Program.*

Goal: Maximize potential for expansion of the economic base to support the costs of providing municipal services and education to Ewing residents.

Objectives:

- Continue the redevelopment of the Olden Avenue Corridor through coordination of the municipal Master Plan and the Olden Avenue Redevelopment Plan to create a unified vision for the residential community and local businesses.
- Identify remaining tracts of land with potential for economic development through the use of Redevelopment, Planned Developments and General Development Plans that encourage connectivity to existing neighborhoods and enhance opportunity to make Ewing a destination to live, work and play.
- Consider incorporating the findings of the Urban Land Institute (ULI) study of Ewing, Trenton and Lawrence into recommendations for development and redevelopment in the Land Use Element of the Master Plan.
- Encourage economic development that supports bicycle and walk to work programs through mixed-use community design where appropriate.

**COMMENT:** *The implements this goal and associated objectives in its entirety.*

Goal: Analyze existing land use patterns and underlying zoning and make recommendations for changes where incongruous land uses directly abut one another.

Objectives:

- In accordance with State Plan policies and procedures, encourage future development to occur at appropriate locations and intensities in accordance with transportation and environmental capacities.

- Discourage deviations from established land use patterns that would permit incompatible and/or conflicting land uses being developed adjacent to one another; where appropriate amend zoning to prohibit incongruous land uses.
- Where appropriate, allow for low intensity mixed use smart growth developments such as second story residential uses together with neighborhood commercial business uses.

**COMMENT:** *This goal and its associated objectives are addressed in various places within the OARP. The Plan seeks to address incongruous land uses, but more importantly seeks to integrate a design approach that encourages coordination in order to limit conflicting uses and their functions. The Plan also seeks to improve the natural environment and allows for mixed-use smart growth developments that enhance the existing land development patterns. This specifically is addressed in Area #1, Area #2, Area #4, Area #5, Area #7 and Area #8*

### Circulation

Goal: Provide alternative routes for regional traffic to disperse and diffuse traffic to reduce and eliminate existing and potential congestion.

Objectives:

- Look at alternative east-west and north-south grid connections to facilitate vehicular traffic through the Township in an expeditious manner.

**COMMENT:** *This goal and its associated objective are addressed in Area #2, Area #4, Area #5, and Area #7. A complete discussion of this occurs on page 49 of the Plan. In addition, the Plan looks to ensure additional opportunities are not lost through ill-advised development.*

Goal: Combine circulation and land use objectives wherever possible.

Objectives:

- Implement a network of pathways for bicycle and pedestrian use through reservation of open space in new planned developments and existing abandoned rail R.O.W and other property.
- Reduce the impact of roadway design on existing land uses and the ability to attract a more diverse business mix. Specifically, look at Olden Avenue R.O.W. and create a roadway design that is aesthetically pleasing and invites investment opportunity through a more welcoming and functional design as a commercial district rather than a highway.
- Expand network of pathways through Township acquisition or jurisdiction over stream corridors, flood plains, unused rights-of-way, etc.

Goal: Designate and encourage the development of meaningful pedestrian corridors and bikeways linking Township, County and State recreational and community facilities within Ewing and surrounding municipalities.

Goal: Preserve and enhance areas of open space with emphasis on linkages to create greenways.

Objectives:

- Provide for and map greenways along stream corridors, existing parks, dedicated open space in private development, etc.

- Identify and map environmentally constrained lands for preservation using Green Acres R.O.S.I. funding or open space dedication by private developers.

Goal: Coordination of flood mitigation with flood plain and wetlands protection.

Objectives:

- Use flood reduction measures of the Township's pending Flood Reduction Plan, and flood hazard mitigation strategies from the Flood Hazard Mitigation Plan developed by the Township, to enhance and expand floodplains along the Township's major stream corridors.

**COMMENT:** *These goals and their associated objectives are addressed throughout the Plan. The integration of land use and circulation objectives is critical toward realizing the vision and intent of the OARP. The Plan seeks to implement a stronger pedestrian environment while also facilitating an extended network of paths and greenways. In the Parks & Open Space section of page 52, the Plan addresses the implementation of the Ewing-Lawrence Greenway initiative as well as coordination with natural characteristics. While more must be accomplished in subsequent phases with regard to roadway redesign of Olden Avenue itself, the intent to fully combine traffic calming with solid economic development initiatives is evident.*

#### Housing

Goal: Maintain and encourage diversity in the type and character of available housing to promote opportunity for varied residential communities.

Objectives:

- Provide housing opportunities for all groups, family sizes and income levels.
- Where appropriate allow for second story residential apartments in mixed-use buildings.
- Encourage clustering housing options in appropriately zoned locations and at reasonable densities in order to conserve energy and open space and make use of available infrastructure.
- Encourage all neighborhoods, existing and proposed, to embrace an active street tree planting and maintenance program.
- Encourage the development of creative senior citizen housing options to accommodate an aging population. Amend the zoning ordinance where appropriate to permit by right a variety of senior citizen housing options including active adult housing.

**COMMENT:** *This goal and its associated objectives are addressed in various ways throughout the Plan. Specifically, Area #2 is one subarea where this is addressed in its entirety. Additionally, Area #5 facilitates many of these qualities as well.*

Goal: Continue to provide affordable housing in accordance with the Third Round COAH obligation.

**COMMENT:** *The OARP corresponds to the Township's Housing Element and Fair Share Housing Plan.*

#### Utilities

Goal: Maximize and upgrade the use of existing utility systems.

Objectives:

- Continue to upgrade aging infrastructure.
- Upgrade existing infrastructure including retention/detention basins and underground systems in the older, established areas of town and reduce the amount of outfalls releasing directly into stream or river networks.

**COMMENT:** *The Plan will implement this goal and its associated objectives upon approval of development applications.*

Goal: Continue to implement the Flood Hazard Mitigation Plan.

Objectives:

- Acquire property, where financially feasible and appropriate along the flood plain of the Shabakunk Creek.
- Create a regional partnership with Lawrence and Trenton to create a regional retention/detention basin at the confluence of the West Branch and Main Branch of the Shabakunk to promote the inherent benefit to downstream communities and the Assumpink Creek.
- Continue the enhanced maintenance of the Ewing golf range detention facility.
- Continue to implement flood hazard mitigation along the Shabakunk Creek.

**COMMENT:** *This goal and its associated objectives are addressed in various ways throughout the Plan. Specifically, Area #2 is one subarea where this is addressed in its entirety as the former golf range is located in the heart of the subarea, is envisioned to become public-park. Flood mitigation will continue to expand as new development is proposed and coordinated through the provisions of the OARP and the new DEP Stormwater Regulations. Area #3 and Area #8 are additional areas where enhanced flood mitigation is envisioned to occur.*

**Comment on the Goals and Objective section:**

*We find no inconsistencies between the proposed OARP and the Master Plan goals and objectives. Specifically, the goals and objectives are consistent with the OARP.*

**-Land Use Plan Element**

The Land Use Element and plan is the single most important element of a Master Plan as it typically is the culmination of the research conducted during the master planning process and ultimately lays out parameters for the future zoning for the community. The Township's Land Use Element is broken into two primary areas; with I part consisting of the description of the existing conditions and the other representing the Land Use Plan. Within this element are numerous recommendations both generic and specific to areas in the Township, as well as narrative regarding the OARP.

**COMMENT:** *We point out that the Future Land Use Map is not consistent with the text of the Master plan, therefore its utilization as a basis for consistency is suspect at best. It in-fact appears as though the Map merely transferred the highway commercial land use patterns from the past Master Plan and zoning code. As is found throughout the 2006 Master Plan, the text discusses the need for different approaches in pedestrianism and the incorporation of design approaches that reduce auto-dependency.*

The Master Plan breaks the Township into four (4) predominantly north-south bands of land area with distinct development patterns are evident in Ewing Township. These bands are described below and portrayed graphically using aerial photography in the master Plan entitled, *Land Use Bands*. Band 3 & 4 have direct application to the Olden Avenue Redevelopment Area with Band 4 containing the majority of the OARP area.

*Band 3- This represents the largest area of residential development and encompasses almost the entire eastern half of the Township. This band's eastern edge runs in a north-south direction from Ewing's shared municipal border with Lawrence Township in the area of the Antheil Elementary School; heads south through Ewingville Road; and continues along Prospect Street to the border with the City of Trenton. Band 3 is separated from Band 2 at the eastern extent of Lower Ferry Road. The western portion of North Olden Avenue's primary commercial corridor within Band 3 contains significant potential for redevelopment.*

*Band 4- The southern section of Band 4 contains the eastern portion the North Olden Avenue commercial corridor and is included in the Urban Land Institute's (ULI) 2004 study entitled "Mercer County, New Jersey: A Strategy for Redevelopment" - referred to as "Mercer Crossings" due to the confluence of Ewing, Trenton and Lawrence. This corridor is currently in the initial stage of a revitalization effort that the Township must continue to encourage and promote.*

**COMMENT:** *The Olden Avenue Redevelopment Plan is located in both Band 3 and 4 of the Master Plan.*

#### Future Land Use Plan

The Future Land Use Plan details the location and amount of space allocated to each land uses within Ewing and provides clear direction for the Township's development over the next decade. The main goal of the Land Use Element is to safeguard the future of Ewing Township by ensuring that fiscally responsible development and redevelopment maintains the character of the Township. Additionally, the Element will provide recommendations to enhance the existing disjointed pattern of commercial development along the Township's main corridors.

The Olden Avenue Redevelopment Area is located along the Township's main commercial corridor. It extends from Pennington Road to the municipal border and primarily contains retail, restaurant and automobile-related sales and service uses. The reliance on auto-dependent access has limited the full marketability and reduced the aesthetic appeal of this area. As stated previously, the Urban Land Institute (ULI), at the invitation of the Mercer County Planning Board, conducted a study that included the Olden Avenue Redevelopment Area and offered several recommendations to help revitalize the area described as Mercer Crossings. The ULI study recommendations relevant to Ewing Township are identified later in the document.

**COMMENT:** *These statements sets the stage for the OARP and the land use and bulk standards of the Plan.*

#### Olden Avenue Redevelopment Area

Since the adoption of the Olden Avenue Redevelopment Plan on May 13, 1999, three anchor stores (Home Depot, Shop Rite and Aldi's food market) have located on Olden Avenue. They have demonstrated how the redevelopment plan has been able to assist

a property owner or tenant with expediting significant improvements to a parcel. Other notable improvements since the redevelopment plan inception include Eckerd's pharmacy, Sovereign Bank, Yardville National Bank and newly built fast-food establishments. The large-scale redevelopments initiated by Aldi's, Home Depot and Shop Rite have significantly improved their sites, thereby improving the overall appearance and perception of sections of the corridor. Their presence and steady volume of customers sends a reassuring signal to other potential investors that the Olden Avenue corridor is improving.

However, additional work needs to occur to fully revitalize the corridor. The Planning Board as well as the Zoning Board of Adjustment should carefully consider all land development applications for parcels located within the Olden Avenue Redevelopment Area, with specific attention given to the aesthetic impact that the development will have on the corridor. Moreover, variances for signage and waivers for landscaping should be heavily scrutinized and only granted if a true hardship occurs on-site. An over abundance of unnecessary signage and limited landscaping are two fundamental aesthetic elements that can limit the Olden Avenue corridor's development potential.

The anchor stores with large parking lots in the front cater to the automobile. While the intention of the Ewing Township Redevelopment Agency is to let market forces guide the types of development that occur, the Township can take measures to promote smaller scale, pedestrian- and bicycle-oriented development along Olden Avenue. This "human scale" development brings commercial and retail uses closer to the street and provides easier and safer access for customers.

**COMMENT:** *These statements sets the stage for the OARP. They are the intent behind the enhance development and design standards as well as the concept of eliminating the Plan as a mere overlay. The Plan supercedes the land use, bulk and design provisions of the current Land Development Ordinances, many of which contradicted the Master Plan. The Plan presents complete land use and bulk regulations. All other provisions of the Township LDO, such as trash and recycling, drainage, sanitary sewer, technical submission requirements, and fees remain in effect. Taking this approach not only implements the Master Plan as it pertains to the Olden Avenue Redevelopment Area, but strengthens the role of the Planning Board through a unified set of development ordinances that are not competing.*

#### The ULI Study

The ULI study evaluates the redevelopment opportunities available to the Mercer Crossings area and provides a guide to redevelopment of the area by offering immediate and long-term strategies. The following is a list of recommendations taken from the study that after a comprehensive review, Ewing Township may consider implementing:

1. Mercer County, Ewing Township, City of Trenton and Lawrence Township must work together and appoint a local facilitator charged with implementing a Multi-jurisdictional Redevelopment Plan.
2. Adopt an interlocal agreement that ensures all four jurisdictions will have equal responsibility in establishing:
  - a. A shared vision;
  - b. A collaborative planning strategy or district;
  - c. A master plan for the area;
  - d. Zoning ordinances based on the shared vision;
  - e. A coordinated site plan review process;

- f. A Multi-jurisdictional Capital Improvement Plan; and
  - g. Opportunities for the transfer of development among the jurisdictions.
3. An improved system of street circulation in the entire area through road extensions and new roads that offer east-west travel.
  4. Improved transit connections through coordination with NJ Transit to increase the number of routes.
  5. Development of an inventory of the areas in need of enhancement to inform prospective developers of desired investment.
  6. A mixed type of housing stock that will offer one-, two- and three-bedroom units.
  7. A means to attract a variety of people to the area by including retail and entertainment possibly at the transfer facility/Boehm site. A means to attract possible redevelopment of the Spruce Street and Princeton Avenue intersection by relocating the Farmers Market south to the more accommodating transfer facility/Boehm site. A market plan for the existing development potential of the Capital Plaza frontage on Olden Avenue.
  8. Minimal traffic calming measures to force drivers to slow down and “take-in” the area, as well as create a pedestrian friendly location that is more attractive to residential development.
  9. The creation of pocket parks and random open space areas for public enjoyment.
  10. A streetscape improvement plan for Olden Avenue and Spruce Street to enhance the corridor and make it more aesthetically pleasing. This should include:
    - a. Access management control;
    - b. Tree planting along sidewalks;
    - c. Wider sidewalks;
    - d. Attractive street lighting fixtures, banners and signage that includes the areas newly branded name; and
    - e. Intersection design improvement that are pedestrian friendly and facilitate the movement of traffic.

The ULI study acknowledges that the Olden Avenue Redevelopment Area did not become what it is today overnight and it will take time to fully revitalize. However, by initially focusing on short-term improvement, the Township can take immediate steps to help re-establish this location as a destination point for the residents of Ewing and Mercer County. This area will only reach its full potential if all of the jurisdictions involved work in a coordinated manner with one agency taking the lead and ensuring that plans and projects are followed through to fruition. Open and consistent communications between Ewing, Trenton, and Lawrence and Mercer County must occur to allow this area to prosper once again.

**COMMENT:** *This section of the Master Plan, while conceptual, sets the stage for the OARP.*

#### Land Use Element Summary and Recommendations

- Scrutinize applications in the Olden Avenue Redevelopment Area, particularly with regard to landscaping and signage with the intention of bringing about a positive visual image along the corridor;
- Implement relevant recommendations of the ULI, Mercer Crossings Study along Olden Avenue and Spruce Street corridors;
- Adopt an FAR ordinance;

- Redefine/decrease total lot coverage requirements in order to control residential parking fields;

**Comment on the Land Use Element:**

The OARP and the Land Use Element and Plan are consistent. In-fact, the OARP is an implementation mechanism of the Master Plan.

**-Circulation Plan Element**

Introduction

Due to more rapid growth in other parts of Mercer County, there has been a change of commercial uses along Olden Avenue, Spruce Street, and Artic Parkway, and significant highway improvements near the Interstate 95 interchanges that have affected the traffic patterns in the Township... The number one goal for the transportation system is to provide safe and efficient movement for all roadway users. This includes motor vehicles, transit users, pedestrians and bicyclists. Most streets throughout the day have acceptable levels of service and Ewing should emphasize keeping the existing system in good repair. Areas of congestion include Route 95, Bear Tavern Road, Parkway and Olden Avenue, and Route 31 during the morning and afternoon peak hours near major employers and schools. Some critical transportation investments could strategically enhance the mobility and accessibility for residents and a reasonable level of development consistent with the land use plan.

**COMMENT:** These statements set the stage for the OARP. The integration between the land use standards and the circulation component of the OARP work hand in hand.

Existing Conditions

Ewing Township has a high dependency on the automobile, particularly the single occupancy vehicle, similar to other suburban municipalities with the State of New Jersey.

Transit use is slightly lower than the county and state numbers indicating that work destinations are not accessible to transit.

The walk to work rate of 6.9% is higher than the county and State averages. This shows the importance of having a sidewalk system that connects to more than just residential neighborhoods.

**COMMENT:** These statements set the stage for the OARP. The integration between the land use standards and the circulation component of the OARP work hand in hand.

**-Accident Data Analysis**

Based on the input from the Township's Police Department, accident reports were obtained and tabulated. These reports are not necessarily all the accidents for each year but represent the data provided to analyze.

Accident report data for Township of Ewing was obtained for the immediate past two-years (2002 and 2003). The data were assessed to identify locations with unusual accident history in order to highlight areas for further/detailed accident study and could be used to suggest appropriate mitigation aimed at enhancing safety. Accident data showing more than 20 accidents per year were disaggregated by type to enable postulation of accident causes in terms of geometry and traffic control devices.

A review of the data identifies a total of 229 accidents in Ewing Township for 2002 and 2003. Of these, a total of 142 (62%) accidents occurred along Olden Avenue between Pennington Road and Princeton Avenue with the highest frequency occurring in the area of Artic Parkway (39, 17%), Prospect Street (32, 14%), Parkside Avenue (28, 12%) and Pennington Road (24, 11%).

Fifty-five accidents occurred at intersections within Ewing Township, with 54 occurring in 2003. Of the 54 intersection accidents in 2003, 13 (25%) occurred at the intersection of Lower Ferry Road with Parkway Avenue and 9 (17%) occurred at the intersection of Olden Avenue with Parkside Avenue. A further review of the accident types at these two locations identified that 10 of the accidents at Lower Ferry Road involved southbound vehicles turning left from Parkway Avenue with through vehicles along Lower Ferry Road. At the intersection of Olden Avenue and Parkside Avenue, six (6) of the accidents involved through vehicles along Olden Avenue hitting through northbound vehicles. This could indicate red light running or signal visibility questions that need to be addressed. A total of 22 accidents occurred at various intersections along Olden Avenue.

One fatality occurred in 2003 along Olden Avenue in the area of 5<sup>th</sup> Street. Accidents with injuries totaled 112, which translate to approximately 49% of all the accidents that were reported. The highest number of accidents with injuries (15) occurred along Olden Avenue in the area of Pennington Road, with 12 accidents with injuries occurring along the same avenue in the area of Prospect Street and Artic Parkway. Of the accidents with injuries, 78 (70%) occurred along Olden Avenue.

The Summary indicates that many accidents occur at or near intersections and that driver inattention or the complexity of traffic conflicts near the intersections contributes to these locations experiencing more accidents. Each location needs further analysis to see if geometric, access management, or other visibility issues could be addressed to mitigate the circumstances at various locations. Another option that may be considered is lowering the speed limit to create more time for drivers to react to situations along the roadway. Besides focusing on Olden Avenue, Route 31 and Upper Ferry Road/ Ewingville Road and Parkway Avenue and Lower Ferry Road are known to have frequent accidents.

**COMMENT:** *These statements sets the stage for the importance of addressing the issues contained in the OARP.*

-Roadway Jurisdiction

*North Olden Avenue (CR 622)*

The North Olden Avenue Extension continues outside Trenton near Route 206 to Parkway Avenue as an urban minor arterial. It is an undivided four-lane county facility with a 40 m.p.h. speed limit for the 2.23 miles in Ewing and seven traffic signals along the commercial corridor. Within the last two years, three major commercial operators opened a Home Depot opposite Artic Parkway, a Shop Rite near Prospect Street and Parkside Avenue and an Aldi food store. Development is adjacent to the highway and there are sidewalks along part but not all the corridor. Ewing Township is pursuing community designs that could slow traffic through a "road diet" to improve pedestrian accessibility and a "Main Street" appearance. This is dependent on cooperation and coordination with Mercer County.

**COMMENT:** *These statements set the stage for the OARP. The integration between the land use standards and the circulation component of the OARP work hand in hand. Additionally, the*

OARP requires additional coordination between the County, Township and business community to achieve this.

#### Olden Avenue Revitalization

Olden Avenue is a designated redevelopment area in Ewing Township. There is a state Livable Communities grant to develop and design a strategic business plan for the corridor. The Redevelopment Authority is interested in developing streetscapes and increasing cross-town transit use along the corridor. Special emphasis is needed to enhance pedestrian activity along this commercial corridor, as business density and building proximity to the street is conducive for interconnecting access along the corridor. Additionally, the reliance on auto-dependent access has reduced the aesthetic appeal of the corridor.

The ULI study for "Mercer Crossings" includes part of Olden Avenue, Artic Parkway, Spruce and Brunswick Pike. The report on this is favorable; however, the concepts were not tested from a traffic perspective. Many of the ideas suggested in the report to encourage revitalization include using traffic calming to slowdown traffic and increase pedestrian and bicycle activity, which are consistent with the emphasis of this Circulation Element. The ULI report emphasizes improving the grid pattern of roadways and proposes the eventual extension of Calhoun Street to connect to Olden Avenue through a possible redevelopment area.

**COMMENT:** These statements set the stage for the OARP. The integration between the land use standards and the circulation component of the OARP work hand in hand. Additionally, the OARP requires additional coordination between the County, Township and business community to achieve this.

#### Transportation Element Summary and Recommendations

The Ewing Circulation Plan recommends several actions to guide transportation improvements for the future. As mentioned before, the number one goal for the transportation system is to provide safe and efficient movement for all roadway users. This includes motor vehicles, transit users, pedestrians and bicyclists. Ewing Township has many opportunities to meld transportation improvements at the local and regional levels to support Smart Growth, manage travel demand, and increase transit, pedestrian and bicycle options. By working in partnership with various agencies, adjacent municipalities and private developers, a dynamic circulation plan will balance quality of life goals and community desires with essential reasonable land development growth. This can be done through traffic calming and explicit roadway designs. Transportation projects link people to vital activities in their community and provide connections between various land uses. The Circulation Plan suggests limited strategic roadway improvements that are needed to provide for future growth and development of Ewing Township.

**COMMENT:** These statements set the stage for the OARP. The integration between the land use standards and the circulation component of the OARP work hand in hand to provide a true multi-modal transportation system.

#### -Olden Avenue

Olden Avenue has established as a Redevelopment Zone in Ewing Township and improvements are warranted. It is divided into seven areas including commercial areas

and neighborhoods. The corridor is conducive to improving pedestrian access to businesses by completing sidewalks and upgrading access along the corridor. The design of the area is to reflect the land uses along that part of the corridor. As part of the Urban Land Institute (ULI) report known as "Mercer Crossings", additional development and redevelopment opportunities were analyzed and includes potential street realignments and extensions that attempt to mitigate current and future traffic concerns in the area surrounding the convergence of the municipal borders of Ewing, Trenton and Lawrence. The Circulation Plan should consider the ULI report including the access improvements near the Capitol Plaza and possible street extensions to serve the region.

**COMMENT:** *These statements set the stage for the OARP. The integration between the land use standards and the circulation component of the OARP work hand in hand. Additionally, the OARP requires additional coordination between the County, Township and business community to achieve this.*

#### -Traffic Calming

There are several techniques available that neighborhoods concerned with cut-through traffic can use to lessen these impacts. In order to reclaim their neighborhoods, speed humps and other measures can be used to reduce speed and traffic volumes in some areas.

New Jersey has made available specific criteria to be used in determining whether speed humps are appropriate under A-2166. In centers, neckdowns of roads at intersections can increase pedestrian use and help emphasize the human scale of development in business areas. Chicanes or diverters also reroute traffic to effectively slowdown traffic or reduce through traffic volumes on local streets.

**COMMENT:** *These statements set the stage the OARP.*

#### -Improved Connectivity of Pedestrian and Bicycle Facilities

Another concern is the inconvenience and sometimes the lack of facilities to provide for adequate accessibility for pedestrians and bicyclists to certain areas of the Township and particularly parks, schools, recreational fields, and local businesses. The Township shall continue to encourage the provision of needed connections and support the filling-in of missing links for sidewalks and bicycle compatible roads through the review of all future development applications.

A multi-jurisdictional committee to consider developing a Regional Greenways should be established and administered by Mercer County.

**COMMENT:** *These statements set the stage for the OARP. The integration between the land use standards and the circulation component of the OARP work hand in hand. Additionally, the OARP requires additional coordination between the County, Township and business community to achieve this.*

#### -Strategic Improvements and Connections

Ewing Township in conjunction with the County, the DVRPC, the State and in some instances adjacent municipalities needs to ensure that items on the strategic improvement list move expeditiously and in coordination with the development patterns that come before the Township and region. The improvements are consistent with the goals of supporting Smart Growth by focusing growth in infill areas where infrastructure

currently exists and will encourage the use and growth of public transit options and alternatives. Furthermore, alternatives are to maximize accessibility potential for pedestrian, bicycle and transit users and reduce reliance on auto-dependent travel.

A series of Strategic Improvements for enhancing traffic flow to meet future development needs includes:

- North Olden Avenue improvements to facilitate redevelopment activities and slow down traffic including streetscapes and expanded bus service to reduce congestion
- Extension of Calhoun Street to Olden Avenue
- Enhanced bicycle, greenway and pedestrian systems

**COMMENT:** *These statements set the stage for the OARP. The integration between the land use standards and the circulation component of the OARP work hand in hand. Additionally, the OARP requires additional coordination between the County, Township and business community to achieve this.*

-Comprehensive Roadway Maintenance Plan  
Safety Improvements

Based on analysis of the accident data, develop a priority of locations that need further investigation and quick remediation. These include areas along Olden Avenue near critical intersections and Capitol Plaza, Olden Avenue and Fifth Street, Princeton Avenue and Spruce Street, and Route 31. Additionally, to improve safety and slow traffic along Spruce Street, it is recommended that the intersection of Prospect Street and Spruce Street be upgraded to include a traffic signal.

**COMMENT on Circulation:**

*The OARP and the Circulation Element are substantially consistent with the Master Plan. In many ways, the OARP implements the Master Plan. Although more work needs to be done to fully implement the Circulation Element through the OARP, the Plan addresses the important issues described in the Master Plan.*

**Comment regarding the consistency between the OARP and the Township Master Plan:**

*The OARP advances a number of other general recommendations contained in the various elements of the master plan. The OARP ordinance is not inconsistent with the master plan. Additionally, pursuant to the requirements of the Local Redevelopment and Housing Law (40A: 12A-7d), the OARP appears to be "substantially consistent with the municipal master plan or designed to effectuate the master plan."*

APPENDIX

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*..... Technical Roadway Reference Material. ....*